



SOUTHERN
AFRICAN
DEVELOPMENT
COMMUNITY

REGULATORY SIGNS

SECTIONS

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CHAPTER 2

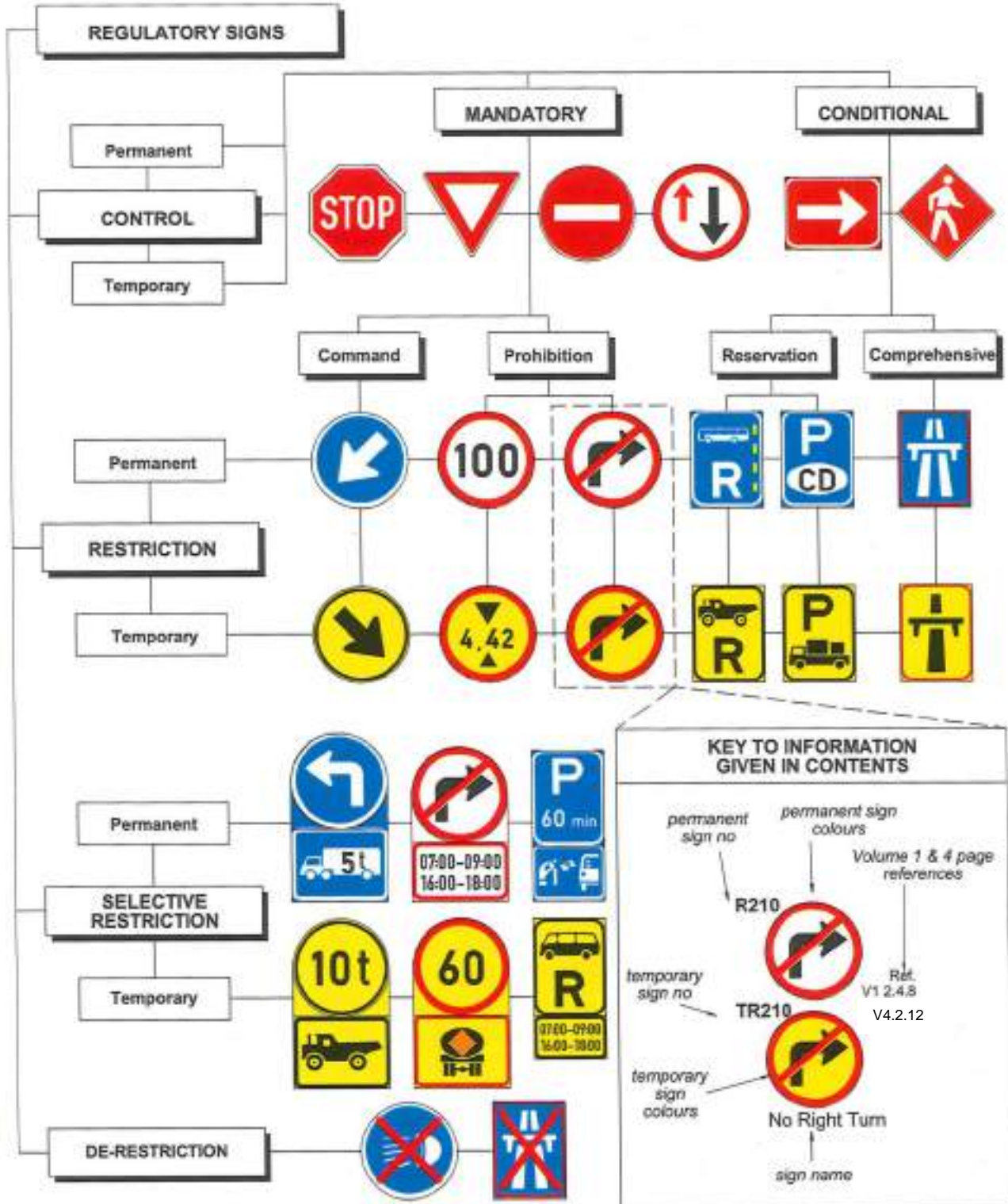
CHAPTER 2: REGULATORY SIGNS

2.0 CONTENTS

This contents listing illustrates each officially approved sign in the regulatory sign class with the sign number and name. A page reference is given within this chapter where each sign is discussed and a cross reference is given to Volume 4 where the symbol and other data are detailed.

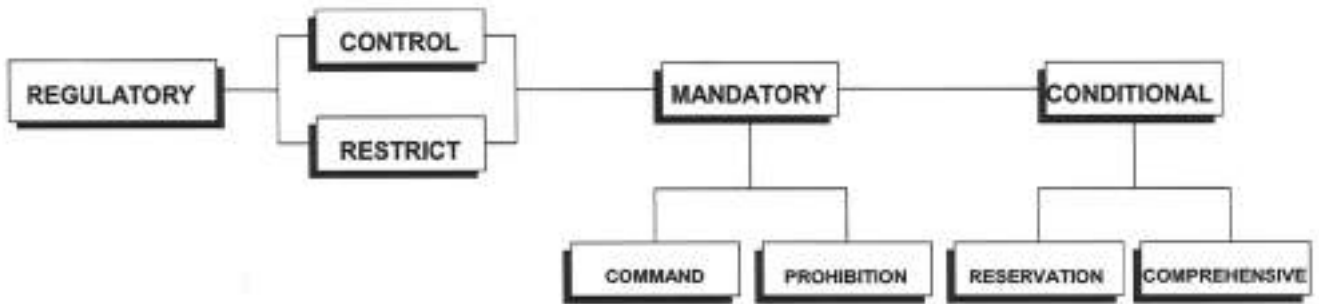
The front page of the Contents gives an over view of the regulatory sign class and its subdivisions which are explained in more detail on the following pages and in Section 2.1.

REGULATORY SIGN CLASSIFICATION AND COLOUR CODE

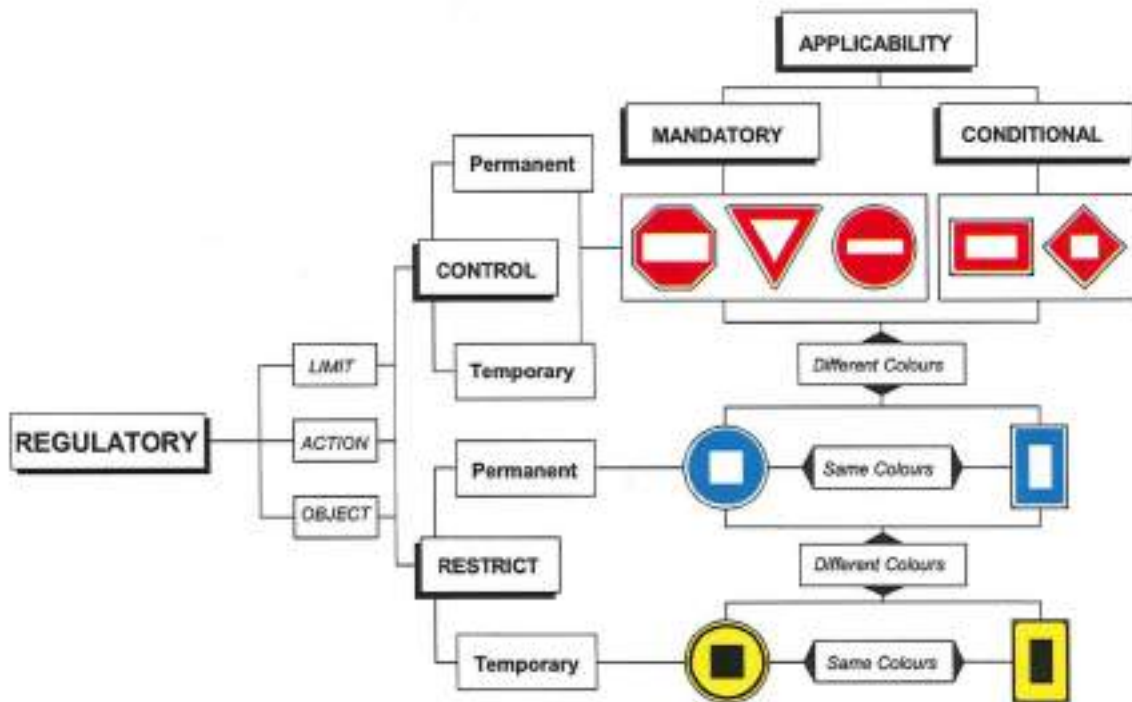


The front page of the Contents gives an overview of the Regulatory sign class and its subdivision using actual sign examples. Figures 2.1 to 2.3 expand upon this illustration to show how the shape and colour of signs have significance. The Regulatory sign class is a relatively complex grouping of signs, not least for the reason that these signs, more than any others, have legal significance both for road users and for authorities erecting the signs. It is important therefore that the applicability of each type of sign is clearly understood by all. In Figures 2.1 and 2.2 the

generic way to emphasise the importance of sign shape and sign colour. Regulatory sign message can generally be grouped into messages given by NUMBERS (LIMITS), ARROWS (ACTIONS) and most commonly SYMBOL (OBJECT), although in a limited number of cases, for instance, an ACTION can be illustrated by a SYMBOL. Regulatory signs are grouped into six different numbered series – R1, R100, R200, R300, R400 and @500 with a small number of specific de-restriction in a R600 series.



Detail 2.1.1 Basic Regulatory Sign Class Subdivisions



Detail 2.1.2 Development of Shape and Colour

Fig 2.1 Regulatory Sign Classification

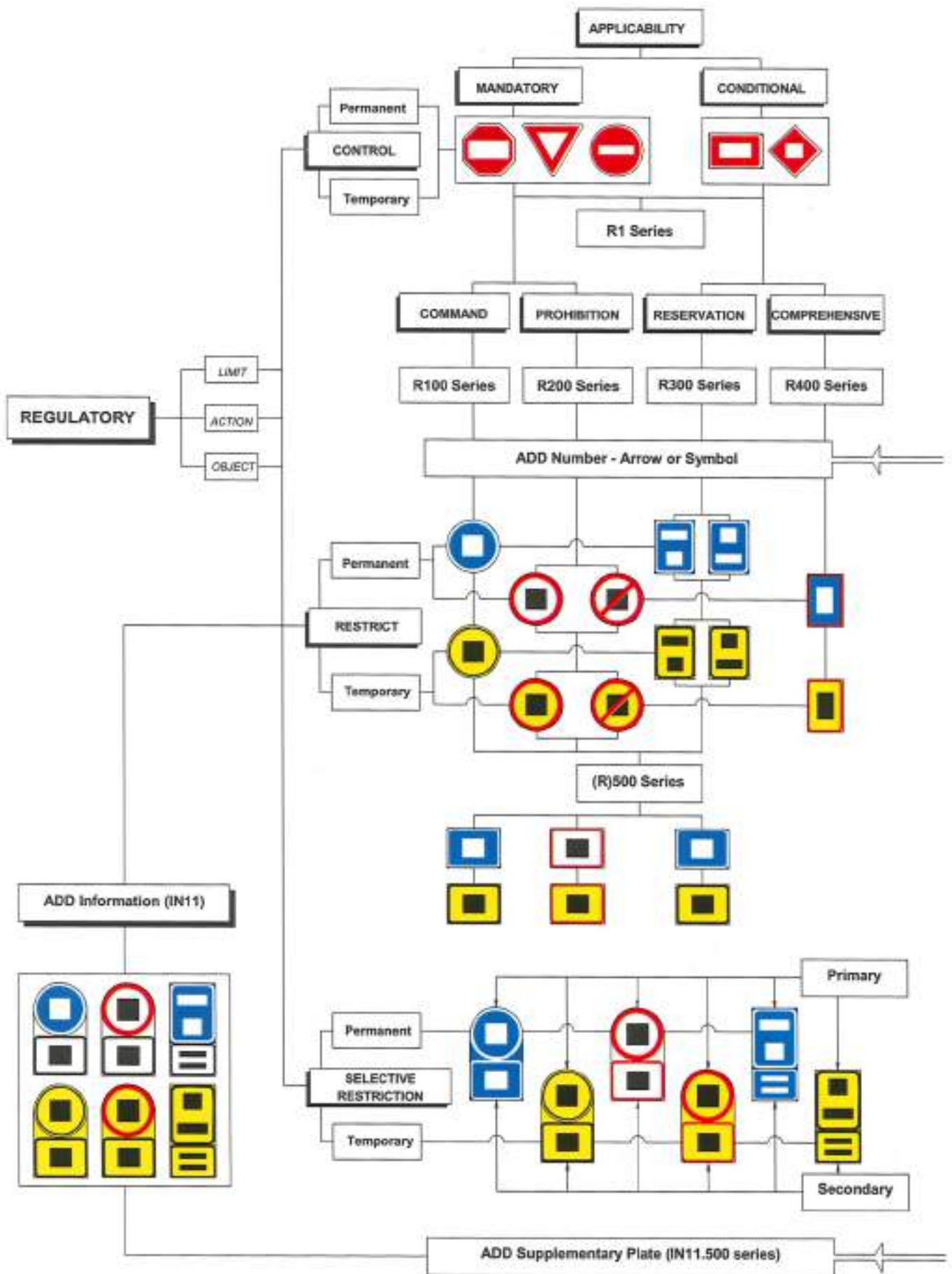
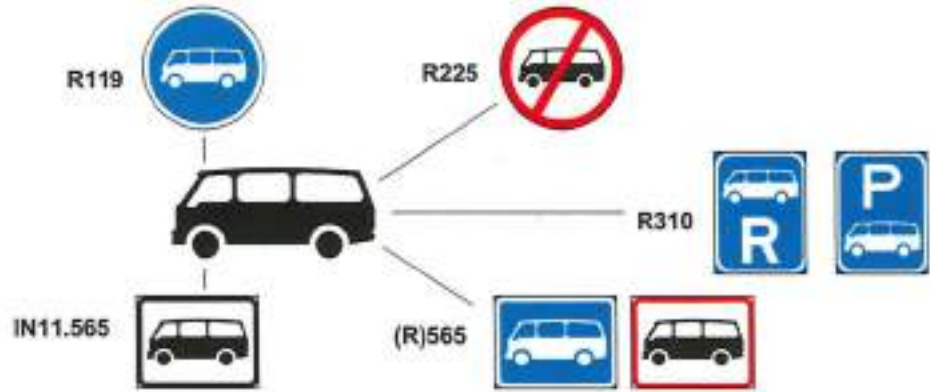


























Fig 2.2

Regulatory Sign Shape and Colour Code



















KEY TO SYMBOL APPLICATION
BY SIGN SERIES

<p>R121</p>  <p>R227</p> <p>R301</p> <p>IN11.560</p> <p>(R)560</p>	<p>R111</p>  <p>R219</p> <p>R304</p> <p>IN11.561</p> <p>(R)561</p>	<p>R116</p>  <p>R222</p> <p>R307</p> <p>IN11.562</p> <p>(R)562</p>	<p>R117</p>  <p>R223</p> <p>R308</p> <p>IN11.563</p> <p>(R)563</p>
<p>R118</p>  <p>R224</p> <p>R309</p> <p>IN11.564</p> <p>(R)564</p>	<p>R119</p>  <p>R225</p> <p>R310</p> <p>IN11.565</p> <p>(R)565</p>	<p>R120</p>  <p>R226</p> <p>R311</p> <p>IN11.566</p> <p>(R)566</p>	<p>R122</p>  <p>R228</p> <p>R312</p> <p>IN11.567</p> <p>(R)567</p>
<p>R125</p>  <p>R229</p> <p>R313</p> <p>IN11.568</p> <p>(R)568</p>	<p>R124</p>  <p>R230</p> <p>R314</p> <p>IN11.569</p> <p>(R)569</p>	<p>R125</p>  <p>R231</p> <p>R315</p> <p>IN11.570</p> <p>(R)570</p>	<p>R126</p>  <p>R232</p> <p>R316</p> <p>IN11.571</p> <p>(R)571</p>
<p>R127</p>  <p>R233</p> <p>R317</p> <p>IN11.572</p> <p>(R)572</p>	<p>R128</p>  <p>R234</p> <p>R318</p> <p>IN11.573</p> <p>(R)573</p>	<p>R129</p>  <p>R235</p> <p>R319</p> <p>IN11.574</p> <p>(R)574</p>	<p>R320</p>  <p>IN11.575</p> <p>(R)575</p>
<p>R321</p>  <p>R321</p> <p>IN11.576</p> <p>(R)576</p>	<p>R322</p>  <p>R322</p> <p>IN11.577</p> <p>(R)577</p>	<p>R323</p>  <p>R323</p> <p>IN11.578</p> <p>(R)578</p>	<p>R324</p>  <p>R324</p> <p>IN11.579</p> <p>(R)579</p>
<p>R130</p>  <p>IN11.580</p> <p>(R)580</p>	<p>R131</p>  <p>R236</p> <p>IN11.581</p> <p>(R)581</p>	<p>R240</p>  <p>R240</p> <p>IN11.582</p> <p>(R)582</p>	<p>R138</p>  <p>R338</p> <p>IN11.583</p> <p>(R)583</p>

Section 2.2: CONTROL - Regulatory Signs

● = See Section 2.10 for variations appropriate to individual countries.



























<p>R1</p>  <p>References R1, R1.1, R1.3 and R1.4 V1 2.2.1-2.2.5 V4 2.2.1 and 2.2.3</p> <p>Stop</p>	<p>R1.1</p>  <p>Stop</p>	<p>R1.2</p>  <p>Ref. V1 2.2.1 V4 2.2.2</p> <p>Stop/Yield</p>	<p>R1.3</p>  <p>3 - Way Stop</p>	<p>R1.4</p>  <p>4 - Way Stop</p>
<p>R1.5A</p>  <p>References V1 2.2.3 V4 2.2.1</p> <p>Stop (Stop/Go Control)</p>	<p>R1.5B ●</p>  <p>References V1 2.2.3 V4 2.2.4</p> <p>Go (Stop/Go Control)</p>	<p>R2</p>  <p>References V1 2.2.6 V4 2.2.5</p> <p>Yield</p>	<p>R2.1</p>  <p>References V1 2.2.7 V4 2.2.6</p> <p>Yield to Pedestrians</p>	<p>R2.2 ●</p>  <p>References V1 2.2.8 V4 2.2.7</p> <p>Yield at Mini Circle</p>
<p>R3</p>  <p>References V1 2.2.9 V4 2.2.8</p> <p>No Entry</p>	<p>R4.1</p>  <p>References V1 2.2.10 V4 2.2.9</p> <p>One - Way (Left)</p>	<p>R4.2</p>  <p>References V1 2.2.10 V4 2.2.9</p> <p>One - Way (Right)</p>	<p>R4.3</p>  <p>References V1 2.2.10 V4 2.2.9</p> <p>One - Way (Straight - On)</p>	<p>R5</p>  <p>References V1 2.2.11 V4 2.2.10</p> <p>Pedestrian Priority</p>
<p>R6 ●</p>  <p>References V1 2.2.12 V4 2.2.11</p> <p>Yield to Oncoming Traffic</p>				

Section 2.3: COMMAND - Mandatory Regulatory Signs

● = See Section 2.10 for variations appropriate to individual countries.

<p>R101</p> <p>Ref. V1 2.3.1 V4 2.3.1</p> <p>TR101</p> <p>Minimum Speed</p>	<p>R102</p> <p>Ref. V1 2.3.2 V4 2.3.2</p> <p>TR102</p> <p>Vehicles Exceeding Mass Only</p>	<p>R103</p> <p>Ref. V1 2.3.3 V4 2.3.3</p> <p>TR103</p> <p>Keep Left</p>	<p>R104</p> <p>Ref. V1 2.3.3 V4 2.3.4</p> <p>TR104</p> <p>Keep Right</p>	<p>R105</p> <p>Ref. V1 2.3.4 V4 2.3.5</p> <p>TR105</p> <p>Proceed Left Only</p>
<p>R106</p> <p>Ref. V1 2.3.4 V4 2.3.6</p> <p>TR106</p> <p>Proceed Right Only</p>	<p>R107</p> <p>Ref. V1 2.3.4 V4 2.3.7</p> <p>TR107</p> <p>Proceed Straight Only</p>	<p>R108</p> <p>Ref. V1 2.3.5 V4 2.3.8</p> <p>TR108</p> <p>Turn Left</p>	<p>R109</p> <p>Ref. V1 2.3.5 V4 2.3.9</p> <p>TR109</p> <p>Turn Right</p>	<p>R110</p> <p>Ref. V1 2.3.6 V4 2.3.10</p> <p>TR110</p> <p>Pedestrians Only</p>
<p>R111</p> <p>Ref. V1 2.3.7 V4 2.3.11</p> <p>TR111</p> <p>Cyclists Only</p>	<p>R112</p> <p>Ref. V1 2.3.8 V4 2.3.12</p> <p>TR112</p> <p>Cyclists and Pedestrians Only</p>	<p>R113</p> <p>Ref. V1 2.3.8 V4 2.3.13</p> <p>TR113</p> <p>Cyclists and Pedestrians Only with Barrier</p>	<p>R114</p> <p>Ref. V1 2.3.8 V4 2.3.14</p> <p>TR114</p> <p>Pedestrians and Cyclists Only</p>	<p>R115</p> <p>Ref. V1 2.3.8 V4 2.3.15</p> <p>TR115</p> <p>Pedestrians and Cyclists Only with Barrier</p>
<p>R116</p> <p>Ref. V1 2.3.10 V4 2.3.18</p> <p>TR116</p> <p>Motor Cycles Only</p>	<p>R117</p> <p>Ref. V1 2.3.10 V4 2.3.17</p> <p>TR117</p> <p>Motor Cars Only</p>	<p>R118</p> <p>Ref. V1 2.3.10 V4 2.3.18</p> <p>TR118</p> <p>Taxi's Only</p>	<p>R119</p> <p>Ref. V1 2.3.10 V4 2.3.19</p> <p>TR119</p> <p>Minibuses Only</p>	<p>R120</p> <p>Ref. V1 2.3.11 V4 2.3.20</p> <p>TR120</p> <p>Midi-buses Only</p>
<p>R121</p> <p>Ref. V1 2.3.11 V4 2.3.21</p> <p>TR121</p> <p>Buses Only</p>	<p>R122</p> <p>Ref. V1 2.3.11 V4 2.3.22</p> <p>TR122</p> <p>Delivery Vehicles Only</p>	<p>R123</p> <p>Ref. V1 2.3.11 V4 2.3.23</p> <p>TR123</p> <p>Goods Vehicles Only</p>	<p>R124</p> <p>Ref. V1 2.3.12 V4 2.3.24</p> <p>TR124</p> <p>Goods Vehicles over GVM Only</p>	<p>R125</p> <p>Ref. V1 2.3.12 V4 2.3.25</p> <p>TR125</p> <p>Construction Vehicles Only</p>

● = See Section 2.10 for variations appropriate to individual countries.

<p>R126</p>  <p>Ref. V1 2.3.12 V4 2.3.26</p> <p>TR126</p>  <p>Vehicles - Dangerous Goods Only</p>	<p>R127 ●</p>  <p>Ref. V1 2.3.12 V4 2.3.27</p> <p>TR127</p>  <p>Abnormal Vehicles Only</p>	<p>R128</p>  <p>Ref. V1 2.3.13 V4 2.3.28</p> <p>TR128</p>  <p>Rickshaws Only</p>	<p>R129 ●</p>  <p>Ref. V1 2.3.13 V4 2.3.29</p> <p>TR129</p>  <p>Tour Buses Only</p>	<p>R130</p>  <p>Ref. V1 2.3.13 V4 2.3.30</p> <p>TR130</p>  <p>Agricultural Vehicles Only</p>
<p>R131</p>  <p>Ref. V1 2.3.13 V4 2.3.31</p> <p>TR131</p>  <p>Animal - Drawn Vehicles Only</p>	<p>R132</p>  <p>Ref. V1 2.3.14 V4 2.3.32</p> <p>Pay Toll</p>	<p>R133</p>  <p>Ref. V1 2.3.15 V4 2.3.33</p> <p>TR133</p>  <p>Switch Head Lamps On</p>	<p>R134</p>  <p>Ref. V1 2.3.16 V4 2.3.34</p> <p>TR134</p>  <p>Buses and Minibuses Only</p>	<p>R135</p>  <p>Ref. V1 2.3.16 V4 2.3.35</p> <p>TR135</p>  <p>Buses and Midibuses Only</p>
<p>R136</p>  <p>Ref. V1 2.3.16 V4 2.3.36</p> <p>TR136</p>  <p>Buses, Midibuses and Minibuses Only</p>	<p>R137 ●</p>  <p>Ref. V1 2.3.17 V4 2.3.37</p> <p>TR137</p>  <p>Roundabout</p>	<p>R138</p>  <p>Ref. V1 2.3.18 V4 2.3.38</p> <p>Trams Only</p>	<p>R139</p>  <p>Ref. V1 2.3.18 V4 2.3.39</p> <p>Buses and Trams Only</p>	<p>R140</p>  <p>References V1 2.3.18 V4 2.3.40</p> <p>Buses, Trams and Minibuses Only</p>

Section 2.4: PROHIBITION - Mandatory Regulatory Signs

● = See Section 2.10 for variations appropriate to individual countries.

<p>R201</p> <p>Ref. V1 2.4.1 V4 2.4.2</p> <p>TR201</p> <p>Speed Limit</p>	<p>R202</p> <p>Ref. V1 2.4.2 V4 2.4.4</p> <p>TR202</p> <p>Mass Limit</p>	<p>R203</p> <p>Ref. V1 2.4.3 V4 2.4.5</p> <p>TR203</p> <p>Axle Massload Limit</p>	<p>R204</p> <p>Ref. V1 2.4.4 V4 2.4.6</p> <p>TR204</p> <p>Height Limit</p>	<p>R205</p> <p>Ref. V1 2.4.5 V4 2.4.7</p> <p>TR205</p> <p>Length Limit</p>
<p>R206</p> <p>Ref. V1 2.4.8 V4 2.4.8</p> <p>No Excessive Noise</p>	<p>R207</p> <p>Ref. V1 2.4.6 V4 2.4.9</p> <p>No Hitch-Hiking</p>	<p>R208</p> <p>Ref. V1 2.4.7 V4 2.4.10</p> <p>TR208</p> <p>No Unauthorized Vehicles</p>	<p>R209</p> <p>Ref. V1 2.4.8 V4 2.4.11</p> <p>TR209</p> <p>No Left Turn Ahead</p>	<p>R210</p> <p>Ref. V1 2.4.8 V4 2.4.12</p> <p>TR210</p> <p>No Right Turn Ahead</p>
<p>R211</p> <p>Ref. V1 2.4.9 V4 2.4.13</p> <p>TR211</p> <p>No Left Turn</p>	<p>R212</p> <p>Ref. V1 2.4.9 V4 2.4.14</p> <p>TR212</p> <p>No Right Turn</p>	<p>R213</p> <p>Ref. V1 2.4.9 V4 2.4.15</p> <p>No U-Turn</p>	<p>R214</p> <p>Ref. V1 2.4.10 V4 2.4.16</p> <p>TR214</p> <p>No Overtaking - All Vehicles</p>	<p>R215</p> <p>Ref. V1 2.4.10 V4 2.4.17</p> <p>TR215</p> <p>No Overtaking - Goods Vehicles</p>
<p>R216</p> <p>Ref. V1 2.4.11 V4 2.4.18</p> <p>TR216</p> <p>No Parking</p>	<p>R217</p> <p>Ref. V1 2.4.12 V4 2.4.19</p> <p>TR217</p> <p>No Stopping</p>	<p>R218</p> <p>Ref. V1 2.4.13 V4 2.4.20</p> <p>TR218</p> <p>No Pedestrians</p>	<p>R219</p> <p>Ref. V1 2.4.14 V4 2.4.21</p> <p>TR219</p> <p>No Cyclists</p>	<p>R220</p> <p>Ref. V1 2.4.15 V4 2.4.22</p> <p>TR220</p> <p>No Cyclists and Pedestrians</p>
<p>R221/TR221 Numbers not allocated</p>	<p>R222</p> <p>Ref. V1 2.4.17 V4 2.4.23</p> <p>TR222</p> <p>No Motor Cycles</p>	<p>R223</p> <p>Ref. V1 2.4.17 V4 2.4.24</p> <p>TR223</p> <p>No Motor Cars</p>	<p>R224</p> <p>Ref. V1 2.4.17 V4 2.4.25</p> <p>TR224</p> <p>No Taxi's</p>	<p>R225</p> <p>Ref. V1 2.4.17 V4 2.4.26</p> <p>TR225</p> <p>No Minibuses</p>




● = See Section 2.10 for variations appropriate to individual countries.

<p>R226</p>  <p>Ref. V1 2.4.18 V4 2.4.27</p> <p>TR226</p>  <p>No Midi-Buses</p>	<p>R227</p>  <p>Ref. V1 2.4.18 V4 2.4.28</p> <p>TR227</p>  <p>No Buses</p>	<p>R228</p>  <p>Ref. V1 2.4.18 V4 2.4.29</p> <p>TR228</p>  <p>No Delivery Vehicles</p>	<p>R229</p>  <p>Ref. V1 2.4.18 V4 2.4.30</p> <p>TR229</p>  <p>No Goods Vehicles</p>	<p>R230</p>  <p>Ref. V1 2.4.19 V4 2.4.31</p> <p>TR230</p>  <p>No Goods Vehicles over G.V.M</p>
<p>R231</p>  <p>Ref. V1 2.4.19 V4 2.4.32</p> <p>TR231</p>  <p>No Construction Vehicles</p>	<p>R232</p>  <p>Ref. V1 2.4.19 V4 2.4.33</p> <p>TR232</p>  <p>No Vehicles Carrying Dangerous Goods</p>	<p>R233</p>  <p>Ref. V1 2.4.19 V4 2.4.34</p> <p>TR233</p>  <p>No Abnormal Vehicles</p>	<p>R234</p>  <p>Ref. V1 2.4.20 V4 2.4.35</p> <p>TR234</p>  <p>No Rickshaws</p>	<p>R235</p>  <p>Ref. V1 2.4.20 V4 2.4.36</p> <p>TR235</p>  <p>No Tour Buses</p>
<p>R236</p>  <p>Ref. V1 2.4.20 V4 2.4.37</p> <p>TR236</p>  <p>No Agricultural Vehicles</p>	<p>R237</p>  <p>Ref. V1 2.4.20 V4 2.4.38</p> <p>TR237</p>  <p>No Animal-Drawn Vehicles</p>	<p>R238</p>  <p>Ref. V1 2.4.21 V4 2.4.39</p> <p>TR238</p>  <p>No Horses and Riders</p>	<p>R239</p>  <p>Ref. V1 2.4.21 V4 2.4.40</p> <p>TR239</p>  <p>Width Limit</p>	<p>R240</p>  <p>Ref. V1 2.4.21 V4 2.4.41</p> <p>TR240</p>  <p>No Towed Vehicles</p>
<p>R241</p>  <p>Ref. V1 2.4.21 V4 2.4.42</p> <p>TR241</p>  <p>No Hawkers</p>				

Section 2.5: RESERVATION – Conditional Regulatory Signs


NOTE: “Reservation” has been omitted from all sign names due to space constraints

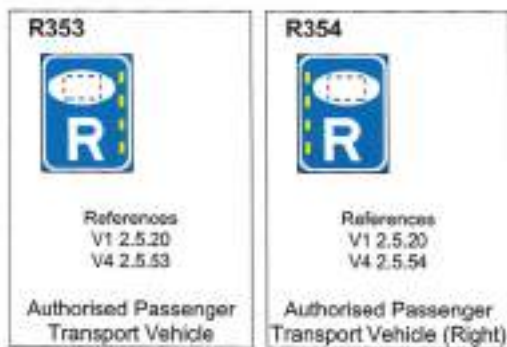
● = See Section 2.10 for variations appropriate to individual countries.

<p>R301 TR301</p>   <p>References V1 2.5.1 V4 2.5.1</p> <p>Bus</p>	<p>R301-P TR301-P</p>   <p>References V1 2.5.1 V4 2.5.1</p> <p>Bus Parking</p>	<p>R302 TR302</p>   <p>References V1 2.5.2 V4 2.5.2</p> <p>Bus Lane</p>	<p>R303 TR303</p>   <p>References V1 2.5.2 V4 2.5.3</p> <p>Begin Bus Lane</p>	<p>R304 TR304</p>   <p>References V1 2.5.3 V4 2.5.4</p> <p>Bicycle Lane</p>
<p>R304-P TR304-P</p>   <p>References V1 2.5.3 V4 2.5.4</p> <p>Bicycle Parking</p>	<p>R305-P TR305-P</p>   <p>References V1 2.5.4 V4 2.5.5</p> <p>Parking</p>	<p>R306-P TR306-P</p>   <p>●</p> <p>References V1 2.5.4 V4 2.5.6</p> <p>Limited Parking</p>	<p>R307 TR307</p>   <p>References V1 2.5.7 V4 2.5.7</p> <p>Motor Cycle</p>	<p>R307-P TR307-P</p>   <p>References V1 2.5.7 V4 2.5.7</p> <p>Motor Cycle Parking</p>
<p>R308 TR308</p>   <p>References V1 2.5.7 V4 2.5.8</p> <p>Motor Car</p>	<p>R308-P TR308-P</p>   <p>References V1 2.5.7 V4 2.5.8</p> <p>Motor Car Parking</p>	<p>R309 TR309</p>   <p>●</p> <p>References V1 2.5.13 V4 2.5.9</p> <p>Taxi</p>	<p>R309-P TR309-P</p>   <p>●</p> <p>References V1 2.5.13 V4 2.5.9</p> <p>Taxi Parking</p>	<p>R310 TR310</p>   <p>References V1 2.5.13 V4 2.5.10</p> <p>Minibus</p>
<p>R310-P TR310-P</p>   <p>References V1 2.5.13 V4 2.5.10</p> <p>Minibus Parking</p>	<p>R311 TR311</p>   <p>References V1 2.5.13 V4 2.5.11</p> <p>Midi-Bus</p>	<p>R311-P TR311-P</p>   <p>References V1 2.5.13 V4 2.5.11</p> <p>Midi-Bus Parking</p>	<p>R312 TR312</p>   <p>References V1 2.5.7 V4 2.5.12</p> <p>Delivery Vehicle</p>	<p>R312-P TR312-P</p>   <p>References V1 2.5.7 V4 2.5.12</p> <p>Delivery Vehicle Parking</p>
<p>R313 TR313</p>   <p>References V1 2.5.7 V4 2.5.13</p> <p>Goods Vehicle</p>	<p>R313-P TR313-P</p>   <p>References V1 2.5.7 V4 2.5.13</p> <p>Goods Vehicle Parking</p>	<p>R314 TR314</p>   <p>References V1 2.5.8 V4 2.5.14</p> <p>Goods Vehicle over Indicated G.V.M</p>	<p>R314-P TR314-P</p>   <p>References V1 2.5.8 V4 2.5.14</p> <p>Goods Vehicle over In- dicated G.V.M Parking</p>	<p>R315 TR315</p>   <p>References V1 2.5.8 V4 2.5.15</p> <p>Construction Vehicle</p>

● = See Section 2.10 for variations appropriate to individual countries.

<p>R315-P TR315-P</p> <p>References V1 2.5.8 V4 2.5.15</p> <p>Construction Vehicle Parking</p>	<p>R316 TR316</p> <p>References V1 2.5.8 V4 2.5.16</p> <p>Vehicle Dangerous Goods</p>	<p>R316-P TR316-P</p> <p>References V1 2.5.8 V4 2.5.16</p> <p>Vehicle Dangerous Goods Parking</p>	<p>R317 TR317</p> <p>References V1 2.5.8 V4 2.5.17</p> <p>Abnormal Vehicle</p>	<p>R317-P TR317-P</p> <p>References V1 2.5.8 V4 2.5.17</p> <p>Abnormal Vehicle Parking</p>
<p>R318 TR318</p> <p>References V1 2.5.9 V4 2.5.18</p> <p>Rickshaw</p>	<p>R318-P TR318-P</p> <p>References V1 2.5.9 V4 2.5.18</p> <p>Rickshaw Parking</p>	<p>R319 TR319</p> <p>References V1 2.5.13 V4 2.5.19</p> <p>Tour Bus</p>	<p>R319-P TR319-P</p> <p>References V1 2.5.13 V4 2.5.19</p> <p>Tour Bus Parking</p>	<p>R320 TR320</p> <p>References V1 2.5.14 V4 2.5.20</p> <p>High Occupancy Vehicle</p>
<p>R320-P TR320-P</p> <p>References V1 2.5.14 V4 2.5.20</p> <p>High Occupancy Vehicle Parking</p>	<p>R321 TR321</p> <p>References V1 2.5.9 V4 2.5.21</p> <p>Emergency Vehicle</p>	<p>R321-P TR321-P</p> <p>References V1 2.5.9 V4 2.5.21</p> <p>Emergency Vehicle Parking</p>	<p>R322 TR322</p> <p>References V1 2.5.9 V4 2.5.22</p> <p>Police Vehicle</p>	<p>R322-P TR322-P</p> <p>References V1 2.5.9 V4 2.5.22</p> <p>Police Vehicle Parking</p>
<p>R323 TR323</p> <p>References V1 2.5.9 V4 2.5.23</p> <p>Disabled Persons Vehicle</p>	<p>R323-P TR323-P</p> <p>References V1 2.5.9 V4 2.5.23</p> <p>Disabled Persons Vehicle Parking</p>	<p>R324 TR324</p> <p>References V1 2.5.10 V4 2.5.24</p> <p>Authorized Vehicle</p>	<p>R324-P TR324-P</p> <p>References V1 2.5.10 V4 2.5.24</p> <p>Authorized Vehicle Parking</p>	<p>R325</p> <p>References V1 2.5.21 V4 2.5.25</p> <p>Bus Stop</p>
<p>R326 ●</p> <p>References V1 2.5.21 V4 2.5.26</p> <p>Minibus Stop</p>	<p>R327 TR327</p> <p>References V1 2.5.14 V4 2.5.27</p> <p>Bus and Minibus</p>	<p>R327-P TR327-P</p> <p>References V1 2.5.14 V4 2.5.27</p> <p>Bus and Minibus Parking</p>	<p>R328 TR328</p> <p>References V1 2.5.14 V4 2.5.28</p> <p>Bus and Minibus Lane</p>	<p>R329 TR329</p> <p>References V1 2.5.14 V4 2.5.29</p> <p>Begin Bus and Minibus Lane</p>

<p>R330 TR330</p>  <p>References V1 2.5.15 V4 2.5.30</p> <p>Bus and Midi-bus</p>	<p>R330-P TR330-P</p>  <p>References V1 2.5.15 V4 2.5.30</p> <p>Bus and Midi-bus Parking</p>	<p>R331 TR331</p>  <p>References V1 2.5.15 V4 2.5.31</p> <p>Bus and Midi-bus Lane</p>	<p>R332 TR332</p>  <p>References V1 2.5.15 V4 2.5.32</p> <p>Begin Bus and Midi-bus Lane</p>	<p>R333 TR333</p>  <p>References V1 2.5.15 V4 2.5.33</p> <p>Bus, Midi-bus and Minibus</p>
<p>R333-P TR333-P</p>  <p>References V1 2.5.15 V4 2.5.33</p> <p>Bus, Midi-bus and Minibus Parking</p>	<p>R334 TR334</p>  <p>References V1 2.5.16 V4 2.5.34</p> <p>Bus, Midi-bus and Minibus Lane Right</p>	<p>R335 TR335</p>  <p>References V1 2.5.16 V4 2.5.35</p> <p>Begin Bus, Midi-bus and Minibus Lane</p>	<p>R336 TR336</p>  <p>References V1 2.5.16 V4 2.5.36</p> <p>High Occupancy Vehicle Lane</p>	<p>R337 TR337</p>  <p>References V1 2.5.16 V4 2.5.37</p> <p>Begin High Occu- pancy Vehicle Lane</p>
<p>R338</p>  <p>References V1 2.5.17 V4 2.5.38</p> <p>Tram</p>	<p>R339</p>  <p>References V1 2.5.17 V4 1.5.39</p> <p>Tram Lane</p>	<p>R340</p>  <p>References V1 2.5.17 V4 2.5.40</p> <p>Begin Tram Lane</p>	<p>R341</p>  <p>Ref. V1 2.5.21 V4 2.5.41</p> <p>Tram Stop</p>	<p>R342</p>  <p>References V1 2.5.17 V4 2.5.42</p> <p>Bus and Tram</p>
<p>R343</p>  <p>References V1 2.5.18 V4 2.5.43</p> <p>Bus and Tram Lane</p>	<p>R344</p>  <p>References V1 2.5.18 V4 2.5.44</p> <p>Begin Bus and Tram Lane</p>	<p>R345</p>  <p>Ref. V1 2.5.18 V4 2.5.45</p> <p>Bus, Minibus and Tram</p>	<p>R346</p>  <p>Ref. V1 2.5.18 V4 2.5.46</p> <p>Bus, Minibus and Tram Lane</p>	<p>R347</p>  <p>Ref. V1 2.5.19 V4 2.5.47</p> <p>Begin Bus, Minibus and Tram Lane</p>
<p>R348 TR348</p>  <p>References V1 2.5.19 V4 2.5.48</p> <p>Bus Lane - Right</p>	<p>R349 TR349</p>  <p>References V1 2.5.19 V4 2.5.49</p> <p>Bus and Minibus Lane - Right</p>	<p>R350</p>  <p>References V1 2.5.19 V4 2.5.50</p> <p>Bus and Tram Lane - Right</p>	<p>R351</p>  <p>Ref. V1 2.5.20 V4 2.5.51</p> <p>Bus, Minibus and Tram Lane - Right</p>	<p>R352 TR352</p>  <p>References V1 2.5.20 V4 2.5.52</p> <p>High Occupancy Vehicle Lane - Right</p>



Section 2.6: COMPREHENSIVE – Conditional Regulatory Signs

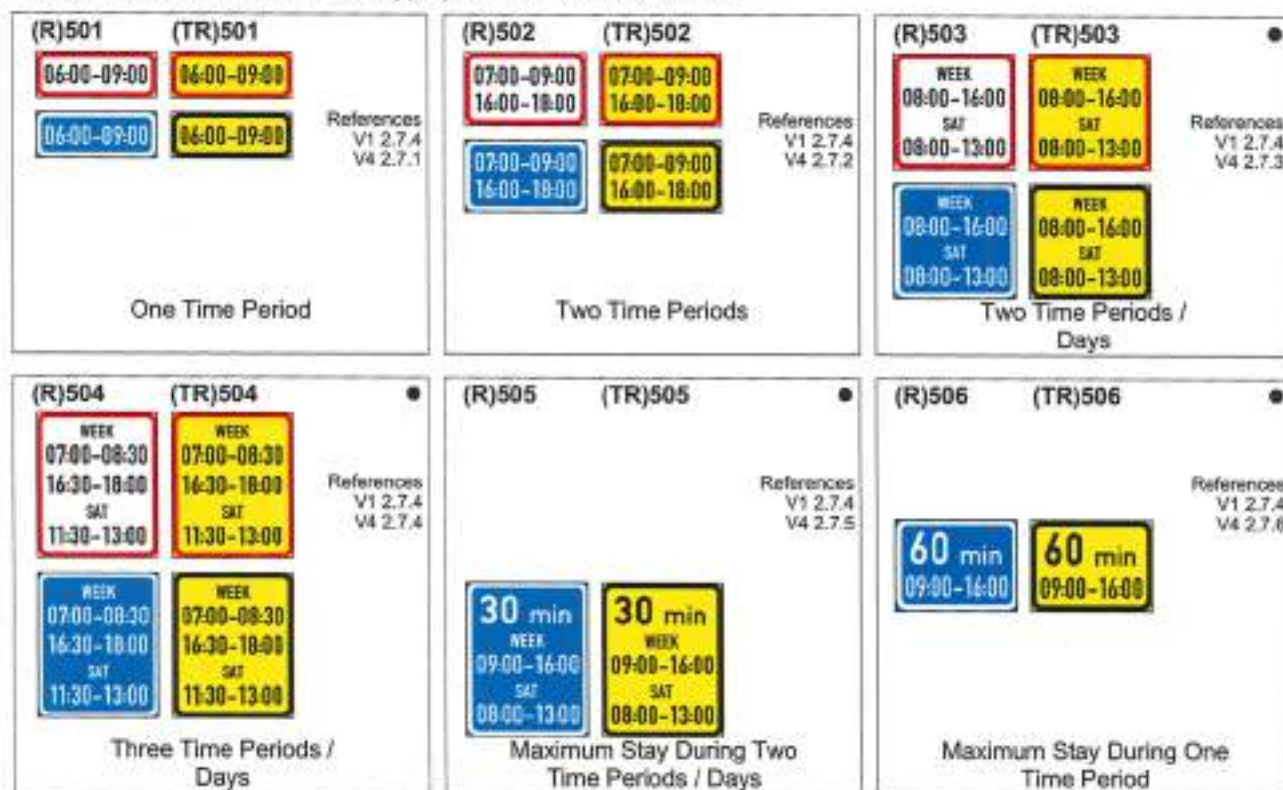


**Section 2.7: SELECTIVE RESTRICTION – Regulatory Signs
EXCLUSIVE SECONDARY MESSAGE – Regulatory Signs**

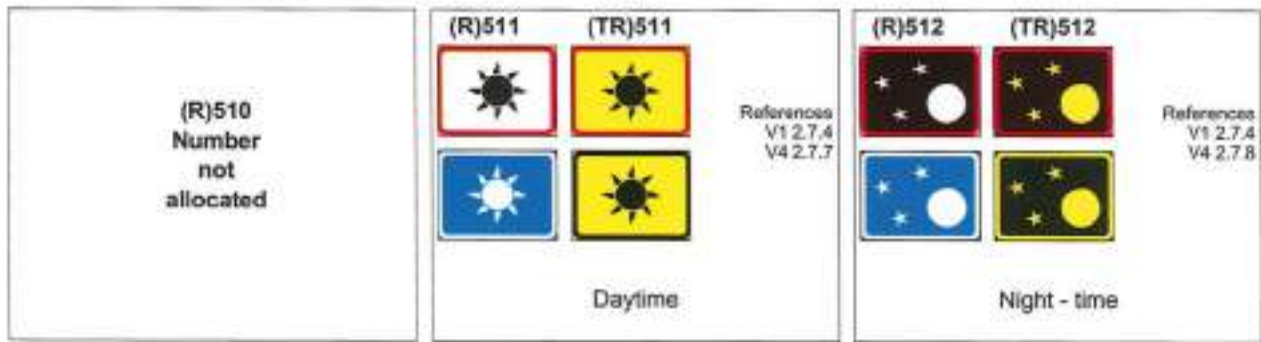
- NOTE:** (1) Exclusive Secondary Message signs shall ONLY be used with another REGULATORY sign.
(2) An Exclusive Secondary Message sign uses the same colours as that REGULATORY sign.

TIME LIMIT Sub - Group

● = See Section 2.10 for variations appropriate to individual countries.

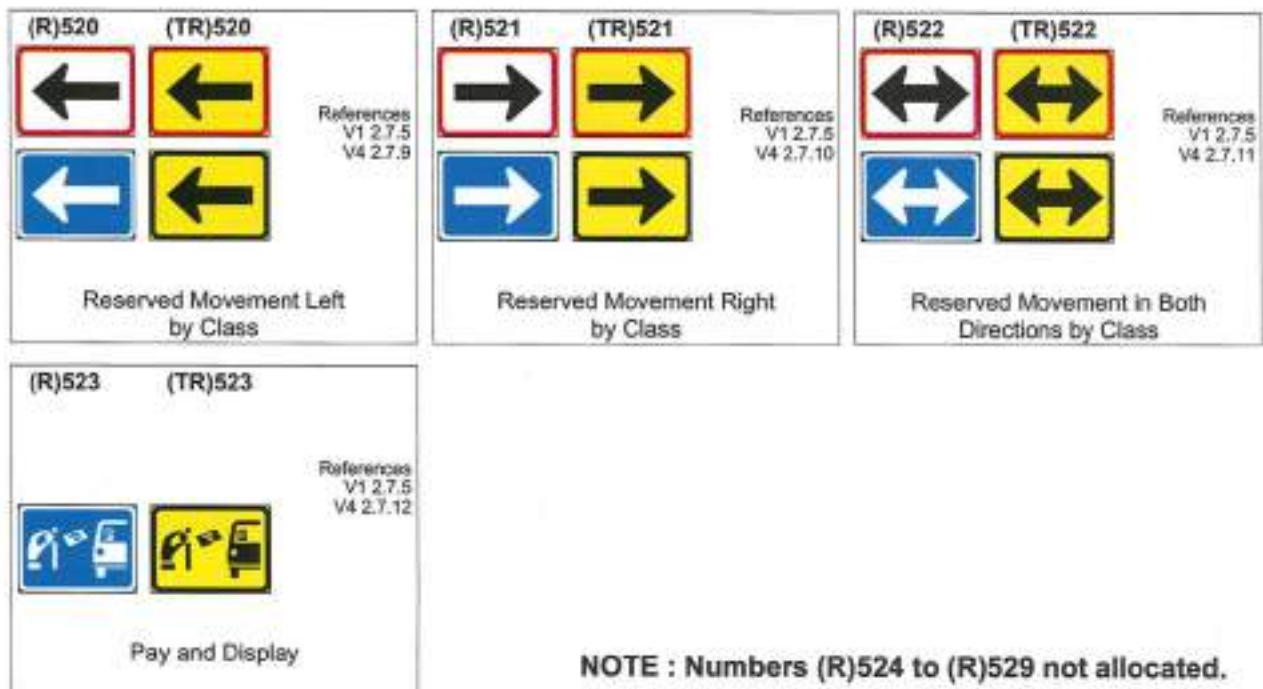


NOTE : Numbers (R)507 to (R)510 not allocated.



NOTE : Numbers (R)513 to (R)519 not allocated.

ACTION Sub - Group



NOTE : Numbers (R)524 to (R)529 not allocated.

TEXT MESSAGE Sub – Group (Objects Category)


● = See Section 2.10 for variations appropriate to individual countries.

<p>(R)530 (TR)530</p> <p>References V1 2.7.6 V4 2.7.13</p> <p>Public Transport Operator (Logo)</p>	<p>(R)531 (TR)531</p> <p>References V1 2.7.6 V4 2.7.14</p> <p>Public Transport Operator (Text)</p>	<p>(R)532 (TR)532</p> <p>References V1 2.7.6 V4 2.7.15</p> <p>Mass Limit - Text</p>
<p>(R)533 (TR)533 ●</p> <p>References V1 2.7.6 V4 2.7.16</p> <p>Motor Cycle Engine Size Limit</p>	<p>(R)534 (TR)534 ●</p> <p>References V1 2.7.6 V4 2.7.17</p> <p>"And Local Access Only" Limit</p>	<p>(R)535 (TR)535 ●</p> <p>References V1 2.7.6 V4 2.7.18</p> <p>Distance "For" Limit</p>
<p>(R)540 (TR)540 ●</p> <p>References V1 2.7.6 V4 2.7.19</p> <p>Maximum Number of Vehicles Limit</p>	<p>NOTE : Numbers (R)536 to (R)539 not allocated.</p>	










NOTE : Numbers (R)541 to (R)559 not allocated.

SYMBOL MESSAGE Sub – Group (Objects Category)

● = See Section 2.10 for variations appropriate to individual countries.

<p>(R)560 (TR)560</p>  <p>References V1 2.7.7 V4 2.7.20</p> <p>Bus</p>	<p>(R)561 (TR)561</p>  <p>References V1 2.7.7 V4 2.7.21</p> <p>Bicycle</p>	<p>(R)562 (TR)560</p>  <p>References V1 2.7.7 V4 2.7.22</p> <p>Motor Cycle</p>
<p>(R)563 (TR)563</p>  <p>References V1 2.7.7 V4 2.7.23</p> <p>Motor Car</p>	<p>(R)564 (TR)564 ●</p>  <p>References V1 2.7.7 V4 2.7.24</p> <p>Taxi</p>	<p>(R)565 (TR)565</p>  <p>References V1 2.7.7 V4 2.7.25</p> <p>Minibus</p>
<p>(R)566 (TR)566</p>  <p>References V1 2.7.7 V4 2.7.26</p> <p>Midi - bus</p>	<p>(R)567 (TR)567</p>  <p>References V1 2.7.7 V4 2.7.27</p> <p>Delivery Vehicle</p>	<p>(R)568 (TR)568</p>  <p>References V1 2.7.7 V4 2.7.28</p> <p>Goods Vehicle</p>
<p>(R)569 (TR)569</p>  <p>References V1 2.7.7 V4 2.7.29</p> <p>Goods Vehicle Over Indicated GVM</p>	<p>(R)570 (TR)570</p>  <p>References V1 2.7.7 V4 2.7.30</p> <p>Construction Vehicle</p>	<p>(R)571 (TR)571</p>  <p>References V1 2.7.7 V4 2.5.31</p> <p>Vehicle Conveying Dangerous Goods</p>
<p>(R)572 (TR)572 ●</p>  <p>References V1 2.7.7 V4 2.7.32</p> <p>Abnormal Vehicle</p>	<p>(R)573 (TR)573</p>  <p>References V1 2.7.7 V4 2.7.33</p> <p>Rickshaw</p>	<p>(R)574 (TR)574 ●</p>  <p>References V1 2.7.7 V4 2.7.34</p> <p>Tour Bus</p>

● = See Section 2.10 for variations appropriate to individual countries.

<p>(R)575 (TR)575</p>  <p>References V1 2.7.8 V4 2.7.35</p> <p>High Occupancy Vehicle</p>	<p>(R)576 (TR)576</p>  <p>References V1 2.7.8 V4 2.5.36</p> <p>Emergency Vehicle</p>	<p>(R)577 (TR)577 ●</p>  <p>References V1 2.7.8 V4 2.5.37</p> <p>Police Vehicle</p>
<p>(R)578 (TR)578</p>  <p>References V1 2.7.8 V4 2.6.38</p> <p>Disabled Persons Vehicle</p>	<p>(R)579 (TR)579</p>  <p>References V1 2.7.8 V4 2.7.39</p> <p>Authorised Vehicle</p>	<p>(R)580 (TR)580</p>  <p>References V1 2.7.8 V4 2.7.40</p> <p>Agricultural Vehicle</p>
<p>(R)581 (TR)581</p>  <p>References V1 2.7.8 V4 2.7.41</p> <p>Animal - Drawn Vehicle</p>	<p>(R)582 (TR)582</p>  <p>References V1 2.7.8 V4 2.7.42</p> <p>Towed Vehicle</p>	<p>(R)583 (TR)583</p>  <p>References V1 2.7.8 V4 2.5.43</p> <p>Tram</p>

In principle SELECTIVE RESTRICTION signs may be created by combining any R1, R100, R200, or R300 series sign with any sign in the (R)500 series. In practice the vast majority of possible combinations are never likely to be used. Signs used are likely to fall into two groups – those used commonly, and those required rarely for specialised applications.

SELECTIVE RESTRICTION signs comprise two component parts. The upper component is a PRIMARY MESSAGE sign from one of the R1, R100, R200, or R300

series which is displayed with a lower component SECONDARY MESSAGE sign from the (R)500 series. The function of the secondary message is that it changes the applicability of the primary message. Primary and secondary sign components ALWAYS use the same colour code. (R)500 sign shall not be used on their own.

All MANDATORY, COMPREHENSIVE and SECONDARY MESSAGE signs can be categorised as representing a restriction in some way, as a LIMIT, as an ACTION, or as an OBJECT.

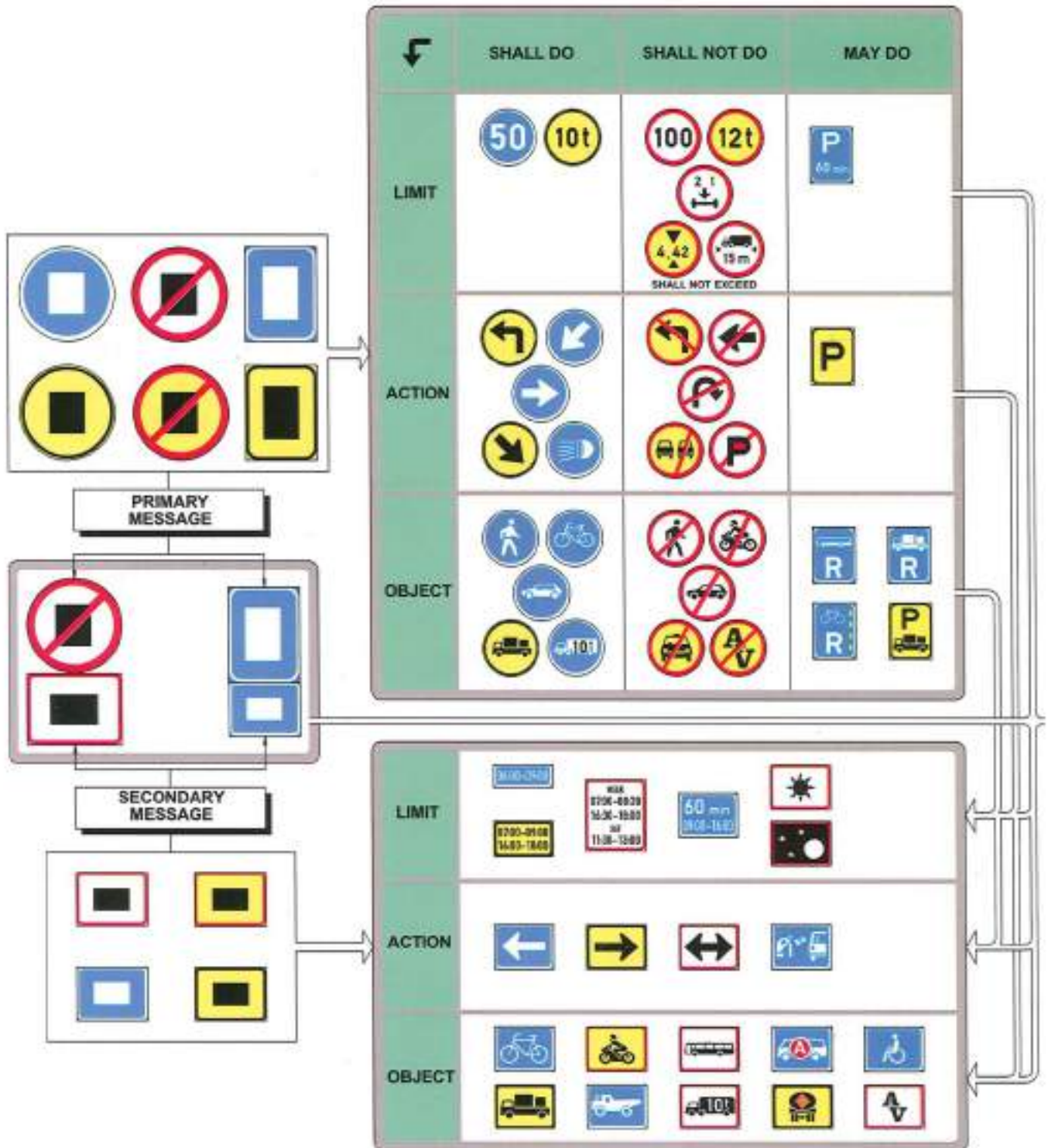


Fig 2.4 SELECTIVE RESTRICTION Regulatory Signs - SYSTEM and EXAMPLES



Section 2.8: COMBINATION – Regulatory Signs

Regulatory signs may be enhanced, without affecting their legal meaning, by combination with other road traffic signs such as:

- (a) SUPPLEMENTARY PLATE information sign IN11;

- (b) HIGH VISIBILITY background signs;

- (c) flashing yellow signals SS3; OR combinations of several of these

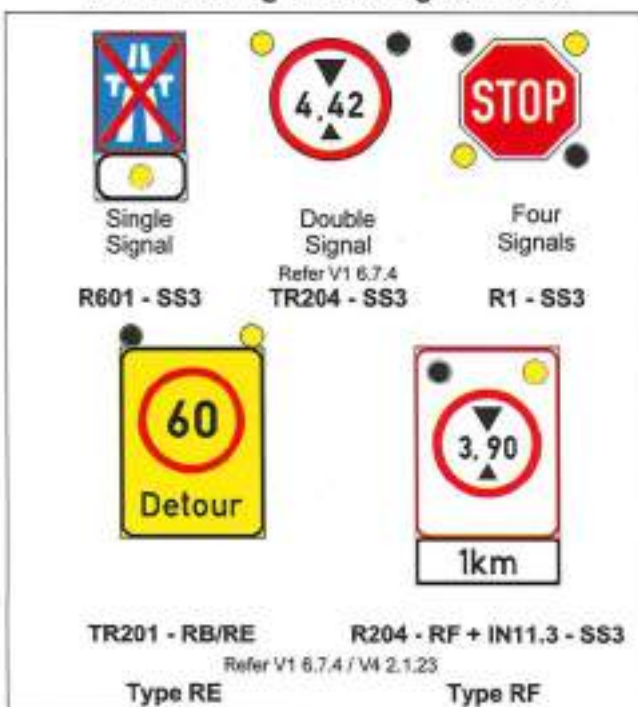
Regulatory Signs with Supplementary Plates



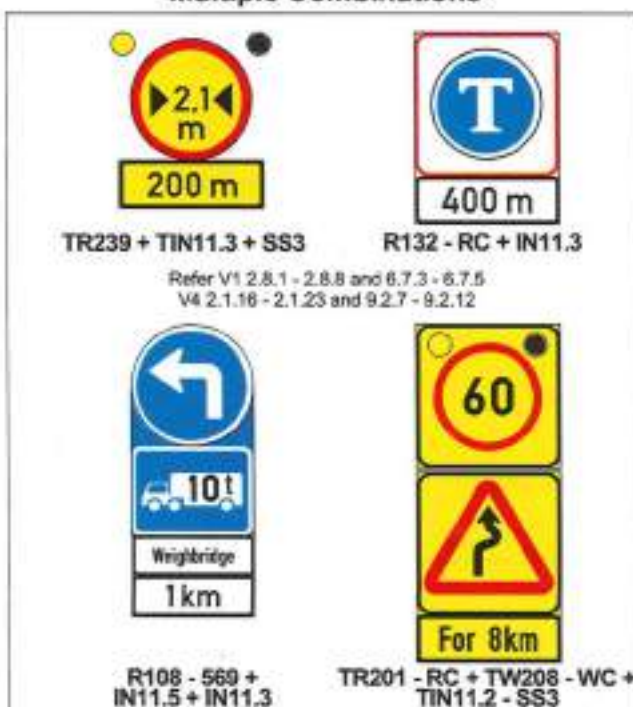
Regulatory Signs on High Visibility Backgrounds



with Flashing Yellow Signals - SS3



Multiple Combinations



Section 2.9: DE-RESTRICTION Signs

All MANDATORY and CONDITIONAL regulatory signs impose some form of restriction on some, or all, road users. A restriction, once applied, normally remains in force until it is changed. A restriction may be changed by removal, or DE-RESTRICTION, or by the application of another different restriction. DE-RESTRICTION is achieved by displaying the original sign with a RED CROSS (R)600 superimposed on the face of the sign.

This option may be used with almost any COMMAND, PROHIBITION, RESERVATION or COMPREHENSIVE sign but is most common with the latter. SPEED LIMIT restrictions shall only be changed by display of a new SPEED LIMIT sign. In general, the use of the (R)600 element with PROHIBITION signs using a diagonal slash, is not recommended.



TYPICAL EXAMPLES

(Signs in the R600 series are standard versions of DE-RESTRICTION sign)

<p>R132-600</p> <p>Ref. V1 2.9.1 V4 2.8.1</p> <p>End of Toll Road</p>	<p>R133-600</p> <p>Ref. V1 2.9.1 V4 2.8.2</p> <p>TR133-600</p> <p>End of Requirement to Switch Headlamps On</p>	<p>R202-600</p> <p>Ref. V1 2.9.1 V4 2.8.4</p> <p>TR202-600</p> <p>End of Mass Limit</p>	<p>R304-600</p> <p>TR304-600</p> <p>References V1 2.9.1 V4 2.8.5</p> <p>End of Bicycle Lane</p>
<p>R401-600</p> <p>TR401-600</p> <p>References V1 2.9.1 V4 2.8.7</p> <p>End of Dual Carriageway Freeway</p>	<p>R402-600</p> <p>TR402-600</p> <p>References V1 2.9.1 V4 2.8.8</p> <p>End of Single Carriageway Freeway</p>	<p>R403-600</p> <p>References V1 2.9.1 V4 2.8.9</p> <p>End of Woonef</p>	

CHAPTER 2: REGULATORY SIGNS

2.1 INTRODUCTION

2.1.1 General

- 1 Regulatory signs are used to control the actions of road users in the sense that road users shall take, or not take, specific actions as indicated by such signs. Failure to obey regulatory signs shall be an offence in terms of various Acts, Ordinances, Regulations or By - laws as may be in force from time to time.
- 2 Regulatory signs may be used to indicate a general law or regulation applicable in the interests of safety, such as a SPEED LIMIT, or in the interests of good traffic order and efficient road use such as a traffic signal, or a STOP or YIELD sign. Alternatively a regulatory sign may be used to indicate a change in the general level of control existing for road users to a different one. Examples of applications include NO OVERTAKING, BUS LANE RESERVATION or PARKING RESERVATION signs. Regulatory signs may therefore be required as a result of changes in road geometry or road network characteristics.
- 3 The signs covered by this Chapter comprise a systematic framework of regulatory signs which will make expansion to cater for future needs a more simple task (see Figure 2.1 and 2.2).
- 4 In order to assist the message transfer process, emphasis has been placed on the development of symbols which can communicate a visible, clear and comprehensible, legally significant regulatory message to those to whom the signs apply. This in turn should result in fair, and simplified, enforcement procedures.
- 5 In exercising control over road users within the road system, road authorities may need to regulate all road users or selected groups or classes of road user. The road authorities may also wish to regulate all or some road users in one of several different ways. It is therefore important that the chosen regulatory sign clearly indicates WHAT or WHO is regulated, and HOW it is regulated.
- 6 It is a basic philosophy of the regulatory signing system that whenever possible, signing should be POSITIVE, e.g. a regulatory sign should not have its critical primary message such as "STOP" or "NO ENTRY" modified so that it becomes applicable only to a portion of the approaching traffic.

In such a situation it is recommended that a sign be used which indicates that entry is available ONLY to (some class of vehicle or road user). This philosophy can also be applied to other signs and will be incorporated into the detail given in the descriptive sections on individual signs.

- 7 In this Chapter, on the pages dealing with specific regulatory signs each Subsection commences with a statement of the functional significance of the sign in terms of the relevant regulations. This statement will normally include the significance of the sign as given by the Road Traffic Regulations but may in some cases amplify this. **The significance given in the Road Traffic Regulations shall be the applicable significance in law.**

2.1.2 Types of Regulatory Signs

- 1 Regulatory signs may be PERMANENT or TEMPORARY. (See Chapter 10- "Glossary of Terms"- for further details of PERMANENT and TEMPORARY signs). In the case of regulatory signs care shall be taken when specifying TEMPORARY signs. It is a common feature of PERMANENT regulatory signs that they apply only for certain times of the day. Such signs are not considered as "temporary" but are considered to apply selectively (see paragraph 2.1.4.3 etc. and Section 2.7). It is also necessary in certain instances to display a regulatory message on a sign and then for some time remove the message completely from the driver's view. This type of simple variable message can be achieved by using a split sign, part of which can be turned so that the driver sees only the blank grey reverse surface of the sign (see Chapter 9). Such signs should be considered as TEMPORARY.
- 2 TEMPORARY regulatory signs may be used for short periods of time (a matter of minutes), or for relatively long periods of time running into months. The use of TEMPORARY regulatory signs, often in conjunction with other classes of temporary road traffic sign, indicates that for some reason circumstances on the roadway are not normal, or PERMANENT. Typical situations in which TEMPORARY regulatory signs may be expected to be used are:
 - (a) at road, building or other construction sites;
 - (b) at accident sites;
 - (c) for temporary traffic control by police or traffic officers, or by scholar patrols;
 - (d) for roadside traffic surveys.

PERMANENT or TEMPORARY versions of the same sign have the same legal significance. This significance shall not be affected by the inadvertent display of a PERMANENT regulatory sign when a TEMPORARY regulatory sign is appropriate, or vice versa.

- 3 Where conditions require greater visual impact or emphasis regulatory signs may be displayed within a HIGH VISIBILITY background (see Section 2.8).
- 4 In circumstances where varying conditions of topography, weather, or traffic control needs occur, it may be necessary to provide a VARIABLE MESSAGE regulatory sign which is capable of displaying a number of different temporary messages appropriate to the particular site.

2.1.3 Basic Classification of Regulatory Signs

- 1 Regulatory signs have been classified into groups. These are arranged into a framework to make the allocation of new signs to the correct group more simple. This framework takes into account the function of the different regulatory sign groups and their applicability. The framework uses a combination of shapes and colour codes to identify the groups in the classification. Specific colours are allocated to regulatory sign borders, backgrounds and symbols in order to give a sign group a particular significance.

- 2 The terms regulate, control, restrict and reserve have similar but subtly different meanings. For the purposes of the classification of regulatory signs each is accorded a different significance in order to clarify the stages of the classification. In this context to REGULATE is considered to mean to CONTROL, or to RESTRICT. A restriction may be applied in the form of a LIMIT, such as a speed limit, or it can be applied to an ACTION such as a turning movement, or to an OBJECT such as a heavy vehicle. A restriction may also be applied to a combination of limits, actions or objects.
- 3 The APPLICABILITY of a regulation may be MANDATORY in that drivers or other road users SHALL, or SHALL NOT, take some action as indicated by the sign. For example at a T-junction with a one-way street all drivers SHALL turn in the one direction indicated by a mandatory sign. A regulation may also be APPLICABLE in a CONDITIONAL manner, in which case a driver, or other road user, may choose to come under the jurisdiction of the conditional sign or not. If, for example, a driver approaches a one-way street on a crossing road the driver may go straight on or turn into the one-way street. If the driver chooses to enter the one-way street it SHALL only be in the direction indicated by the conditional sign.
- 4 The basic classification of permanent and temporary regulatory signs is summarised in Figure 2.1. This classification is expanded further into a comprehensive framework of sign applications in Figure 2.2 (see Section 2.0). This framework is described further in Sub - section 2.1.5.

2.1.4 Format of Regulatory Signs

- 1 Regulatory signs shall be easy to understand and to enforce, and difficult to misinterpret. The classification framework lends itself to clear distinction between the various groups within the framework. The limit, action or object which is the subject of the regulation is indicated by the legend on the sign and its applicability is given by the shape and colour of the sign groups.
- 2 In the majority of cases the legend used on regulatory signs is a symbolic representation of the limit, action or

object. Normally this means that limits are indicated by numerals and/or letters, actions by an arrow and objects by a symbol representing the object. The number of symbols available for use on regulatory signs has been significantly increased and, in general, those that have existed for some time have been modernised and stylised to improve legibility and recognition distances (see Figure 2.3).

- 3 It is a general rule that regulatory signs should only display one symbol per sign. In some instances in order to achieve the desired significance in relation to an application it may be necessary to display two symbol messages at one time. This should normally be provided by two signs mounted together (see paragraph 2.1.4.15). Such signs are termed SELECTIVE RESTRICTION signs and may involve such message combinations as the restriction of an action by a specific object, rather than all objects (traffic), or the application of a limit to a specific object, rather than all objects (traffic). Table 2.1 gives the likely range of message combinations which may be required. The first part of each message combination is the PRIMARY message and the relevant symbol shall appear in the uppermost sign and in its standard sign shape. The SECONDARY message and the relevant symbol shall appear in a rectangular (secondary) sign below the primary sign. **Unless qualified by a secondary message varying its applicability to certain times a regulatory sign shall be applicable day and night, 24 hours every day.**
- 4 Symbols for use on primary message regulatory signs are recorded in the Road Traffic Regulations and only such symbols shall be used on primary message regulatory signs (see Figure 2.3 and Sub - section 2.1.12).
- 5 The SHAPE of a regulatory sign is used to indicate the applicability of the sign group. As depicted in Figure 2.1 the general rule is that MANDATORY signs are CIRCULAR and CONDITIONAL signs are RECTANGULAR. **This shape code does not apply to secondary messages which form part of selective restriction signs.**

TABLE 2.1 SELECTIVE RESTRICTION MESSAGE COMBINATIONS TABLE 2.1

Primary - Secondary

Limit Limit
(e.g. Mass limit for a limited time)

Limit - Object
(e.g. Minimum speed limit for trucks)

Action - Limit
(e.g. No right turn at limited times)

Action - Object
(e.g. No right turn by buses)

Object - Limit
(e.g. No pedestrians at limited times)

NOTES:

- (1) Secondary messages are provided by EXCLUSIVE SECONDARY MESSAGE signs in the (R)500 series (see Section 2.4)
- (2) Signs in the (R)500 series are classified into TIME LIMIT, ACTION, TEXT or SYMBOL MESSAGE Sub-groups. Text messages may refer to limits, actions or objects.

- 6 Regulatory signs which would have the most serious consequences in safety terms, should they not be noticed or obeyed, have been identified for many years as deserving special consideration regarding their conspicuity and uniqueness. Such signs are the STOP sign R1 and the NO ENTRY sign R3. These signs have a **common function in that they apply regulations which exercise control over the right of way of traffic.** They have therefore been designated as CONTROL signs and form a major sub-division of regulatory signs. CONTROL signs may occur in the MANDATORY or CONDITIONAL applicability categories. **The majority have exclusive shapes and because of their importance PERMANENT and TEMPORARY CONTROL signs retain the same SHAPES and COLOURS.** Signs may apply to all traffic or to specific types of traffic. SELECTIVE CONTROL signs should be used with caution because of the possibly serious consequences of the misinterpretation of such signs.
- 7 The influence of sign shape on the regulatory sign classification is summarised in Figures 2.1 and 2.2.
- 8 The COLOURS of a regulatory sign are used in addition to the shape as a further indication of the applicability of the type of sign and to indicate whether the sign is permanent or temporary.
- 9 A MANDATORY regulation may take the form of a COMMAND or a PROHIBITION. The function of a mandatory command restriction sign is to indicate that the limit or action displayed SHALL be complied with:
- (a) by drivers of all objects (traffic), e.g. KEEP LEFT sign R103;
 - (b) by drivers of a specific object displayed, e.g. BUSES ONLY sign R121.
- The function of a mandatory prohibition restriction sign is to indicate that the limit or action displayed SHALL NOT be exceeded or taken:
- (a) by drivers of all objects (traffic), e.g. SPEED LIMIT sign R201;
 - (b) by drivers of a specific object displayed, e.g. NO MOTORCYCLES sign R222.
- The difference between the different categories of mandatory restriction signs is indicated by the sign colours as given in Table 2.2 and in Figure 2.2.
- 10 **In addition to this colour coding mandatory prohibition restriction signs which refer to ACTIONS or OBJECTS have a diagonal RED SLASH.**

This red slash is located to have the least effect in obscuring the sign symbol and may slope from lower left to upper right or lower right to upper left of a sign. **Mandatory prohibition restriction signs referring to maximum permissible values or LIMITS, SHALL NOT use a red slash.**

- 11 A CONDITIONAL regulation may be a RESERVATION or the COMPREHENSIVE application of several regulations. The function of a conditional reservation restriction sign is to indicate to road users that a roadway, a portion of roadway, or some other road traffic facility such as a parking area, is reserved in terms of some limit, action or object, or combination of these. This category of sign is commonly used to apply to only drivers of one class of vehicle. However, since use of the roadway, portion of roadway, or other road traffic facility such as a parking area, is OPTIONAL, the reservation only becomes applicable to the user when the facility is in use. **In terms of the reservation the driver of any other object (all other traffic or classes of vehicle) shall not use the area so reserved.** This can be summarised as - **if the driver of an object (displayed by symbol) wishes to use the reserved area, ONLY the driver of such object (displayed by symbol) MAY use it, and NO driver of any OTHER class of object shall use it.** In the case of conditional reservation parking signs **if the driver of an object wishes to park and parking places are reserved for such object by an appropriate reservation sign, the driver of the object SHALL use the reserved places and no other, and NO driver of any OTHER class of object shall use the reserved places.** The function of a conditional comprehensive restriction sign is to indicate to road users that the use of a roadway, a portion of roadway, or some other road traffic facility, is subject to compliance with a number of road traffic regulations. The difference between different categories of conditional restriction signs is indicated by the sign colours as given in Table 2.3 and Figure 2.2.
- 12 Reservation signs may apply to the drivers of moving vehicles, parked vehicles, or, in the case of a range of public transport vehicle types, to places where drivers of such vehicles are permitted to stop. To indicate these differences signs relating to reservations involving moving vehicles include the letter "R" and those relating to parking include the letter "P". The signs may also include a symbol to indicate the class of vehicle to which the RESERVATION or PARKING RESERVATION applies. On the signs for drivers of moving vehicles the symbol is located above the letter "R" whereas on the signs for parked vehicles the symbol is located

TABLE 2.2	MANDATORY SIGNS COLOUR CODE			TABLE 2.2	
	Control Group	Command Group		Prohibition Group	
	All	Permanent	Temporary	Permanent	Temporary
Border	White	White	Yellow/Black	Red	Red
Background	Red	Blue	Yellow	White	Yellow
Symbol	White	White	Black	Black	Black
Examples	STOP sign	MINIMUM SPEED sign		MASS LIMIT sign	

below the letter "P". Public Transport STOP RESERVATION signs have a unique symbol and signface layout.

- 13 As mentioned in paragraph 2.1.4.3 it may sometimes be necessary to use SELECTIVE RESTRICTION signs in order to indicate when a primary sign message is only applied selectively to some limit, action or object. The colour code applicable to the rectangular secondary message signs is that of the primary message sign which it qualifies. Both signs in the selective restriction combination therefore have the same border, back-ground and symbol colours. SELECTIVE RESTRICTION RESERVATION signs will commonly indicate the time(s) of applicability of the signs as the secondary message (see Figure 2.4 and Section 2.7).
- 14 The colour of the reverse side of regulatory signs shall be grey semi-matt or natural aluminium finish, with the following exceptions:
 - (a) temporary portable signs (see Subsection 2.1.9);
 - (b) (b) STOP sign R1, the reverse side of which shall have a white semi-matt finish.
- 15 When legibility at high speed is not a basic requirement of selective restriction signs two or three symbols may be included within the sign face of a single sign. Examples of such signs are:
 - (a) a mandatory command restriction sign indicating a shared cycle-track/footpath;
 - (b) a wide range of conditional reservation restriction signs relating to public transport vehicles.
 (See also Section 2.8).
- 16 Several regulatory signs incorporate a time or times into their message. These time limits are fundamental to the functional operation of such signs in that they indicate the period of validity of the regulation related to the sign. **The sign is in effect inoperative outside the times indicated.** Due to restricted space or legibility requirements the time limit(s) is displayed as a secondary sign below the primary sign. These signs are selective restriction signs and the secondary, or sometimes tertiary, message shall be indicated in the same colour code as the primary message. **A secondary sign of this type is not a supplementary plate sign (see Figures 2.2 and 2.3).**
- 17 It is recommended that selective restriction signs be manufactured from one piece of material to avoid the risk of misinterpretation should one part of a two or more part sign fall off or be removed.

2.1.5 Framework for Regulatory Signs

- 1 It is essential to the understanding of the regulatory signing system and of its sub-division into groups, and the applicability of these groups of signs, that simple, easy to understand terms be used to describe them.
- 2 Regulatory signs are therefore classified as follows:
 - (a) CONTROL group (R1 to R6).
These signs indicate the applicability of right of way control regulations by mandatory command prohibition restrictions, or by conditional reservation restriction and conditional comprehensive restriction signs.
 - (b) COMMAND group (R100 series).
These signs indicate the applicability of regulatory restrictions by mandatory command other than control regulations.
 - (c) PROHIBITION group (R200 series).
These signs indicate the applicability of regulatory restrictions by mandatory prohibition other than control regulations.
 - (d) RESERVATION group (R300 series).
These signs indicate the applicability of regulatory restrictions by reservation on the basis of conditional use, other than control regulations.
 - (e) COMPREHENSIVE group (R400 series).
These signs indicate the comprehensive applicability of several regulatory restrictions on the basis of conditional use, other than control regulations.
 - (f) EXCLUSIVE SECONDARY MESSAGE signs (R)500 series (Brackets are used to indicate that these exclusive secondary message signs shall not be displayed on their own.) These signs, when displayed with one of the primary signs covered in paragraphs 2.1.5.2 (a) to (e), indicate that the applicability of such primary signs is modified by the secondary sign. The sign combination is a SELECTIVE RESTRICTION sign which is numbered in two parts e.g. R201-565.
 - (g) DE-RESTRICTION signs (R600 series) are used to indicate the termination of a previously imposed regulation.

The short name used to describe the group as indicated above in capital letters is used for the rest of this manual.

- 3 This classification framework is summarised in Figure 2.2. The figure also illustrates the basic regulatory sign SHAPE and COLOUR code. The various individual regu-

TABLE 2.3	CONDITIONAL SIGNS COLOUR CODE				TABLE 2.3
	Control Group	Reservation Group		Comprehensive Group	
	All	Permanent	Temporary	Permanent	Temporary
Border	White	White	Black	Red	Red
Background	Red	Blue	Yellow	Blue	Yellow
Symbol	White	White	Black	White	Black
Examples	ONE WAY ROADWAY sign	BUS LANE RESERVATION sign		DUAL CARRIAGEWAY FREEWAY BEGINS sign	

latory signs are covered in detail in subsequent sections. Examples of the various categories of sign are illustrated in Figure 2.5. These examples have been related to regulatory signing for the control of heavy vehicles since this is an area of application in which signs of the type illustrated are commonly called for.

2.1.6 Retroreflectivity and Illumination

- 1 PERMANENT control, command, prohibition and comprehensive regulatory signs shall have fully retroreflective borders, backgrounds and symbols when the significance of the sign applies during daytime and night-time. Black areas used on PERMANENT regulatory signs shall have a semi-matt finish. PERMANENT prohibition and reservation signs are commonly qualified by a time of day for which their applicability is valid. The use of retroreflective materials for border and symbols is recommended to cover the possible applicability of such signs during dusk. If such signs have no night-time significance semi-matt finishes may be specified.
- 2 All TEMPORARY regulatory signs shall be fully retroreflective apart from black symbols or borders which shall have a semi-matt finish. If certain urban maintenance units are organised in such a manner that they operate only in daylight hours the temporary regulatory signs used or displayed by these units may have a semi-matt finish.
- 3 External or internal illumination may be specified as an alternative to the use of retroreflective materials.

2.1.7 Location

- 1 In general the lateral and vertical location of regulatory signs shall comply with the provisions of Chapter 1.
- 2 Regulatory signs should be located as close as possible to the point from which the relevant regulation is applicable. In many cases it is necessary, for purposes of fair and reasonable enforcement, that regulatory signs be repeated at regular intervals. Details will be given in the descriptive sections on individual signs.
- 3 Special care should be taken with the placing of NO STOPPING and NO PARKING signs in urban areas to adequately indicate to drivers the extent of "no stopping" and "no parking" zones. Details will again be covered in the descriptive sections on individual signs.
- 4 In order to successfully alert drivers to the existence of regulations ahead, which may necessitate actions on the part of drivers or even the re-planning of their routes, it may be necessary to locate regulatory signs in advance of the point of application the regulations. This may be necessary in such circumstances as:
 - (a) a roadway height, width, vehicle length or mass restriction ahead;
 - (b) restricted turn conditions ahead for all traffic or for a specific class of vehicle.

In such cases the regulatory sign should be combined with a supplementary distance plate (see Subsection 2.1.8 and Section 2.8).

2.1.8 Supplementary Plates

- 1 SUPPLEMENTARY PLATE signs IN11 (Chapter 5, Subsection 5.2.6) may occasionally be used with

regulatory signs to supplement the messages of the signs, by giving:

- (a) descriptive information by text or symbol :
 - e.g.- police, customs, toll;
 - remote location (Oiviershoek Pass from the N3);
 - tow-away zone, etc.
- (b) a distance:
 - e.g. - a distance **to** the point of application of the regulation;
 - a distance **for which** the regulation applies with the word "For".
- 2 The message/legend of a supplementary plate sign used with a regulatory sign should be such that if it were to be removed from the main sign the significance of the main sign would remain unaltered.
- 3 **Selective restriction sign secondary messages are not supplementary plate signs.** In certain cases a supplementary plate may be added to a selective restriction sign.
- 4 SUPPLEMENTARY PLATE signs IN11 may incorporate a symbol or a text legend.
- 5 Whenever possible a supplementary plate sign should be dimensioned to match the width of the regulatory sign with which it is to be used.
- 6 Supplementary plate signs used with PERMANENT regulatory signs shall have a **white** background, **black** border and **black** legend. Those used with TEMPORARY regulatory signs shall have a **yellow** background and **black** border and legend. The materials used shall be as given in Section 2.1.6. By contrast it should be noted that SELECTIVE RESTRICTION secondary message regulatory signs **ALWAYS have the same colour code as the primary message regulatory sign with which they are used** (see Figures 2.3 and 2.4 and Section 2.7).
- 7 The use of distance supplementary plate signs with TEMPORARY regulatory signs is recommended to indicate to drivers the extent of a temporary regulation, particularly at roadworks sites e.g. "For 8 km".
- 8 The use of a distance supplementary plate sign with a regulatory sign to indicate the distance to the point of applicability of a regulatory sign e.g. "100 m" or "150 m", is not likely to be common. However, in certain instances such information may be necessary to aid smooth traffic flow and to reduce the risk of unnecessary or hazardous manoeuvres.

2.1.9 Portable Regulatory Signs

- 1 Portable regulatory signs are commonly used for temporary traffic control. Such signs should, with the exception of CONTROL group signs, be in TEMPORARY sign colours.
- 2 The reverse side of a portable regulatory sign which is used or displayed in such a manner that it may be a source of danger to traffic travelling in the opposite direction should be marked with 150 mm wide yellow retroreflective horizontal stripes on a black semi-matt background.

2.1.10 Size

- 1 Full dimensional details of all regulatory signs are given in Volume 4 of the Manual. Standard sizes covered in Volume 4 are:
 - (a) circular signs - diameter
 - 150 mm (for parking meters only)
 - 300 mm (for cycle tracks only)
 - 450mm
 - 600mm
 - 900mm
 - 1200 mm
 - 1600 mm
 - (b) rectangular signs - H x W (ratio 4:3)
 - 300 x 225 mm
 - 450 x 340 mm
 - 600 x 450 mm
 - 800 x 600 mm
 - 900 x 675 mm
 - 1200 x 900 mm
 - 1600 x 1200 mm
- 2 Except when indicated otherwise the size of regulatory signs should be as given in Table 2.4.
- 3 The dimensions in paragraph 2.1.10.1 (a) also apply to STOP signs. YIELD signs are dimensioned in the same manner as warning signs. The largest sizes given are normally intended for use on overhead signs on a HIGH VISIBILITY background.
- 4 BUS and MINIBUS STOP RESERVATION signs R325 and R326 have unique sizes which are covered in Section 2.6 and in Volume 4.

2.1.11 Applications

- 1 Various combinations of sign types which may include regulatory signs are covered in Section 2.8.
- 2 Examples of specific applications of regulatory signs in combination with other regulatory signs or other classes of sign will be covered in Volume 2 of the Manual particularly in Chapters 3, 7, 8, 11 and 13.

2.1.12 Other Regulatory Signs

- 1 When no specific symbol is available to cover a required regulatory sign primary message the appropriate message should be indicated within the sign in black semi-matt or white retroreflective letters of the DIN 1451 letter style, of the style and size permitted by the space available, but not less than 70 mm high. This provision does not apply to secondary message signs used on selective restriction signs or to BUS and MINIBUS STOP RESERVATION signs R325 and R326.
- 2 The need to manufacture any such sign shall be reported immediately by the authority to:

The Secretary
Route Numbering and Road Traffic Signs Sub-Committee
c/o Department of Transport
Private Bag X193
Pretoria
0001

This will enable a co-ordinated approach to be adopted for the design of an appropriate symbol for general use wherever possible.

TABLE 2.4 **MINIMUM REGULATORY SIGN SIZES** **TABLE 2.4**

	Operating speed (km/h)			Stopping/ Parking	Overhead Signs
	100 or more	70 to 90	60		
Circular sign diameter (mm)	1200	900	600	450	1600 ⁽¹⁾
Rectangular sign- x W (mm)	1200 x 900	900 x 675	600 x 450	450 x 340	1600 x 1200 ⁽¹⁾

NOTES:

- (1) When used on a HIGH VISIBILITY or DIAGRAMMATIC overhead sign a regulatory sign may be used with and without a distance or arrow. If the regulatory sign is a permanent COMMAND sign an additional contrasting semi-matt border shall be placed outside the normal white border of such signs. (See Volume 4, Chapter 2).
- (2) If a temporary COMMAND sign is located overhead the inset black border shall become the same width as the normal white border on a permanent COMMAND sign. (See Volume 4, Chapter 2).
- (3) Special reduced sizes are recommended for:
 - (a) cyclist and pedestrian control (300 mm diameter and 300 x 225 mm);
 - (b) KEEP LEFT signs R103 on traffic bollards (300 mm diameter);
 - (c) repeater signs on parking meters (150 mm diameter);
 - (d) special application STOP signs R1.1 (See Subsection 2.2.1 - 450 mm).
- (4) See Table 2.6 for details of PAY TOLL sign R132 sizes.

MANDATORY COMMAND



R102

All heavy vehicles exceeding 10 tonnes (GVM/GCM) only/must.



R123

All goods vehicles exceeding 3500 kg (GVM/GCM) only/must.



R124

All goods vehicles exceeding 10 tonnes (GVM/GCM) only/must.

MANDATORY PROHIBITION



R202

No heavy vehicles exceeding 10 tonnes (GVM/GCM).



R229

No goods vehicles exceeding 3500 kg (GVM/GCM).



R230

No goods vehicles exceeding 10 tonnes (GVM/GCM).

CONDITIONAL RESERVATION



R313

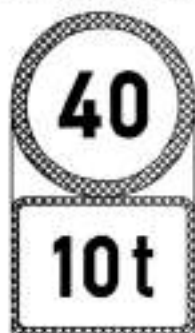
Reserved for all goods vehicles exceeding 3500 kg (GVM/GCM).



R314-P

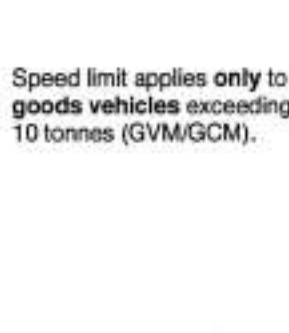
Parking reserved for goods vehicles exceeding 10 tonnes (GVM/GCM).

SELECTIVE RESTRICTION



R201-202

Speed limit applies only to all heavy vehicles exceeding 10 tonnes (GVM/GCM).



R201-314

Speed limit applies only to goods vehicles exceeding 10 tonnes (GVM/GCM).

NOTES:

- (1) Masses in tonnes are equivalent to the gross vehicle mass (GVM) or the gross combination mass (GCM).
- (2) "All heavy vehicles" over 10 tonnes includes buses over 10 tonnes.
- (3) "All goods vehicles" includes all trucks over 3 500 kg (GVM/GCM) but excludes buses.
- (4) "Goods vehicles exceeding 10 tonnes" includes all goods vehicles over 10 tonnes but excludes buses.,

Fig 2.5 Framework Applied to Regulatory Signing for Heavy Vehicles

COLOURS:

PERMANENT AND TEMPORARY

Border:	White retroreflective
Background:	Red retroreflective
Legend:	White retroreflective DIN 1451 Style 'W'

R1

For dimensions
ref. Vol. 4
page



2.2.1

2.2.1 Stop

1 The STOP regulatory sign R1 imposes a **mandatory requirement that the driver of a vehicle shall stop such vehicle with its front end in line with such sign, or, if STOP LINE marking RTM1 is used in conjunction with such sign in the prescribed manner, immediately behind such stop line, and that such driver shall not proceed until it is safe to do so.** When a STOP sign R1 is displayed at a railway crossing a **RAILWAY CROSSING hazard marker sign W403 or W404 shall be displayed below the R1 sign.** If a stop line has been marked but for whatever reason is, for the time being, not visible, then the driver shall act as if there is no stop line. It is recommended that the word STOP be marked on surfaced roadways immediately before the stop line (see Chapter 7).

2 A number of derivatives of STOP sign R1, are available. These are illustrated in Figure 2.6 and are described below:

(a) Sign R1.1 may be specified in urban areas where the footpath is narrow and visibility of the standard stop sign is restricted by other signs or by vegetation. The variation consists of an additional STOP sign R1, 450 mm wide, displayed on the same support as the main stop sign at a minimum height to the lower edge of 750 mm above the roadway. STOP sign R1.1 shall have the same mandatory requirements of a driver as sign R1;

(b) STOP/YIELD sign R1.2 imposes:

- (i) **a mandatory requirement that the driver of a vehicle wishing to proceed straight-on or to the right at the junction ahead shall act as for STOP sign R1;**
- (ii) **a mandatory requirement that the driver of a vehicle wishing to proceed to the left at the junction ahead shall yield right of way to all traffic, from the right (both vehicular and pedestrian), on the roadway which is joined by the roadway on which the driver is travelling where such traffic is so close as to constitute a danger or potential danger.**

The YIELD portion of sign R1.2, which includes a left turn arrow, shall not be used without a STOP sign R1, and shall not be used at a traffic signal. When STOP/YIELD sign R1.2-RSA is used the STOP LINE marking RTM1 shall be replaced on the path of the left turn movement by a YIELD

LINE marking RTM2.

- (c) 3 WAY-STOP sign R1.3 shall be used if all-way stop control is required on a three-leg road junction; a 3 WAY-STOP sign R1.3 shall have the same mandatory requirements of a driver as STOP sign R1; additional requirements are covered in paragraph 2.2.1.4;
- (d) 4 WAY-STOP sign R1.4 shall be used if all-way stop control is required on a four-leg road junction; a 4 WAY-STOP R1.4 sign shall have the same mandatory requirements of a driver as STOP sign R1; additional requirements are covered in paragraph 2.2.1.4;
- (e) STOP and GO signs R1.5A and R1.5B may be used for temporary traffic control at roadworks or other temporary public facility maintenance sites; display of sign R1.5A imposes:

(i) **a mandatory requirement that the driver of a vehicle shall stop such vehicle with its front end in line with the stop sign, and-**

(ii) **a mandatory requirement that the driver shall not proceed until permitted to do so by the display of the GO sign, and then with caution.**

Signs R1.5A and R1.5B shall be mounted back-to-back so that the legend STOP is displayed on one side and the legend GO on the other side. The signs may be mounted on a pedestal or staff to permit easy rotation. The colours of sign R1.5B have been altered to black on yellow from the white on green of the earlier sign to conform to the temporary sign colour code and to impart a message of "caution" consistent with the application of the sign.

(See paragraph 2.2.1.9 for details of stop signs used at police or customs check points and at toll-booths and paragraph 2.2.1.11 for guidelines and further details of application.)

3 Signs R1, R1.3 and R1.4 are not recommended for display when a YIELD sign R2 would control a road junction safely and with less inconvenience to the public. **Display of an R1 sign or any of its derivatives shall be given careful attention.**

4 When considering the use of all-way stop signs, R1.3 and R1.4 the effects such signs may have on traffic must be particularly carefully assessed. All-way stop signs may be useful as a safety measure in some

instances. Such control should ordinarily be used only when the volume of traffic on the intersecting roads is approximately equal. A traffic control signal is more satisfactory for a road junction carrying a heavy volume of traffic. In difficult cases it may become necessary to evaluate each option within the framework of traffic control alternatives on the basis of level of service, capacity, economic factors, environmental impact and collision history. Ultimately an engineering study may be necessary. This should involve a detailed examination of a wide range of factors, including:

- (a) sight distances;
- (b) drainage;
- (c) road widths;
- (d) channelisation and road markings;
- (e) road surface and edge condition;
- (f) parking;
- (g) approach gradients and road crossfall;
- (h) street lighting;
- (i) pedestrian movements;
- (j) advance signing.

It has been generally found that when all-way stop control is introduced at heavily trafficked or multi-lane road junctions, driver confusion may result and significant delays occur, thereby reducing the level of service. Frustration of driver's mobility on higher category roads due to the use of all-way stops, may result in a redistribution of traffic onto lower category roads. This in turn may create new traffic, environmental and accident problems remote from the routes and junction in question. Investigations aimed at producing a numerical warrant for all-way stop control have indicated that the range of traffic flows for which control might be warranted is so limited and the probability of these conditions being truly representative of the optimum performance range is so small, that a practical warrant does not exist. If, however, a traffic signal volume warrant indicates that traffic signals are justified then all-way stop control may be installed as an interim measure until signalisation can be implemented. This would include the situation where there is no existing electricity supply to the site. It should be noted that, in the case of this type of control, the need for manual control by traffic officer often still exists during peak periods. Although it is quite feasible to undertake an environmental impact assessment, the measurement and prediction of this impact is demanding.

- 5 There are potential environmental dis-benefits associated with all-way stop control, such as:
 - (a) increased noise;
 - (b) increased exhaust emissions;
 - (c) increased speeds on blocks between controls.

A significant drawback to using accident statistics to evaluate traffic control methods is the problem of getting reliable local historical data and the problem of making accurate predictions relating to future accident patterns. The issue is complicated by the phenomenon of "accident migration" which is difficult to detect, with-out very extensive before-and-after studies, and even more difficult to predict. This can be reflected in an apparent reduction in accidents, as measured at a particular site, which may or may not be related to a coincidental increase in accidents at an adjacent site. Experience has shown however that the installation of all-way stop control can result in a significant reduction

in the severity of accidents. The foregoing points are summarised in the form of installation guidelines in paragraph 2.2.1.11(c).

- 6 All-way stop control should not be applied at road junctions when one or more of the following conditions pertain to the junction:
 - (a) the road is a trunk road or major arterial road;
 - (b) the junction has more than four approach legs;
 - (c) the approach roads have a speed limit of 80 km/h or more or that the 85-percentile speed of traffic exceeds 85 km/h;
 - (d) the junction is on a public passenger transport route;
 - (e) where pedestrian movements on an average day exceed 200 persons in any one hour across any single approach road;
 - (f) the junction lies on a route between junctions controlled by co-ordinated traffic signals.

In addition the installation of all-way stop control is not recommended when one or more of the following conditions exist:

- (a) if the approach roads are divided carriageway roads;
 - (b) if any approach road to the junction has more than one lane for traffic;
 - (c) when the gradient of any approach road at the stop line exceeds 8%;
 - (d) at skew or offset junctions;
 - (e) the junction is within 150 m of any other traffic control device;
 - (f) where excessive traffic speeds are experienced and are a problem;
 - (g) where adjacent property would suffer dis-benefit due to increased vehicle noise and/or exhaust emissions;
 - (h) if the traffic flow on one road (both approaches) exceeds the total traffic flow on the other road (both approaches) by more than 20% of the total traffic through the junction.
- 7 Since vehicles on all approaches to a road junction controlled by all-way stop signs are required to stop, the informal "first in-first away" right of way rule has been formalised to allow for departure from the stop line by all vehicles in an orderly manner. It is therefore **a mandatory requirement that the driver of a vehicle approaching a 3 WAY STOP sign R1.3 or a 4 WAY STOP sign R1.4 shall stop such vehicle in accordance with the provisions of STOP sign R1, AND such driver shall not proceed into the junction until every vehicle which has stopped at any other STOP LINE marking RTM1 at such junction first, and which would, in the normal course of events, cross the path of the driver's vehicle, has cleared the junction.** This has the effect that the vehicle may be made to proceed past the STOP sign before such other vehicle(s) if the driver is certain that the path of such other vehicle(s) shall not cross or otherwise conflict with the path of the driver's vehicle during the time the driver requires to clear the path of such other vehicle(s). This will assist with the legal process of apportioning blame in the event of an accident within the junction.
 - 8 Sign R1 may be displayed by a temporary scholar patrol. Such signs should be 450 mm wide and should

COLOURS:
 PERMANENT AND TEMPORARY
 Border: White retroreflective
 Background: Red retroreflective
 Legend: White retroreflective
 DIN 1451 Style "A"

STOP SIGN

R1.1



STOP/YIELD

R1.2



For dimensions
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 2.2.1
 2.2.2

COLOURS:
 PERMANENT AND TEMPORARY
 Border: White retroreflective
 Background: Red retroreflective
 Legend: White retroreflective
 DIN 1451 Style "A"

3-WAY STOP

R1.3



4-WAY STOP

R1.4



For dimensions
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 2.2.3

STOP SIGN

R1.5A



COLOURS:
 TEMPORARY

Border: White retroreflective
 Background: Red retroreflective
 Legend: White retroreflective
 DIN 1451 Style "A"

COLOURS:
 TEMPORARY

Border: Black semi-matt
 Background: Yellow retroreflective
 Legend: Black semi-matt
 DIN 1451 Style "A"

GO SIGN

R1.5B



For dimensions
 ref. Vol. 4
 page
 2.2.4

Fig 2.6

Stop Sign Derivatives

TABLE 2.5 **MINIMUM STOPPING SIGHT DISTANCES** **TABLE 2.5**

Effective Approach Speed (km/h)	Minimum Sight Distance Required (m)
50	70
60	95
70	125
80	150
85	165

be manufactured from a light-weight material and in a back-to-back or double sided manner, when used in two-way streets, so that sign R1 is displayed in both directions by each scholar operating a patrol. A scholar patrol shall include a scholar operating R1 signs on each side of the roadway. Temporary scholar patrol signs do not qualify as all-way stop control signs although both approaching streams of traffic will commonly be stopped at the same time. A STOP regulatory sign R1 displayed by a member of a scholar patrol imposes **a mandatory requirement that the driver of a vehicle stop such vehicle in front of such sign or immediately behind a YIELD LINE marking RTM2 in conjunction with which the sign is used, and remain stationary until the sign is no longer displayed.**

- 9 Where vehicles are to stop for the requirements of police officers, traffic officers, customs officials or at toll plazas relevant SUPPLEMENTARY PLATE signs IN11.577 indicating the POLICE symbol, CUSTOMS, or TOLL may be displayed below the STOP sign R1. The requirements of such signs shall be as for a STOP sign R1 without the SUPPLEMENTARY PLATE sign IN11 except that *it shall be a mandatory requirement that the driver shall not proceed until directed to do so by a police officer, traffic officer, or customs or toll official.* The colours of such SUPPLEMENTARY PLATE signs should conform to the permanent and temporary colour code for information signs and a symbol, when used, should conform to the colours appropriate to that symbol (see Section 2.8 and Chapter 5).
- 10 Where advance visibility of a stop sign R1 or any of its derivatives is inadequate and stop control may be unexpected, the use of permanent or temporary TRAFFIC CONTROL "STOP" AHEAD warning signs W302 or TW302, as appropriate, is recommended in accordance with the provisions of Subsection 3.4.2. The use of a distance SUPPLEMENTARY PLATE sign IN11.3 with the warning sign is also recommended. The distance displayed should take into account the stopping distance requirements of approaching vehicles and the likely existence of traffic queues at the stop sign. Minimum stopping sight distances are given in Table 2.5. An allowance of an additional 40% distance should be considered when the approaching road surface is gravel.

When the stop signs are all-way stop signs R1.3 or R1.4 and advance warning sign W302 or TW302 is required it is recommended that the distance information plate include the legend "4-WAY" or "3-WAY" as appropriate. Temporary SCHOLAR PATROL AHEAD warning sign TW305 shall be displayed in advance of an

R1 sign used at a temporary scholar patrol crossing and temporary "STOP-GO" CONTROL AHEAD warning sign TW343 shall be displayed in advance of an R1.5A and R1.5B sign set used at a STOP and GO control when these respective signs are in use (see Subsections 3.4.5 and 3.4.35 respectively).

- 11 Owing to the unique requirements that STOP sign R1, and its derivatives, place on road users, applications of these signs should be undertaken with care and attention to detail. Guidelines for the use of the various R1 signs are given in the following paragraphs:
- (a) STOP sign R1 is recommended for use on the minor approach leg(s) to a road junction when:
 - (i) the total traffic on the minor leg(s) differs from the total traffic on the major leg(s) by more than 20%, AND
 - (ii) the shoulder sight distance to either side is inadequate for safe installation of a YIELD sign R2 (see Figure 2.7 and paragraph 2.2.1.12), AND
 - (iii) the shoulder sight distance to both sides is adequate for safe crossing of the major roadway by traffic which has stopped on the minor roadway (see Figure 2.8 and paragraph 2.2.1.11 (c));
 - (b) STOP sign variant R1.1 is recommended for use under the same conditions as the standard R1 sign when, in addition:
 - (i) visibility of the R1 sign mounted at a standard height is, or may become, limited due to reduced side space, building canopies and/or supports, vegetation etc.;
 - (c) STOP/YIELD sign R1.2 should only be considered to vary the control of left turning traffic at road junctions when:
 - (i) the shoulder sight distance to the right, available to left turning traffic, complies with the provisions of Figure 2.7; AND
 - (ii) neither a YIELD sign R2 nor a STOP sign R1, ONLY, would be adequate to control all traffic movements safely.
 - (d) ALL-WAY STOP signs R1.3 and R1.4 may be considered for use when:
 - (i) the operation of a traffic control signal is warranted, as an interim measure until the signal is installed (the time period for such an installation may be considerable due to the inability of the road authority to meet the capital costs of signal

installation, particularly if there is no electricity available at a remote site.);

- (ii) a traffic control signal is not warranted **AND**, on the basis of a detailed engineering analysis, with particular regard to shoulder sight distance from the STOP LINE, it can be reasonably concluded that the level of safety will be improved and the accident potential reduced, **AND** all other feasible measures to improve safety and reduce accidents have been implemented (see paragraph 2.2.1.12).

Remote candidate sites for traffic control signals can present special maintenance problems. Rather than perpetuate a potentially dangerous and uneconomic situation the use of an ALL-WAY STOP control may be the only feasible solution available.

- (e) STOP and GO signs R1.5A and R1.5B are recommended for use at roadworks and other temporary public facility maintenance sites when :
 - (i) traffic volumes are more than 200 vehicles per hour, **AND**
 - (ii) single lane traffic is required, **AND**
 - (iii) drivers' visibility of the length of the site is restricted, **OR**
 - (iv) to control construction traffic crossing the path of general traffic within a work zone.

The use of signs R1.5A and R1.5B is not recommended for traffic control at road junctions or at sites which require traffic control during the hours of darkness. In the latter case the use of temporary traffic signals is recommended.

- 12 STOP sign R1 and ALL-WAY STOP signs R1.3 and R1.4 have been used within residential neighbourhoods to control the speed of traffic and limit the effects of "rat-running". This may appear an economical way of dealing with such problems due to low initial costs BUT additional fuel used in stop-start conditions is inefficient on a national scale and the widespread use of STOP or ALL-WAY STOP control rather than other forms of control results in higher noise and fume pollution levels. New township developments and longer term traffic control solutions aimed at "traffic calming" should preferably make use of techniques which are more

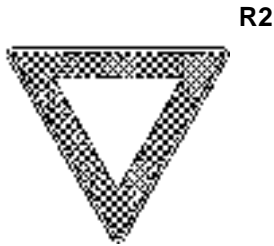
environmentally attractive. These include:

- (a) YIELD signs;
 - (b) small traffic circles or mini-circles;
 - (c) partial or complete road closures;
 - (d) landscaping;
 - (e) speed humps.
- 13 With the exception of STOP and GO signs R1.5A and R1.5B, and sign R1 used by a temporary scholar patrol, STOP sign R1 and its derivatives should be displayed on the left side of the roadway on which vehicles are required to stop. R1 signs shall be located as close as possible to the STOP LINE marking RTM1 when this is provided. The preferred distance of the sign from the line is 1,5 m in advance. If site conditions or junction geometry prevent this, the signs may be located up to 15 m, but not further, in advance of the line. If the roadway is a one-way roadway, or junction channelisation is provided signs R1, R1.1, R1.3 and R1.4 may be repeated on the right hand side of the roadway or on a median island and shall be repeated on the right side when the one-way, turning roadway or sliproad has two or more traffic lanes. All signs shall be sited in such a manner as not to reduce visibility of traffic on the intersecting roadway (see Volume 2, Chapter 3, Section 3.1, Figure 3.1).
 - 14 All CONTROL regulatory signs, whether used permanently or temporarily, shall be displayed in the PERMANENT colours only. With the exception of STOP and GO sign R1.5A and R1.5B, and sign R1 used by a temporary scholar patrol or for other temporary use such as a police road block, the colour of the reverse side of stop signs shall be white.
 - 15 Apart from signs used by scholar patrols which are covered in paragraph 2.2.1.8, STOP sign R1 and its various derivatives should be sized in accordance with the provisions of Table 2.4 for circular regulatory signs. The YIELD portion of sign R1.2 should be sized as for warning signs, as given in Table 3.1, and so that the side length of the triangle is effectively one size larger than the diameter of the STOP sign above it (see Volume 4, Chapter 2).

YIELD

For dimensions
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page

2.2.5



COLOURS:

PERMANENT AND TEMPORARY

Outer border: White retroreflective
Inner border: Red retroreflective
Background: White retroreflective

2.2.2 Yield

- 1 The YIELD regulatory sign R2 imposes **a mandatory requirement that the driver of a vehicle approaching such a sign shall yield right-of-way to all:**
 - (a) **traffic on the roadway which is joined by the roadway on which he is travelling; or**
 - (b) **rail traffic on the railway line which is crossed by the roadway on which he is travelling;**

where such traffic is so close as to constitute a danger or potential danger.

When a YIELD sign R2 is displayed at a railway crossing a RAILWAY CROSSING hazard marker sign W403 or W404 shall be displayed below the R2 sign. It is recommended that a YIELD LINE marking RTM2 be applied to permanent road surfaces and that a YIELD CONTROL AHEAD marking WM5 be marked on surfaced approach roadways for additional emphasis (see Chapter 7).
- 2 On a particular section of roadway which has been accorded priority over entering side roads it is preferable that the control method applied to all of these side roads be the same e.g. yield OR stop, not both forms of control.
- 3 Sign R2 should not be displayed:
 - (a) against the major traffic flow at a junction, OR
 - (b) in conjunction with a traffic control signal (unless controlling traffic using a left-turning roadway or sliproad physically separated from through or rightturning traffic by a raised traffic island), OR
 - (c) as an all-way yield condition (this requirement does not include YIELD signs provided at traffic circles, including mini-circles), OR
 - (d) at a junction where a STOP sign R1 is displayed on other approaches.

(See paragraph 2.2.2.7 and Figure 2.6).
- 4 The R2 sign variant YIELD TO PEDESTRIANS sign R2.1 shall be used at mid-block pedestrian crossings. The similar variant YIELD AT TRAFFIC CIRCLES sign R2.2 may be used at traffic circles (see Subsections 2.2.3 and 2.2.4).
- 5 Where advance visibility of a YIELD sign R2 is inadequate, or a yield control may be unexpected, the use of a TRAFFIC CONTROL "YIELD" AHEAD warning sign W303 within the provisions of Subsection 3.4.3 is recommended. The use of a distance SUPPLEMENTARY PLATE sign IN11.3 with the warning sign is also recommended. The distance displayed should take into account the stopping distance requirements of approaching vehicles. Yield control and information arrow markings may also be used to advantage in advance to indicate that yield control is ahead.
- 6 YIELD sign R2 is recommended for use on the minor approach leg(s) to a road junction when:
 - (a) the total traffic on the minor leg(s) differs from the total traffic on the major leg(s) by more than 20%, AND
 - (b) the shoulder sight distance to both sides is adequate for safe crossing of the major roadway (see Figure 2.7), AND
 - (c) kerb or curve alignment permits left turning vehicles to negotiate the junction safely at a speed of more than 20 km/h, OR
 - (d) a left turning roadway is provided at a channelised junction without full acceleration lane facilities in the major roadway.
- 7 Sign R2 should be displayed on the left side of the roadway as near as possible to the point at which the driver is required to stop when yielding right of way. The preferred location of the sign is 1,5 m in advance of the yield line when this is used. If site conditions or junction geometry prevent this, the signs may be located up to 15 m, but not further, in advance of the line. If the roadway is a one-way roadway, or junction channelisation is provided, sign R2 may be repeated on the right side of the roadway or on a median island and shall be repeated on the right side when the one-way, turning roadway or sliproad, has two or more traffic lanes (see Volume 2, Chapter 3).
- 8 The size of the sign should be as given in Table 3.1 for warning signs. The sign shall be mounted with the apex of the triangle lower-most (see Chapter 3, Section 3.1).

YIELD TO PEDESTRIANS

COLOURS:

PERMANENT AND TEMPORARY

YIELD segment

Outer border: White retroreflective
 Inner border: Red retroreflective
 Background: White retroreflective

PEDESTRIANS segment

Border & symbol: White retroreflective
 Background: Red retroreflective

R2.1

For dimensions
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 page

2.2.6



2.2.3 Yield to Pedestrians

- 1 The YIELD TO PEDESTRIANS regulatory sign R2.1 imposes **a mandatory requirement that the driver of a vehicle approaching such a sign shall yield right- of-way to pedestrians crossing the public road, or waiting to cross the public road.**
 - 2 Sign R2.1 shall be located between 3 m and 6 m in advance of the road marking indicating a pedestrian crossing. YIELD TO PEDESTRIANS signs R2.1 shall be used at mid-block pedestrian crossings marked in accordance with the provisions of Chapter 7 and the guidelines given in Volume 2.
 - 3 The crossing point shall be marked with a BLOCK PEDESTRIAN CROSSING marking TM4 which shall be preceded on each approach by a YIELD LINE marking RTM2. PEDESTRIAN CROSSING AHEAD LINE marking RM11 shall be used on the approaches.
 - 4 Sign R2.1 may also be located on turning roadways or
- sliproads, which are not subject to some other form of control, to indicate the location of a pedestrian crossing point and the requirement that drivers shall yield right- of-way to pedestrians in the prescribed manner.
 - 5 YIELD TO PEDESTRIANS signs should be located on the left side of the roadway. If a pedestrian refuge island is placed in the centre of a pedestrian crossing an additional sign may be placed on this island to the right of approaching traffic. An R2.1 sign shall be repeated on the right side when a one-way, turning roadway or sliproad has two or more traffic lanes.
 - 6 Sign R2.1 shall not be used in situations where visibility to the crossing point is not of a high standard.
 - 7 The "yield" portion of sign R2.1 should be sized as for warning signs whilst the "pedestrian" symbol plate size is based on the sizes of rectangular RESERVATION signs (see Volume 4, Chapter 2).

YIELD AT MINI CIRCLE

For dimensions
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page

2.2.7



COLOURS:

PERMANENT AND TEMPORARY

Outer border:	White retroreflective
Inner border:	Red retroreflective
Symbol:	Black semi-matt
Background:	White retroreflective

2.2.4 Yield at Mini Circle

- The YIELD AT MINI CIRCLE regulatory sign R2.2 imposes a **mandatory requirement that drivers of vehicles approaching a mini circle shall yield right of way to any vehicle which will cross any YIELD LINE marking RTM2 at such junction before them, and which, in the normal course of events, will cross the path of such drivers' vehicles, and that the driver shall move in a clockwise direction in such junction and attempt not to encroach on the mini circle.** YIELD LINE markings RTM2 shall be applied to permanent road surfaces on all approaches to a traffic circle when YIELD AT MINI CIRCLE signs R2.2 are used. A YIELD CONTROL AHEAD marking WM5 may be marked on surfaced approach roadways for additional emphasis (see Chapter 7).
- The "rules of the road" prescribe the action required of drivers entering a junction containing a traffic island. The use of YIELD AT MINI CIRCLE sign 2.2 and ROUNDABOUT sign R137 (see Subsection 2.3.1) must be carefully controlled. Sign R137 effectively provides exactly the same measure of control as the relevant "rules of the road" appropriate to traffic circles at which no control sign has been erected. The use of sign R2.2 is appropriate at mini circles, particularly those which can be classified as mini-circles, where a modified yield procedure, as described in paragraph 2.2.4.1 is recommended in order to obtain maximum operational efficiency of such junctions. If traffic volumes exceed 150 vehicles per hour on two or more conflicting approaches during any hour of the day at a small or mini circle in an essentially residential area then the use of signs R2.2 is recommended.
- When advance visibility of a YIELD AT MINI CIRCLE sign is inadequate, or the traffic circle may be unexpected, the use of TRAFFIC CIRCLE warning sign W201 within the provisions of Subsection 3.3.1 together with the use of a distance SUPPLEMENTARY PLATE sign IN11.3 is recommended. The distance displayed should take into account the stopping distance requirements of approaching vehicles.
- Sign R2.2 should be displayed on the left side of the road as near as possible to the point at which the driver is required to stop when yielding right of way. The preferred location is 1.5 m in advance of the yield line. If the approach to the mini circle is provided with a median island sign R2.2 may be repeated on the right side of the approach roadway (see Volume 2, Chapter 3).
- Details of road marking arrows for use within mini circles are given in Subsection 7.2.19.
- Traffic circles may be used as part of a particular style of town planning road layout or they may be provided for a more functional traffic control purpose. This purpose may be to restrict the speed and flow of traffic within a residential environment (traffic calming), or it may be to efficiently handle significant flows of conflicting traffic when gap acceptance is a viable traffic control technique, as an alternative to a traffic signal. The design of traffic circles to perform one of these functions involves more than the simple application of road signs and road markings and is outside the scope of this Manual.

COLOURS:
 PERMANENT AND TEMPORARY
 Border & symbol: White retroreflective
 Background: Red retroreflective

NO ENTRY

R3



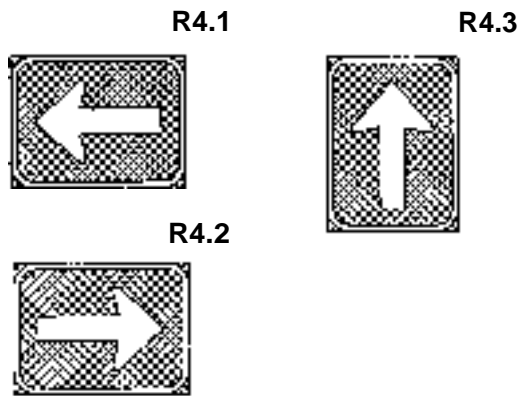
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 page
 2.2.8

2.2.5 No Entry

- 1 The NO ENTRY regulatory sign R3 is to **indicate to the driver of a vehicle that the entry of all vehicular traffic is prohibited.**
- 2 Sign R3 should be displayed to prohibit "wrong way" entry to a roadway when confusion may exist as to the direction of travel in a roadway or at a road junction. Sign R3 may be particularly relevant when one or more ONE-WAY ROADWAY signs R4.1, R4.2 or R4.3 are not adequate or appropriate, for whatever reason. Likely locations are:
 - (a) freeway off-ramp junctions with cross roads;
 - (b) one-way exit only roadways from bus termini or car parks;
 - (c) at junctions where one-way roadways become two-way roadways.
- 3 Consistent with the philosophy of giving a POSITIVE message rather than a NEGATIVE message, wherever possible, the POSITIVE regulatory ONE-WAY ROADWAY signs R4.1, R4.2 or R4.3 are preferred to the NEGATIVE regulatory sign R3 at junctions of one way roadways. However, in special situations where the background environment to signs R4.1 or R4.2 or R4.3 is busy and/or confusing, emphasis may be provided by using both sign types (see Subsections 2.1.1 and 2.2.5 and Volume 2).
- 4 NO ENTRY signs R3 should not be qualified by making them applicable only for a portion of the day, or to some classes of vehicle. If a need exists to reserve access to a portion of roadway or to some off-street facility used by vehicular traffic, to a specific class of vehicle or for a specific time of day, then an appropriate RESERVATION sign should be used. (See paragraph 2.1.1.6 and Section 2.5 and in particular Subsections 2.5.6 and 2.5.7.)
- 5 Care should be taken in siting R3 signs to avoid the possibility of confusing drivers as to which roadway they apply to. The sign should normally be displayed on the left hand side, as near as possible to the beginning of the roadway to which entry is prohibited. Where additional emphasis is required, an additional sign should be displayed on the right hand side of the roadway. In some cases the signs may need to be sited a short distance into the roadway junction to improve visibility and it may often be desirable to orientate the sign to suit the direction of approach of traffic by mounting the sign at an angle to the edge of the road, kerb line or road reserve boundary.
- 6 An R3 sign may be included in the sign face of a map-type direction sign to indicate in advance that the junction ahead is with a one-way roadway and that movement is limited to only certain legs of the junction (see Section 2.8).
- 7 NO ENTRY sign R3 should be sized in accordance with the provisions of Table 2.4.

ONE-WAY ROADWAY

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page
2.2.9



COLOURS:
PERMANENT AND TEMPORARY
Border & arrow: White retroreflective
Background: Red retroreflective

2.2.6 One-way Roadway

- 1 The ONE-WAY ROADWAY signs R4.1, R4.2 and R4.3 impose a **mandatory requirement on drivers, that if they intend to use the roadway concerned, then they shall only proceed in such roadway in the direction indicated by the arrow.**
- 2 Signs R4.1 and R4.2 should be used at junctions with one-way roadways to indicate that two-way travel is not permitted. The additional use of NO ENTRY sign R3 at such junctions should be reserved for special situations of alignment or visibility where emphasis is required (see Subsection 2.2.4 and Volume 2).
- 3 The signs should be carefully sited at the potential point of wrong-way entry to a one-way road to avoid possible confusion as to which leg of the road junction is affected. In most situations the use of two signs, one on each side of the roadway, is recommended. It may be necessary to site the signs a short distance into the one-way roadway and it may often be desirable to orientate the signs to suit the direction of approaching traffic.
- 4 Sign R4.3 may be displayed in a one-way roadway between junctions on the left and/or right side of the roadway to indicate to traffic entering from minor junctions such as property accesses that the roadway is one-way in the direction indicated. This sign may also be appropriate at the start of a one-way roadway to indicate that conditions have changed from two-way to one-way.
- 5 The signs should be sized in accordance with the provisions of Table 2.4.

PEDESTRIAN PRIORITY

COLOURS:

PERMANENT AND TEMPORARY Border

& symbol: White retroreflective

Background: Red retroreflective

R5

For dimensions
ret. Vat 4
page

2.2.10

2.2.7 Pedestrian Priority

- 1 The PEDESTRIAN PRIORITY sign R5 is to **indicate to drivers of vehicles that only:**
 - (a) a vehicle for the purpose of delivering or loading goods;
 - (b) a vehicle used in connection with maintenance;
 - or
 - (c) an emergency vehicle,

shall be permitted in an area set aside as a pedestrian precinct marked by such signs and the vehicle contemplated in paragraphs (a), (b) and (c) shall:

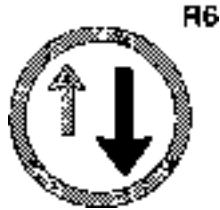
 - (i) yield right-of-way to all pedestrians who are in, or who are crossing or about to cross their path;
 - (ii) observe a maximum speed of 15 km/h unless another speed limit is indicated by an appropriate sign; and
 - (iii) if they are delivering or loading goods, shall only do so in places marked for such purpose in such area by a road traffic sign.

Sign R5 should be displayed at all possible vehicular points of entry to a pedestrian precinct where full access control is not exercised. Sign R5 may be used in conjunction with an appropriate RESERVATION sign if access to the area is required to be given to a specific class of vehicle such as those used by hotel residents or post office officials.
- 2 In some cases traffic regulations will prohibit access to all vehicles and/or motor vehicles at all times. In such cases the use of sign R5 is not required, however, depending on circumstances it may be necessary to permit access to emergency or maintenance vehicles. Such provision is best made by having movable or demountable barriers, of an aesthetic design, with provision for access by such vehicles being covered by by-law rather than signs.
- 3 The design of pedestrian precincts which permit vehicular access should ensure the points of entry or exit of vehicles are clearly identifiable to pedestrians within the precinct.
- 4 Where a relatively high frequency of vehicle access has been deemed acceptable it may be necessary to introduce signs to guide pedestrians (see Chapter 4, and Volume 2, Chapter 14).
- 5 All other signs used to control the movements or actions of vehicles permitted access to a pedestrian precinct may be of minimum size and may be specially mounted to blend harmoniously with the environmental treatment of the area.
- 6 The side length of the sign shall be 600 mm for standard applications. Signs with a side length up to 900 mm may be used if special circumstances require.

YIELD TO ONCOMING TRAFFIC

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2.2.11



COLOURS:

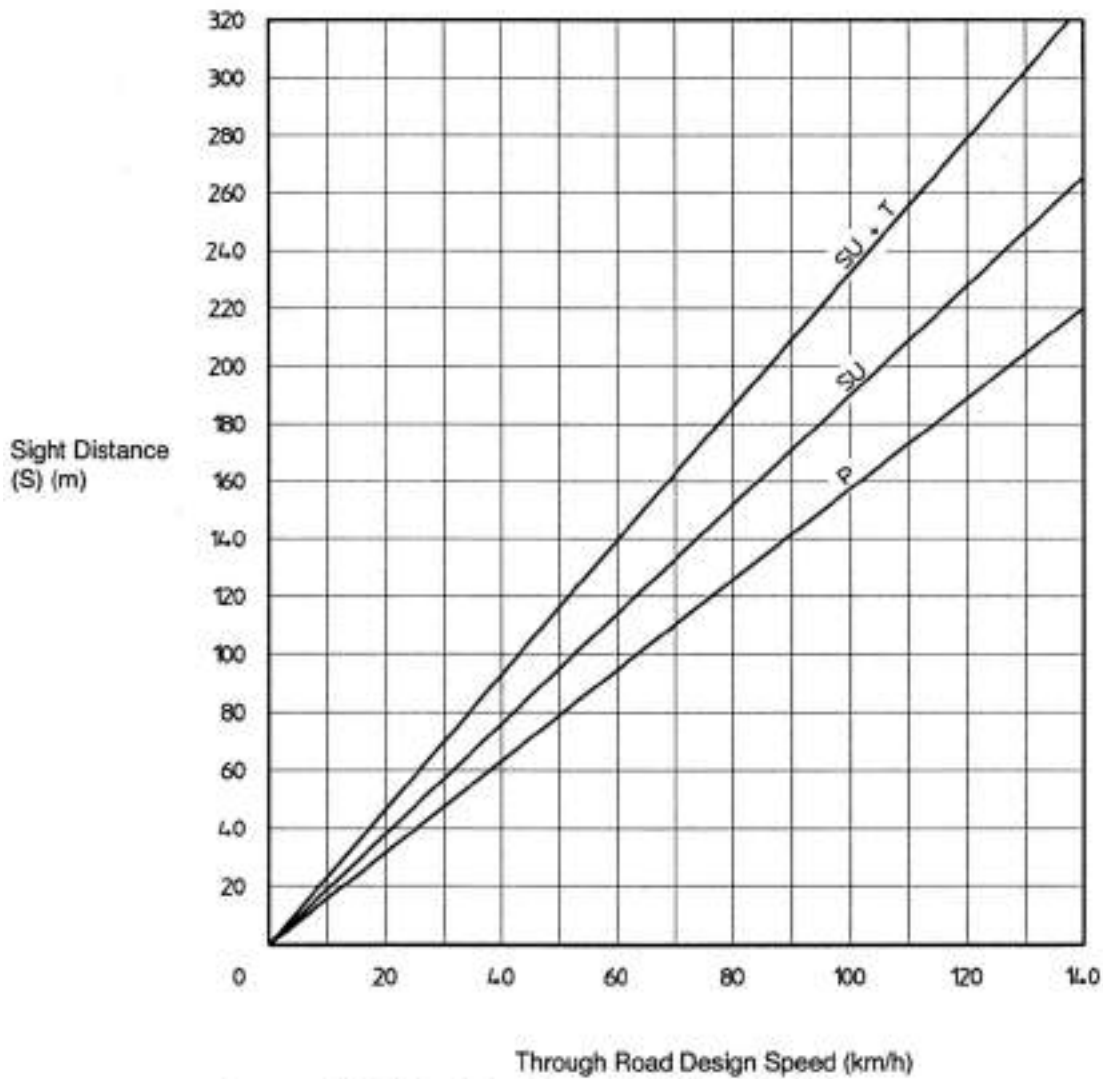
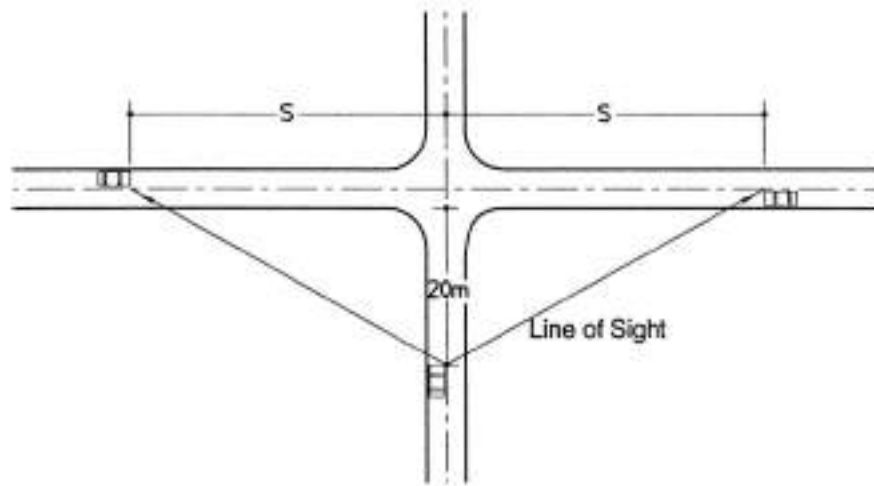
PERMANENT AND TEMPORARY

Border: Red retroreflective
Symbol: Red retroreflective
and black semi-matt
Background: White retroreflective

2.2.8 Yield to Oncoming Traffic

- 1 The YIELD TO ONCOMING TRAFFIC sign R6 imposes **a mandatory requirement that the driver of a vehicle approaching such a sign shall yield right-of-way to all oncoming traffic, at the point where the sign is displayed, where oncoming traffic is so close to the obstruction or constraint in the road-way ahead that the driver will not be able to pass such obstruction or constraint without causing danger or potential danger to the oncoming traffic, or require them to stop.**
- 2 Sign R6 shall not be used at a road junction. Sign R6 shall only be used on a section of roadway between junctions, where a part of the section is only wide enough to accommodate traffic in one direction at a time. It is recommended that sign R6 only be used when approaching drivers can see the whole length of the narrow roadway at night and by day.
- 3 Typical situations where the use of sign R6 may be appropriate are:
 - (a) at a narrow bridge suitable only for traffic in one direction at a time, when it is considered necessary to maintain priority in one direction rather than the other, but where traffic volumes are such that sufficient opportunities to proceed will still occur in the non-priority direction;
 - (b) at chicanes and pinch points which form part of traffic calming measures.

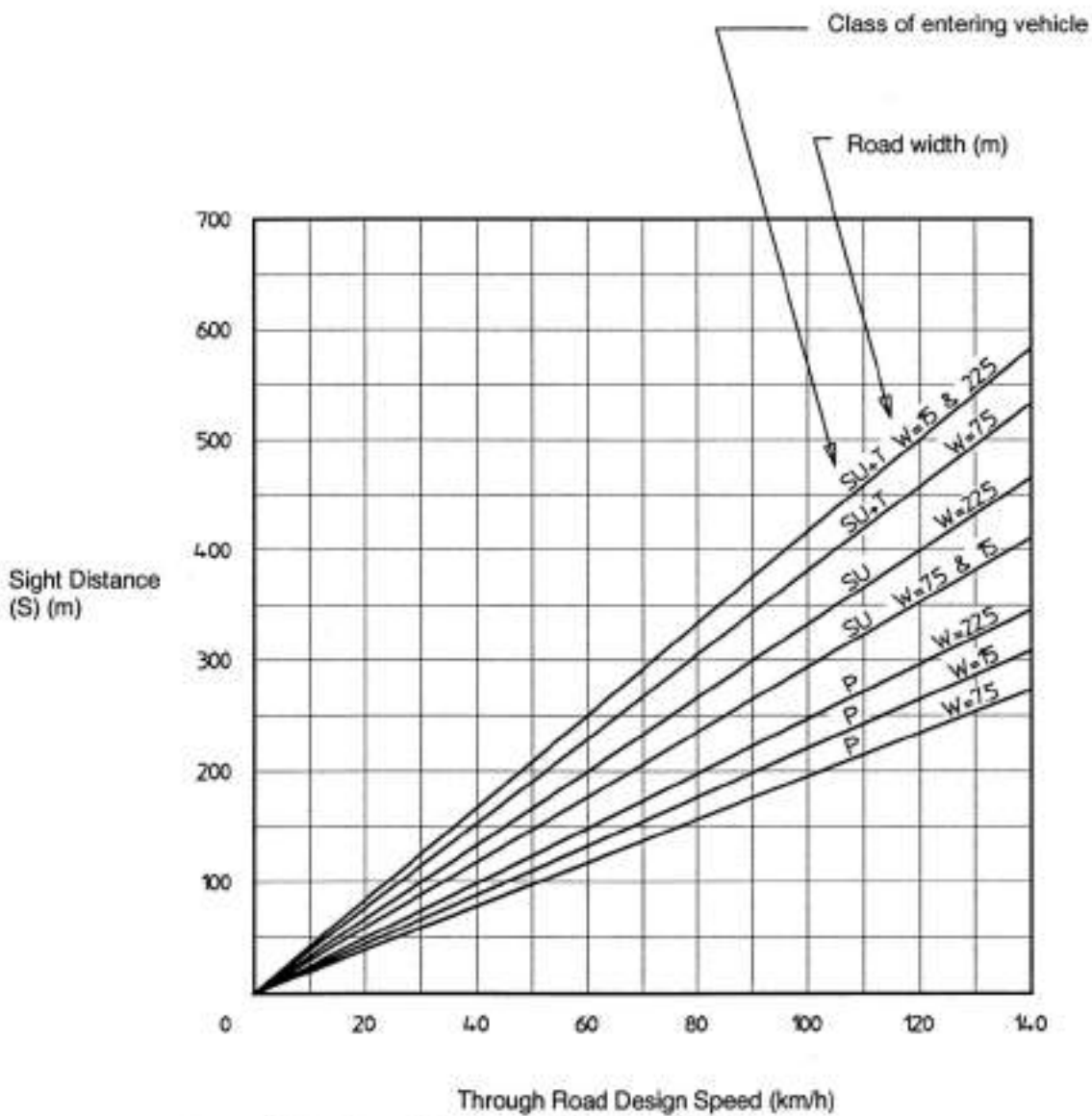
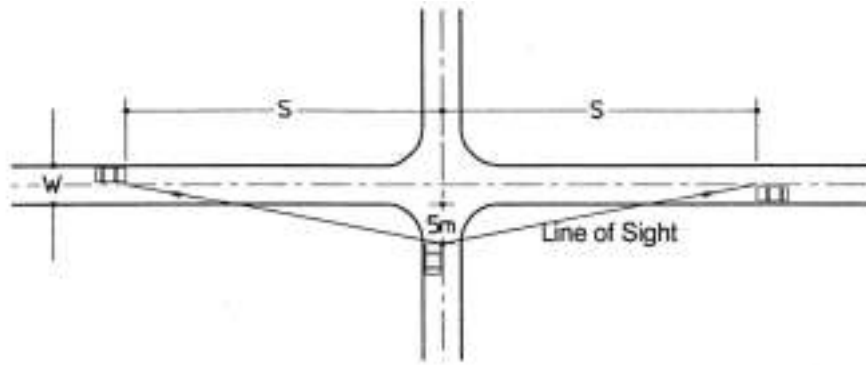
For examples of the application of sign R6 see Volume 2, Chapters 3 and 12.
- 4 Sign R6 may be repeated in advance of the yield point to indicate that such a priority control exists up ahead. When used in this manner the sign display shall include a SUPPLEMENTARY PLATE sign IN11.3 indicating the distance to the yield point. This distance should be sufficient for drivers to slow down and stop if necessary in order to yield right-of-way.
- 5 A YIELD TO ONCOMING TRAFFIC sign R6 should be positioned on the left side of the roadway sufficiently far from the obstruction that a stopped vehicle will not obstruct traffic travelling in the opposite direction. If necessary to ensure this clearance a YIELD LINE marking RTM2 may be marked in the appropriate position.
- 6 Road users who are travelling in the opposite direction should be provided with an ONCOMING TRAFFIC SHALL YIELD diagrammatic sign GS155 or a RIGHT OF WAY sign IN7. If the geometry of the obstruction may make the traffic priority unclear, for example at a traffic calming chicane or pinch point, a diagrammatic sign showing the obstruction GS156 may be used in place of signs GS155 or IN7.
- 7 YIELD TO ONCOMING TRAFFIC sign R6 is classified as a CONTROL sign. In common with all signs in this category of regulatory sign, if a temporary application of sign R6 is called for, the permanent version only shall be used.
- 8 Sign R6 should be sized as for circular regulatory signs as indicated in Table 2.4



Source: Adapted from Urban Transport Guidelines-1 (Draft)

Fig 2.7

Shoulder Sight Distance for Yield Condition



Source: Adapted from Urban Transport Guidelines-1 (Draft)

Fig. 2.8 Shoulder Sight Distance for Stop Condition

MINIMUM SPEED

COLOURS:

PERMANENT

Border: White retroreflective
 Background: Blue retroreflective
 Numerals: White retroreflective
 "BMOD"

TEMPORARY

Inner border: Black semi-matt
 Background & outer border: Yellow retroreflective
 Numerals: Black semi-matt
 "BMOD"

R101



For dimensions
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2.3.1

TR101



2.3.1 Minimum Speed

- 1 The MINIMUM SPEED regulatory sign R101 imposes a **mandatory requirement that drivers of vehicles on a public road shall maintain or exceed the minimum speed indicated in kilometres per hour by means of a number on such sign.** Minimum speeds shall only be displayed in increments of 10 km/h.
- 2 Sign R101 should only be displayed where it is necessary to control the presence of slow-moving vehicles so that they do not impede the normal safe flow of traffic. The maximum value of minimum speed which may be displayed on a MINIMUM SPEED sign R101 shall be 80 km/h. When ordering signs it is recommended that the sign be ordered stating the speed value after the number e.g.. R101-50.
- 3 The sign may be displayed on the same post below a DUAL CARRIAGEWAY FREEWAY BEGINS regulatory sign R401 or a SINGLE CARRIAGEWAY BEGINS regulatory sign R402 to indicate that until signed differently traffic using the freeway ahead is regulated to travel at, or in excess of the minimum speed indicated.
- 4 The sign may be displayed on the same post below a SPEED LIMIT regulatory sign R201 to indicate a change in a previously posted minimum speed limit, or the introduction of a minimum speed limit on a public road on which none had been previously posted.
- 5 When mounted with another sign the lower edge of sign R101 should be at least 1,2 m above the level of the adjacent roadway.
- 6 The sign should be sized to match the sign with which it is displayed or in accordance with Table 2.4. The sign should never be larger than a SPEED LIMIT sign R201 with which it is displayed.
- 7 Sign R101 should be displayed on the left side of the roadway with the provision that a second sign may be displayed on the right side of a one-way roadway. The sign is recommended to be repeated at intervals not exceeding 5 km on a freeway if not displayed at the start of and at all entry points to the freeway.
- 8 Temporary regulatory sign TR101 may be used under the same circumstances as permanent MINIMUM SPEED regulatory signs when temporary roadworks or other conditions require.
- 9 A MINIMUM SPEED sign R101 may be incorporated into the sign face of a guidance sign, either ground mounted or overhead, to indicate that a route or lane is subject to a minimum speed limit and that other route(s) or lane(s) are not. The intention of such a display is to limit the use of certain lanes to vehicles which can travel above the indicated minimum to reduce the congestion and delays caused by slow-moving vehicles to a minimum. The use of sign R101 is preferred to the use of such signs as NO OVERTAKING signs R214 and R215, NO GOODS VEHICLES sign R229 or NO GOODS VEHICLES EXCEEDING MASS sign R230, since sign R101 still permits faster moving heavy vehicles to overtake slow moving vehicles. Diagrammatic guidance signs of this type may be appropriate during restricted traffic conditions common during roadworks, or on climbing lanes or lanes provided to permit overtaking opportunities (see Subsection 2.8.4 and Chapter 4).
- 10 Signs R101 or TR101 should not be displayed if there is no intention of enforcing compliance with them.

VEHICLES EXCEEDING MASS ONLY

For dimensions
ref. Vol. 4
page

2.3.2

R102



TR102



COLOURS:

PERMANENT

Border: White retroreflective
Background: Blue retroreflective
Numerals: White retroreflective "B
MOD" ("t" in
DIN 1451 Style B)

TEMPORARY

Inner border: Black semi-matt
Background & outer border: Yellow retroreflective
Numerals: Black semi-matt
"B MOD" ("t" in
DIN 1451 Style B)

2.3.2 Vehicles Exceeding Mass Only

- 1 The VEHICLES EXCEEDING MASS ONLY regulatory sign R102 imposes a **mandatory requirement that the use of a public road or portion of public road by drivers of vehicles is restricted to vehicles of a mass that exceeds that indicated in tonnes by means of a number, only, and that the driver of such a vehicle shall only use the public road or portion of public road designated by the sign. The value indicated in tonnes is the equivalent of the gross vehicle mass (GVM) or gross combination mass (GCM) of the vehicle in kilograms.**
- 2 Sign R102 should be displayed where it is required to direct all heavy vehicles, including buses, exceeding the mass indicated, into an exclusive heavy vehicle compulsory stop area, a weigh station or an inspection area. If only goods vehicle drivers are required to act in the manner required by sign R102 then GOODS VEHICLES ONLY sign R123 or GOODS VEHICLES EXCEEDING MASS ONLY sign R124 should be used, as is appropriate to the mass required to be regulated.
- 3 The sign should be displayed on the left-hand side of the roadway. On dual carriageway roadways it is recommended that the sign be mounted overhead to indicate the lane into which vehicles exceeding the mass shall move (see Subsection 2.8.4).
- 4 When ordering signs it is recommended that the mass limit value be indicated after the sign number e.g. R102-10 for a PERMANENT mass limit of 10 tonnes. The sign should be sized in accordance with Table 2.4.
- 5 Temporary regulatory sign TR102 may be used under similar circumstances to permanent VEHICLES EXCEEDING MASS ONLY regulatory signs.

KEEP LEFT- KEEP RIGHT

COLOURS:

PERMANENT

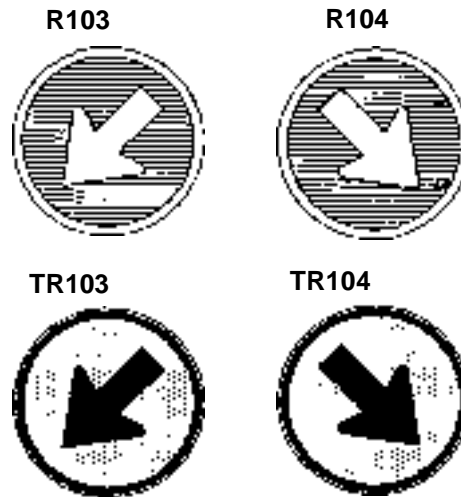
Border & arrow: White retroreflective
Background: Blue retroreflective

TEMPORARY

Inner border & arrow: Black semi-matt
Background & outer border: Yellow retroreflective

For dimensions
ref. Vol. 4
pages

2.3.3
2.3.4



2.3.3 Keep Left and Keep Right

- 1 The KEEP LEFT and KEEP RIGHT regulatory signs R103 and R104 impose a **mandatory requirement that the driver of a vehicle shall pass only to the left-hand side or the right-hand side, as indicated by an arrow, of an obstacle in the roadway on which the sign has been placed.** Signs R103 and R104 may be displayed as a SELECTIVE RESTRICTION sign in conjunction with a secondary message indicating a class of vehicle to which the mandatory requirement applies (see Section 2.7).
- 2 Signs R103 and R104 must be displayed with extreme care, so that the arrow shall point downwards at an angle of approximately 45° towards the side on which traffic must pass. If the arrow is incorrectly aligned the meaning of the sign could be altered to that of the PROCEED LEFT ONLY, PROCEED RIGHT ONLY or PROCEED STRAIGHT ONLY signs R105, R106 and R107.
- 3 The signs should normally be displayed with their lower edge 750 mm above the surface of the roadway. The height of display should, however, take into account the vertical alignment of the roadway. If the sign is located just beyond a crest curve it should be further elevated to improve visibility. Sign R103 is commonly used to indicate the beginning of a median island. In this case it may be mounted lower in combination with a DANGER PLATE hazard marker W402, on one post, to indicate that traffic must pass the sign to the left. The sign does not need to be repeated at subsequent openings in an otherwise continuous median island unless special conditions require the repetition of the message. Sign R103 may, for instance, be displayed on the end of a median island to the left of a NO ENTRY sign R3, when there is a risk of traffic entering the opposing roadway.
- 4 Signs R103 and R104 SHALL NOT be displayed side by side on a channelising island which traffic may pass either to the left or right of, even if by doing so traffic will reach the same destination e.g. a pedestrian refuge in a one-way roadway. Such a device should be signed using SHARP CURVE CHEVRON hazard marker signs W406 and W405 mounted side by side or by DANGER PLATE hazard markers W401 and W402. (These combinations should preferably be manufactured from one piece of material).
- 5 A KEEP RIGHT sign R104 will normally be reserved for use in special situations such as the start of a dedicated and separate portion of roadway, on the left side of the main roadway, such as a cycle lane or a bus lane. In this case general traffic is directed to pass to the right of a channelizing island whilst cyclists or buses may be directed to the left.
- 6 Temporary regulatory signs TR103 and TR104 are widely used at temporary roadworks sites to indicate temporary obstructions in the roadway or temporary changes in direction of the roadway which traffic is required to negotiate. Signs TR103 and TR104 may be used in conjunction with TEMPORARY BARRICADE sign TW411 to demarcate roadway deviations. At the start of such deviations or at the beginning of a tapered reduction in roadway width it is recommended that two TR103 or TR104 signs, as appropriate, be mounted on the same pole, one above the other. The signs should also be elevated as high as is practical to improve visibility of this often critical point in a roadworks zone.
- 7 Signs R103 and R104 should be sized in accordance with the provisions of Table 2.4.

PROCEED LEFT ONLY - PROCEED RIGHT ONLY - PROCEED STRAIGHT ONLY

For dimensions
ref. Vol. 4
pages

2.3.5
2.3.6
2.3.7



COLOURS:

PERMANENT

Border & arrow: White retroreflective
Background: Blue retroreflective

TEMPORARY

Inner border & arrow: Black semi-matt
Background & outer border: Yellow retroreflective



2.3.4 Proceed Left Only, Proceed Right Only and Proceed Straight Only

- 1 The PROCEED LEFT ONLY, PROCEED RIGHT ONLY and PROCEED STRAIGHT ONLY regulatory signs R105, R106 and R107 impose a **mandatory requirement that the driver of a vehicle shall proceed only in the direction indicated by an arrow on such sign.**
- 2 Signs R105 and R106 should be displayed on the far side of a one-way roadway facing traffic wishing to enter the one-way roadway from the stem of a T-junction or from an exit from a site generating significant volumes of traffic.
- 3 Sign R107 should be displayed at the side of a roadway in advance of a junction to indicate that traffic shall only proceed straight on.
- 4 Temporary regulatory signs TR105, TR106 and TR107 may be used at temporary roadworks or at other temporary traffic control situations under the same circumstances as permanent PROCEED LEFT ONLY, PROCEED RIGHT ONLY or PROCEED STRAIGHT ONLY signs. Signs TR105 and TR106 may commonly be used at a roadworks site when a temporary road closure is required and movement is only permitted to move to the left OR right as the case may be. Sign TR105 and TR106 SHALL NOT be mounted together if traffic is permitted to move to the left AND right of the road closure i.e. into a two-way cross road. In such a situation a T-JUNCTION CHEVRON hazard marker sign W409 should be used.
- 5 Sign R105 and R106 or TR105 and TR106 should be located so that traffic obeying the signs turns in front of the signs. The signs should be sized in accordance with Table 2.4.

TURN LEFT- TURN RIGHT

COLOURS:

PERMANENT

Border & arrow: White retroreflective

Background: Blue retroreflective

TEMPORARY

Inner border & arrow: Black semi-matt

Background & outer border: Yellow retroreflective

For dimensions
ref. Vol. 4
pages

2.3.8
2.3.9

R108



R109



TR108



TR109



2.3.5 Turn Left and Turn Right

- 1 The TURN LEFT and TURN RIGHT regulatory signs R108 and R109 impose **a mandatory requirement that the driver of a vehicle shall proceed only in the direction indicated by the arrow on such sign, at the junction ahead**. If the sign applies only to certain period(s) of the day or to a specific class of vehicle this may be indicated by a secondary message below the primary signs. The latter application will classify the combined sign as a SELECTIVE RESTRICTION sign (see Section 2.7).
- 2 Signs R108 and R109 should only be displayed in advance, on an approach to a junction where traffic from that approach may only enter one leg of the junction as indicated by the sign.
- 3 If the mandatory requirement excludes one class of vehicle the movement which that class of vehicle shall undertake should be signed separately.
- 4 Signs R108 and R109 may be used in combination with ONE-WAY ROADWAY signs R4.1 or R4.2 to control traffic movements at a junction. The signs should be sized in accordance with Table 2.4.
- 5 The signs should be displayed on the left side of the roadway at least 15 m in advance of the junction. If the roadway is a one-way roadway a second sign may be located on the right side of the roadway. Care shall be taken to see that no property access lies between the sign and the junction.
- 6 Temporary regulatory signs TR108 and TR109 may be used under the same circumstances as permanent TURN LEFT and TURN RIGHT regulatory signs when a temporary detour is required in an urban area, particularly within a business or central business district.

PEDESTRIANS ONLY

For dimensions
ref. Vol. 4
page

2.3.10

R110



TR110



COLOURS:

PERMANENT

Border & symbol: White retroreflective

Background: Blue retroreflective

TEMPORARY

Inner border & symbol: Black semi-matt

Background & outer border: Yellow retroreflective

2.3.6 Pedestrians Only

- 1 The PEDESTRIANS ONLY regulatory sign R110 **indicates that the public road or portion of public road is set aside for pedestrians only and imposes a mandatory requirement that pedestrians shall only use such public road or such portion of public road.** This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign (see Subsection 2.3.8).
- 2 Sign R110 should be displayed at the beginning of a path, route or lane which has been specifically provided for pedestrians only. It is, however, not required that the normal sidewalk or footpath provided for pedestrian movement in urban business districts or residential areas be so signed unless there is a particular problem of control which requires such a sign. It may be necessary to repeat the sign at intervals along the pedestrian path.
- 3 The sign should be located on the left side of the

pedestrians only facility provided that, if a physical separator exists between the path or route, and an adjacent roadway it may be more effective to locate the sign on the right side of the pedestrians only facility. A NO PEDESTRIANS sign R218 should not be displayed on the traffic side of a PEDESTRIANS ONLY sign R110.

- 4 Sign R110 should not be used as a pedestrian direction sign in combination with an arrow. If there is a need to guide pedestrians, specific rectangular guidance signs should be provided.
- 5 Temporary regulatory signs PEDESTRIANS ONLY are unlikely to be required. However, it is possible that at complex urban roadworks sites or at building construction in city centres the use of temporary PEDESTRIAN ONLY signs TR110 may be appropriate.
- 6 Since sign R110 is relevant to all traffic the sign size should be in accordance with Table 2.4.

COLOURS:

PERMANENT

Border & symbol: White retroreflective

Background: Blue retroreflective

TEMPORARY

Inner border

& symbol: Black semi-matt

Background

& outer border: Yellow retroreflective

R111



For dimensions
ref. Vol. 4
page

2.3.11

TR111



2.3.7 Cyclists Only

- 1 The CYCLISTS ONLY regulatory sign R111 **indicates that the public road or portion of public road is set aside for cyclists only and imposes a mandatory requirement that cyclists shall only use such public road or such portion of public road.** This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign. (See Subsection 2.3.8 for details of shared cyclist/pedestrian facilities).
- 2 Sign R111 should be displayed at the beginning of a path, route or lane which has been specifically provided for cyclists only. It may be necessary to repeat the sign at intervals along the cyclepath, route or lane where crossing traffic, including pedestrians may need to be advised of the presence of the cyclists only facility.
- 3 The sign should be located on the left side of the cyclists only facility provided that if a physical separator exists between the path or route, and an adjacent roadway it may be more effective to locate the sign on the right side of the cyclists only facility.
- 4 A NO CYCLISTS sign, R219, should not be displayed on the traffic side of a CYCLISTS ONLY sign R111.
- 5 Temporary regulatory signs CYCLISTS ONLY are unlikely to be required. However, a temporary version TR111 may be appropriate during complex urban work sites even if permanent versions of the sign are not normally required at the same location.
- 6 Since sign R111 is relevant to all traffic the sign size should be in accordance with the provisions of Table 2.4.

CYCLISTS AND PEDESTRIANS ONLY

For dimensions
ref. Vol. 4
pages

2.3.12
2.3.13
2.3.14
2.3.15

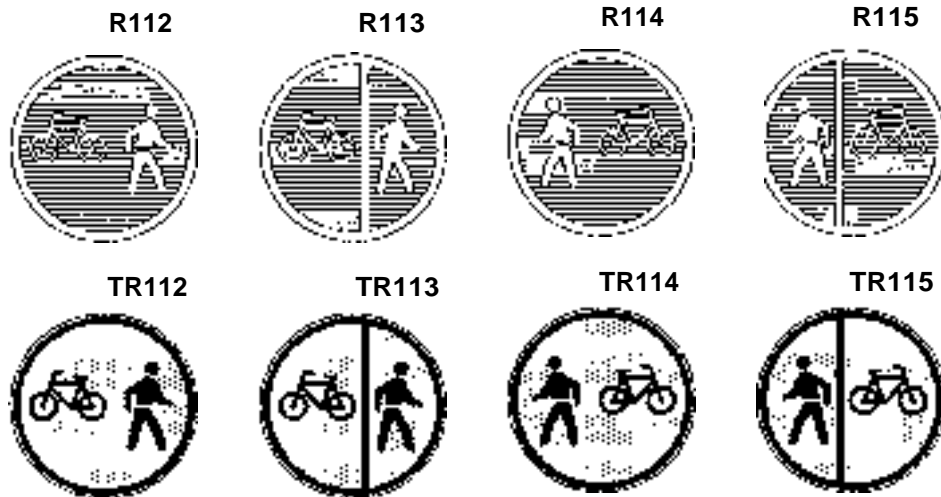
COLOURS:

PERMANENT

Border & symbol: White retroreflective
Background: Blue retroreflective

TEMPORARY

Inner border
& symbol: Black semi-matt
Background
& outer border: Yellow retroreflective



2.3.8 Cyclists and Pedestrians Only

- 1 The CYCLISTS and PEDESTRIANS ONLY regulatory signs R112, R113, R114 and R115 **indicates that the public road or portion of public road is set aside for cyclists and pedestrians only and imposes a mandatory requirement that cyclists and pedestrians shall only use such public road or such portion of public road.** This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign. (See Subsections 2.3.6 and 2.3.7 for details of signs for exclusive pedestrian and exclusive cyclist facilities).
- 2 Signs R112 and R114 should be displayed to indicate that the shared facility is an INTEGRATED one. Cyclists and pedestrians utilise the same path which may or may not be divided longitudinally by a broken white road marking. When such a marking is used it is recommended that painted symbols indicating which portion should be used by cyclists and which by pedestrians should be marked on the roadway surface in appropriate positions.
- 3 Signs R113 and R115 should be displayed to indicate that the shared facility is a SEGREGATED one. Cyclists and pedestrians utilise separate but parallel paths. The division between paths may be a physical separator or a single continuous white painted line. Painted symbols depicting a cyclist and a pedestrian should be applied to the surface of the roadway to indicate which portions each should use.
- 4 The correctly handed versions of the CYCLISTS and PEDESTRIANS ONLY signs should be used for integrated and segregated facilities. The decision as to which sign is most appropriate is likely to be subjective based on local conditions. If in doubt arrange signs so that pedestrians are furthest from passing vehicular traffic. Signs R112 or R113 will then be used in one direction and signs R114 or R115 in the opposite direction.
- 5 Signs R112, R113, R114 or R115 should be displayed, as appropriate, at the beginning of the shared path which has been specifically provided for cyclists and pedestrians only. It may be necessary to repeat the appropriate sign at intervals along the path to indicate to other crossing traffic that it is a cyclist and pedestrian only facility.
- 6 The signs should be located on the left side of the path provided that if a physical separator exists between the path(s) and the adjacent roadway it may be more effective to locate the sign on the right side of the path.
- 7 A NO CYCLISTS and PEDESTRIANS sign R220 should not be displayed on the traffic side of a CYCLISTS and PEDESTRIANS ONLY sign R112, R113, R114 or R115.
- 8 Should temporary regulatory signs CYCLISTS and PEDESTRIANS ONLY be required they should be numbered TR112, TR113, TR114 or TR115 as appropriate.
- 9 Since signs R112, R113, R114 and R115 are relevant to all traffic the sign size should be in accordance with the provisions of Table 2.4. If signs are required purely to control the segregation of cyclists and pedestrians the sign size may be reduced to 300 mm diameter.

2.3.9 Other Command Signs

- 1 Other COMMAND regulatory signs referring to specific classes of vehicle **indicate that the public road or portion of public road is set aside for the class of vehicle indicated on the sign only and impose a mandatory requirement that the driver of the class of vehicle indicated on the sign shall only use such public road or such portion of public road.** This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.
- 2 The signs shown on the following pages cover the following classes of vehicle:
 - R116 MOTOR CYCLES ONLY
 - R117 MOTOR CARS ONLY
 - R118 TAXIS ONLY
 - R119 MINIBUSES ONLY
 - R120 MIDI-BUSES ONLY
 - R121 BUSES ONLY
 - R122 DELIVERY VEHICLES ONLY
 - R123 GOODS VEHICLES ONLY
 - R124 GOODS VEHICLES OVER INDICATED GVM ONLY
 - R125 CONSTRUCTION VEHICLES ONLY
 - R126 VEHICLES CONVEYING DANGEROUS GOODS ONLY
 - R127 ABNORMAL VEHICLES ONLY
 - R128 RICKSHAWS ONLY
 - R129 TOUR BUSES ONLY
 - R130 AGRICULTURAL VEHICLES ONLY
 - R131 ANIMAL DRAWN VEHICLES ONLY
- 3 Any of the above signs should be displayed at the beginning of a roadway which has been specifically set aside for the use of the class of vehicle indicated on the sign, and vehicles of the class indicated shall not use any other roadway, nor shall any other class of vehicle use the roadway signed.
- 4 The use of signs R116 to R131 is likely to be rare. However, as the provision of facilities segregated by vehicle class become more appropriate to major transport terminals and other complex commercial developments the use of such signs will enable improved control and direction of traffic by class of vehicle.
- 5 Command signs R116 to R131 may be incorporated into the sign face of a guidance sign to indicate that use of a route, or lane of a roadway is mandatory to the class of vehicle displayed on the sign.
- 6 Signs R116 to R131 should be used with care to ensure that they do not become over-utilised or incorrectly utilised, thereby reducing their effectiveness when really required.
- 7 Temporary versions of regulatory signs R116 to R131 are unlikely to be required. If required these should be numbered in the TR100 series from TR116 to TR131 and should be manufactured with a black semi-matt symbol and border on a yellow retroreflective back-ground. Such signs have the significance that **for a temporary period due to roadworks or other traffic deviation or detour, the class of vehicle indicated may only use the roadway designated by the sign, and that no other class of vehicle is permitted to use the roadway so signed.**
- 8 The size and positioning of signs R116 to R131 and TR116 to TR131 should follow the general requirements for regulatory signs as given in Table 2.4, Chapter 1 and Volume 2.
- 9 This Subsection includes details on three signs specific to public transport vehicles, namely, MINIBUSES ONLY sign R119, MIDI-BUSES ONLY sign R120 and BUSES ONLY sign R121. The relevant feature used to classify these vehicles for signing purposes is the number of seats. Details of these classifications are given in Chapter 10. Signing for the control of buses has occurred for some time. However, control of mini-buses is as yet not fully developed and midi-buses are a new addition to the range of public transport vehicle types. Whilst the signs provided conform to the general principles of the sign group their application is likely to develop with time (see also Volume 2).
- 10 Signs R122 to R127 refer to various classes of goods vehicle. Signs R122, R125, R126 and R127 refer to specific types of goods vehicle and apply to all such vehicles over 3 500 kg gross vehicle mass or gross combination mass. If it is required to link a specific maximum permissible mass to these signs this may be achieved by using a SELECTIVE RESTRICTION sign (see Section 2.7).
- 11 Signs R123 and R124 are similar in their application. The GOODS VEHICLES ONLY sign R123 refers to all goods vehicles (over 3 500 kg GVM/GCM), whereas the GOODS VEHICLES OVER INDICATED GVM ONLY sign R124 refers only to goods vehicles with a gross vehicle mass or gross combination mass exceeding the figure indicated on the goods vehicle symbol.

MOTOR CYCLES ONLY

R116

For dimensions
ref. Vol. 4
page
2.3.16



TR116

COLOURS:
PERMANENT
Border & symbol: White retroreflective
Background: Blue retroreflective
TEMPORARY
Inner border
& symbol: Black semi-matt
Background
& outer border: Yellow retroreflective



The MOTOR CYCLES ONLY regulatory sign R116 indicates that the public road or portion of public road is set aside for motor cyclists only and imposes a mandatory requirement that motor cyclists shall only use such public road or such portion of public road. This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

MOTOR CARS ONLY

R117

For dimensions
ref. Vol. 4
page
2.3.17



TR117

COLOURS:
PERMANENT
Border & symbol: White retroreflective
Background: Blue retroreflective
TEMPORARY
Inner border
& symbol: Black semi-matt
Background
& outer border: Yellow retroreflective



The MOTOR CARS ONLY regulatory sign R117 indicates that the public road or portion of public road is set aside for motor cars only and imposes a mandatory requirement that drivers of motor cars shall only use such public road or such portion of public road. This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

TAXIS ONLY

R118

For dimensions
ref. Vol. 4
page
2.3.18



TR118

COLOURS:
PERMANENT
Border & symbol: White retroreflective
Background: Blue retroreflective
TEMPORARY
Inner border
& symbol: Black semi-matt
Background
& outer border: Yellow retroreflective



The TAXIS ONLY regulatory sign R118 indicates that the public road or portion of public road is set aside for taxis only and imposes a mandatory requirement that drivers of taxis shall only use such public road or such portion of public road. This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

MINIBUSES ONLY

R119

For dimensions
ref. Vol. 4
page
2.3.19



TR119

COLOURS:
PERMANENT
Border & symbol: White retroreflective
Background: Blue retroreflective
TEMPORARY
Inner border
& symbol: Black semi-matt
Background
& outer border: Yellow retroreflective



The MINIBUSES ONLY regulatory sign R119 indicates that the public road or portion of public road is set aside for minibuses only and imposes a mandatory requirement that drivers of minibuses shall only use such public road or such portion of public road. This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

MIDI-BUSES ONLY

BUSES ONLY

For dimensions
ref. Vol. 4
page

R120



For dimensions
ref. Vol. 4
page

R121



2.3.20

2.3.21

COLOURS:
PERMANENT

Border & symbol: White retroreflective
Background: Blue retroreflective

TEMPORARY

Inner border
& symbol: Black semi-matt
Background
& outer border: Yellow retroreflective

TR120



COLOURS:
PERMANENT

Border & symbol: White retroreflective
Background: Blue retroreflective

TEMPORARY

Inner border
& symbol: Black semi-matt
Background
& outer border: Yellow retroreflective

TR121



The MIDI-BUSES ONLY regulatory sign R120 **indicates that the public road or portion of public road is set aside for midi-buses only** and imposes a mandatory requirement that drivers of midi-buses shall only use such public road or such portion of public road. This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

The BUSES ONLY regulatory sign R121 **indicates that the public road or portion of public road is set aside for buses only** and imposes a mandatory requirement that drivers of buses shall only use such public road or such portion of public road. This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

DELIVERY VEHICLES ONLY

GOODS VEHICLES ONLY

For dimensions
ref. Vol. 4
page

R122



For dimensions
ref. Vol. 4
page

R123



2.3.22

2.3.23

COLOURS:
PERMANENT

Border & symbol: White retroreflective
Background: Blue retroreflective

TEMPORARY

Inner border
& symbol: Black semi-matt
Background
& outer border: Yellow retroreflective

TR122



COLOURS:
PERMANENT

Border & symbol: White retroreflective
Background: Blue retroreflective

TEMPORARY

Inner border
& symbol: Black semi-matt
Background
& outer border: Yellow retroreflective

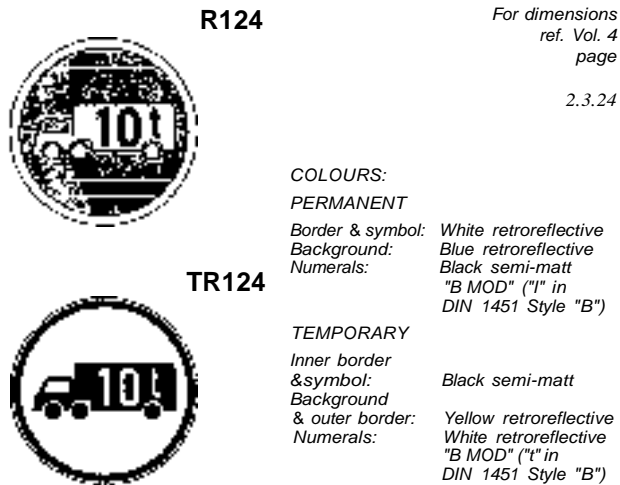
TR123



The DELIVERY VEHICLES ONLY regulatory sign R122 **indicates that the public road or portion of public road is set aside for delivery vehicles only** and imposes a mandatory requirement that drivers of delivery vehicles shall only use such public road or such portion of public road. This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

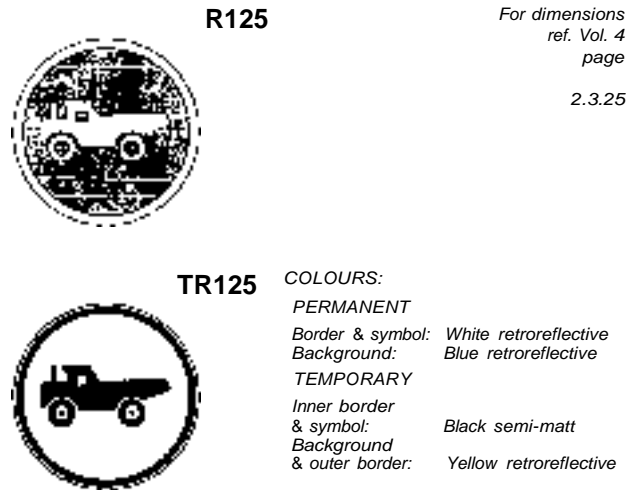
The GOODS VEHICLES ONLY regulatory sign, R123, **indicates that the public road or portion of public road is set aside for goods vehicles only** and imposes a mandatory requirement that drivers of goods vehicles shall only use such public road or such portion of public road. This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

GOODS VEHICLES OVER INDICATED GVM ONLY



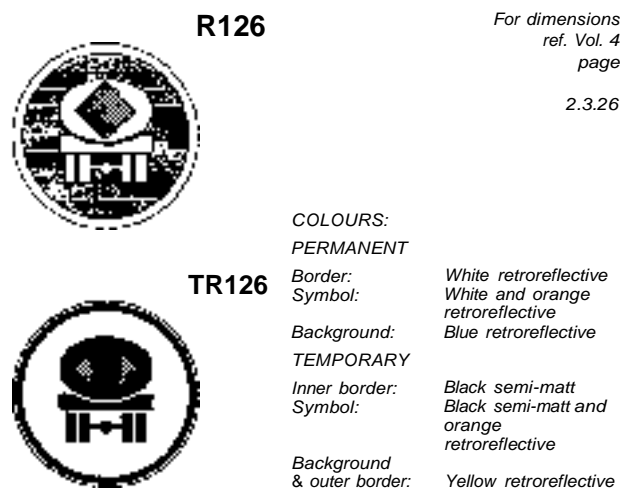
The GOODS VEHICLES OVER INDICATED GVM ONLY regulatory sign R124 indicates that the public road or portion of public road is set aside for goods vehicles over indicated GVM (or GCM) only and imposes a mandatory requirement that drivers of goods vehicles over indicated GVM (or GCM) shall only use such public road or such portion of public road. This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

CONSTRUCTION VEHICLES ONLY



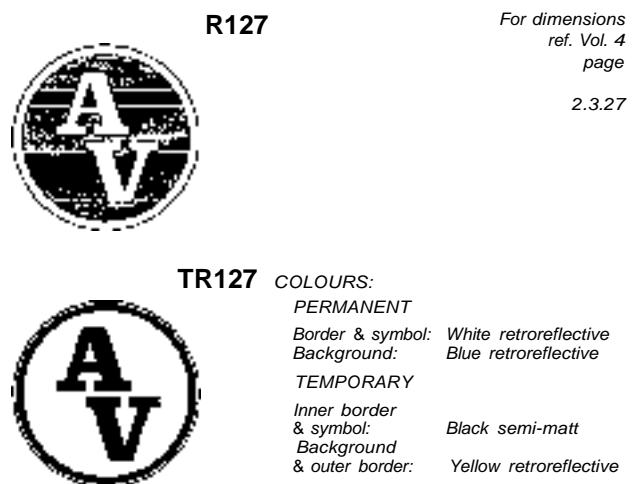
The CONSTRUCTION VEHICLES ONLY regulatory sign R125 indicates that the public road or portion of public road is set aside for construction vehicles only and imposes a mandatory requirement that drivers of construction vehicles shall only use such public road or such portion of public road. This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

VEHICLES CONVEYING DANGEROUS GOODS ONLY



The VEHICLES CONVEYING DANGEROUS GOODS ONLY regulatory sign R126 indicates that the public road or portion of public road is set aside for vehicles conveying dangerous goods only and imposes a mandatory requirement that drivers of vehicles conveying dangerous goods shall only use such public road or such portion of public road. This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

ABNORMAL VEHICLES ONLY



The ABNORMAL VEHICLES ONLY regulatory sign R127 indicates that the public road or portion of public road is set aside for abnormal vehicles only and imposes a mandatory requirement that drivers of abnormal vehicles shall only use such public road or such portion of public road. This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

RICKSHAWS ONLY

TOUR BUSES ONLY

For dimensions
ref. Vol. 4
page
2.3.28

R128



COLOURS:
PERMANENT
Border & symbol: White retroreflective
Background: Blue retroreflective
TEMPORARY
Inner border
& symbol: Black semi-matt
Background
& outer border: Yellow retroreflective

TR128



For dimensions
ref. Vol. 4
page
2.3.29

R129



COLOURS:
PERMANENT
Border & symbol: White retroreflective
Background: Blue retroreflective
Legend: White retroreflective
DIN 1451 Style "1"

TR129



TEMPORARY
Inner border &
symbol: Black semi-matt
Background
& outer border: Yellow retroreflective
Legend: Black semi-matt
DIN 1451 Style " "

The RICKSHAWS ONLY regulatory sign R128 *indicates that the public road or portion of public road is set aside for rickshaws only* and imposes a mandatory requirement that drivers of rickshaws shall only use such public road or such portion of public road. This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

The TOUR BUSES ONLY regulatory sign R129 *indicates that the public road or portion of public road is set aside for tour buses only* and imposes a mandatory requirement that drivers of tour buses shall only use such public road or such portion of public road. This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

AGRICULTURAL VEHICLES ONLY

ANIMAL-DRAWN VEHICLES ONLY

For dimensions
ref. Vol. 4
page
2.3.30

R130



COLOURS:
PERMANENT
Border & symbol: White retroreflective
Background: Blue retroreflective
TEMPORARY
Inner border
& symbol: Black semi-matt
Background
& outer border: Yellow retroreflective

TR130



For dimensions
ref. Vol. 4
page
2.3.31

R131



COLOURS:
PERMANENT
Border & symbol: White retroreflective
Background: Blue retroreflective
TEMPORARY
Inner border
& symbol: Black semi-matt
Background
& outer border: Yellow retroreflective

TR131



The AGRICULTURAL VEHICLES ONLY regulatory sign R130 *indicates that the public road or portion of public road is set aside for agricultural vehicles only* and imposes a mandatory requirement that drivers of agricultural vehicles shall only use such public road or such portion of public road. This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

The ANIMAL-DRAWN VEHICLES ONLY regulatory sign R131 *indicates that the public road or portion of public road is set aside for animal-drawn vehicles only* and imposes a mandatory requirement that drivers of animal-drawn vehicles shall only use such public road or such portion of public road. This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

PAY TOLL

For dimensions
ref. Vol. 4
page

2.3.32

R132



COLOURS:
PERMANENT
Border & symbol: White retroreflective
Background: Blue retroreflective

2.3.10 Pay Toll

- 1 The PAY TOLL regulatory sign R132 imposes **a mandatory requirement that drivers of vehicles shall only proceed on a public road designated as a toll road if they are able to pay the toll charge and that they shall pay the toll charge at the toll plaza or plazas concerned.** The junction indicated on a guidance sign by regulatory sign R132 is therefore the last junction before a toll plaza at which a driver may leave the route on which he is travelling if he does not wish to, or is unable to pay the required toll (see Section 4.11).
- 2 Sign R132 shall be displayed on ground-mounted or overhead direction signs to indicate to drivers that a route ahead, reached either by travelling straight on or by leaving the route on which they are travelling, is subject to toll charges. Display of sign R132 indicates a point of commitment to pay a toll charge, and is the last point at which drivers may take action if they do not wish to pay a toll charge. Sign R132 should be displayed as a stand-alone regulatory sign on the left side of the roadway just beyond the actual point of commitment but in full view of approaching drivers. Sign R132 should be repeated on the right side of multi-lane one-way roadways at such points of commitment (see Volume 2, Chapter 6).
- 3 Sign R132 shall be displayed at all points of payment at toll plazas. Table 2.6 gives details of the sizes of PAY TOLL signs.
- 4 It is recommended that sign R132 be displayed in conjunction with a TRAFFIC CONTROL "STOP" AHEAD warning sign W302 or a SPEED LIMIT regulatory sign R201 in HIGH VISIBILITY backgrounds, as is deemed appropriate, at distances of 800 m and 400 m from the toll plaza. These distances should appear below the signs on SUPPLEMENTARY PLATE signs IN11.3 (see example in Section 2.8).
- 5 Subject to the position of the end of a "PAY TOLL" section of toll route in relation to a toll plaza or a junction a de-restriction version of sign R132 may be erected (see Subsection 2.9.1 and Volume 2, Chapter 6). Due to the infrastructure required to levy and collect toll charges no provision is made for a temporary version of sign R132.

TABLE 2.6

"PAY TOLL" SIGN SIZES

TABLE 2.6

Operating Speed (km/h)	Application	Size (diameter) (mm)
80-120	Stack-type arrow	8d
80-120	Stack-type sign	10d
100-120	Map-type arrow	10d
120	Overhead arrow(s)	10d
120	Overhead lane use control sign	1200 or 1600
120	Stand alone	1200
80 -120	High visibility background	900 or 1600
60	At Toll Booth	900
Stationary	On Toll Booth	150

NOTES:

- (1) Where "7d" is the letter height on the sign.

SWITCH HEAD LAMPS ON

COLOURS:

PERMANENT

Border & symbol: White retroreflective

Background: Blue retroreflective

TEMPORARY

Inner border

& symbol: Black semi-matt

Background

& outer border: Yellow retroreflective

R133



For dimensions
ref. Vol. 4
page

2.3.33

TR133



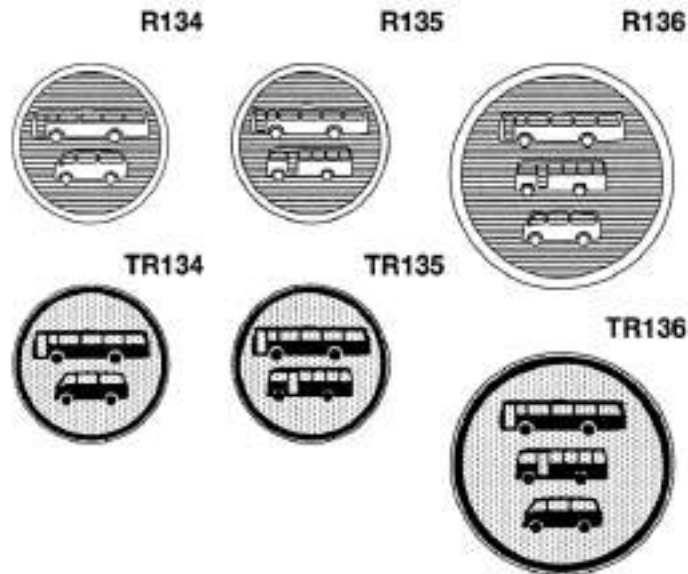
2.3.11 Switch Head Lamps On

- 1 The SWITCH HEAD LAMPS ON regulatory sign R133 imposes **a mandatory requirement that drivers of vehicles on a public road shall switch on such vehicle's head lamps to emit a dipped-beam.**
- 2 Sign R133 should only be displayed in special circumstances where it is necessary to make the presence of a vehicle more evident to on-coming vehicles on two-way roadways. The sign is recommended for use in advance of a tunnel or under conditions of adverse visibility.
- 3 It is recommended that sign R133 be displayed in combination with a TUNNEL warning sign W319, mounted on the same pole above the warning sign. The signs should be located in accordance with the provisions of Subsection 3.4.15.
- 4 Temporary regulatory sign TR133 may be used at roadworks or other temporary conditions when visibility of on-coming vehicles may be diminished due to dust, smoke or poor weather conditions, or a combination of these factors.
- 5 Signs R133 and TR133 may be followed at the appropriate point by a de-restricted version of signs R133 or TR133 (see Subsection 2.9.1 and Volume 2, Chapter 3).
- 6 Signs R133 and TR133 should be sized in accordance with Table 2.4.

BUSES, MIDI-BUSES AND MINIBUSES ONLY

For dimensions
ref. Vol. 4
pages

2.3.34
2.3.35
2.3.36



COLOURS:

PERMANENT

Border & symbol: White retroreflective
Background: Blue retroreflective

TEMPORARY

Inner border
& symbol: Black semi-matt
Background
& outer border: Yellow retroreflective

2.3.12 Buses, Midi-buses and Minibuses Only

- The BUSES, MIDI-BUSES AND MINIBUSES ONLY regulatory signs R134, R135 and R136 indicate **that the public road or portion of public road is set aside for all classes of bus, midi-bus and minibus only and imposes a mandatory requirement that drivers of buses, midi-buses and minibuses shall only use such a public road or such portion of public road.** This has the effect that no other classes of road user shall use the public road or portion of public road.
- The use of signs R134, R135 and R136, as with MINI-BUSES ONLY sign R119, MIDI-BUSES ONLY sign R120 and BUSES ONLY sign R121, is likely to be rare (see Subsection 2.3.10). Signs R134, R135 or R136 should be displayed as appropriate at the beginning of a roadway which has been specifically set aside for shared use by passenger transport buses, midi-buses and minibuses. When signs R134, R135 or R136 are displayed buses, midi-buses and minibuses shall not use any other roadway, nor shall any other class of vehicle use the roadway identified by signs R134, R135 or R136.
- The shared use of passenger transport facilities, either roadways or terminal or parking facilities, has developed as an economical and practical operational feature. These signs, displaying more than one symbol, may be used to reduce the need for multiple signs (see also signs R139 and R140).
- Signs R134, R135 and R136 should be located on the left side of the public road or portion of public road to which they apply. If the public road is a one-way roadway dedicated to passenger transport vehicles the signs are recommended on both the left and right sides of the roadway.
- Signing for public transport facilities can become complex and there is a wide range of regulatory signs available, including signs in the COMMAND (R100), PROHIBITION (R200), and RESERVATION (R300) series. Designers are recommended to read Subsections 2.5.1 and 2.5.8 which include general material relevant to the use of public transport signs and Volume 2, Chapter 8 which deals with the application of public transport signing.
- Temporary regulatory signs TR134, TR135 and TR136 are unlikely to be required, however, if specified they shall be manufactured in the black and yellow colours appropriate to temporary command signs.
- The size and location of signs R134 and R135 should follow the general requirements as given in Chapter 1, Table 2.4. Sign R136 with three symbols should always be one size larger, application for application, than sign R134.

ROUNDABOUT

COLOURS:

PERMANENT

Border & symbol: White retroreflective
Background: Blue retroreflective

TEMPORARY

Inner border
& symbol: Black semi-matt
Background
& outer border: Yellow retroreflective

R137

For dimensions ref. Vol. 4

page

2.3.37



TR137



2.3.13 Roundabout

- 1 The ROUNDABOUT regulatory sign R137 imposes a **mandatory requirement that drivers entering a roundabout shall turn to the left and shall travel round the roundabout in a clockwise direction**, and it imposes a **mandatory requirement that drivers entering a roundabout shall yield right-of-way to traffic approaching from the right, within the roundabout, where such traffic is so close as to constitute a danger or potential danger**. A roundabout shall be considered to be a traffic circle with a large internal island diameter (see paragraph 2.3.13.2).
- 2 **Sign R137 shall only be used at roundabouts which have a large enough internal roadway that a vehicle can be fully contained within the circular roadway between the entry roadway and the next possible exit path from the roundabout.** A typical minimum internal circle diameter for such a requirement is in the range of 20 m to 25 m, based on a typical 12 m bus or goods vehicle. A roundabout of this size or larger may commonly be marked with more than one concentric internal circulatory lane. The use of sign R137 shall only be warranted when, for some reason, the rule of the road applicable to un-signed traffic circles is considered to need reinforcement.
- 3 Sign R137 shall be positioned on the left side of the roadway as near as possible to the point at which the driver is required to stop when yielding right-of-way. The preferred location of the sign is 1,5 m in advance of the YIELD LINE marking RTM2, when this is used (see also Figure 3.1 in Volume 2, Chapter 3).
- 4 When advance visibility of the entry point R137 sign, or of the roundabout itself, is insufficient (see minimum stopping sight distance values given in Table 2.5) TRAFFIC CIRCLE advance warning sign W201 should be provided in accordance with Subsection 3.3.1 and Figure 3.1. If the approaching roadway is a dual carriageway roadway sign W201 may be provided on both the left and right sides of the roadway. In positioning sign(s) W201 account should be taken of the visibility of the junction and likely traffic queue length. If sign(s) W201 are positioned significantly further from the junction than indicated in Figure 3.1 it is recommended that a SUPPLEMENTARY PLATE sign IN11.3 giving the distance to the junction be included with sign W201.
- 5 The use of sign R137 and YIELD AT MINI CIRCLE sign R2.2 must be carefully controlled. Sign R137 effectively provides exactly the same measure of control as the relevant "rule of the road" appropriate to traffic circles at which no control sign has been erected. The use of sign R137 must, as indicated in paragraph 2.3.13.2, only be used when that rule is appropriate. At smaller traffic circles, particularly those which can be classified as mini-circles, the use of YIELD AT MINI CIRCLE sign R2.2, which requires a different yield procedure, is recommended in order to obtain maximum operational efficiency of such junctions (see Subsection 2.2.4 and Volume 2, Chapter 3).
- 6 When a roundabout traffic circle is so large as to effectively consist of a number of T-junctions the use of a combination of PROCEED LEFT ONLY sign R105 and SHARP CURVE CHEVRON sign W408 at each entry point is recommended. Such signs should be positioned on the central island, located to best advantage to be in line with approaching drivers (see Volume 2, Chapter 3).
- 7 The use of a temporary version of the ROUNDABOUT sign is likely to be rare but if required such a sign should be specified by the number TR137.
- 8 ROUNDABOUT signs R137 and TR137 shall be sized in accordance with Table 2.4.

TRAMS ONLY

For dimensions
ref. Vol. 4
page

2.3.38

R138



COLOURS:
PERMANENT

Border & symbol: White retroreflective
Background: Blue retroreflective

2.3.14 Trams Only

- 1 The TRAMS ONLY regulatory sign R138 indicates **that the public road or portion of public road is set aside for trams only and imposes a mandatory requirement that no other class of road user shall use the public road or portion of public road.**
- 2 Sign R138 has been included in the coverage of the Manual to indicate to developers and designers the manner in which signing for trams, or light rail vehicles, may be provided. The development time for such a form of transport is always likely to be significant but it is hoped that the inclusion of a basic range of signs will assist designers at the appropriate time. Relevant RESERVATION signs are also included in Section 2.5.
- 3 The use of shared public roadways or portions of public roadway by trams and other forms of public transport is very possible. Two additional signs, BUSES AND TRAMS ONLY, R139, and BUSES, TRAMS AND MINIBUSES ONLY, R140, are also provided to cater for such possible shared operation. These are detailed below.
- 4 The use of signs R138, R139 and R140 should be in accordance with similar command signs such as R119 and R121. They should be displayed at the beginning of a roadway set aside for the relevant passenger transport vehicle types. When one of the signs is displayed vehicles of the relevant class or classes shown on the signs shall not use any other roadway, nor shall any other class of vehicle use the roadway identified by the sign. Designers are recommended to read Sub-section 2.5.1 which includes general material relevant to the use of public transport signs and Volume 2, Chapter 8 which deals with the application of public transport signing.
- 5 Due to the inclusion of three symbols sign R140 has a special range of larger than standard sizes. For details refer to Volume 4.

BUSES AND TRAMS ONLY

For dimensions
ref. Vol. 4
page

2.3.39

R139



COLOURS:
PERMANENT
Border & symbol: White retroreflective
Background: Blue retroreflective

BUSES, TRAMS AND MINIBUSES ONLY

For dimensions
ref. Vol. 4
page

2.3.40

R140



The BUSES AND TRAMS ONLY regulatory sign, R139, **indicates that the public road or portion of public road is set aside for the use of buses and trams only and imposes a mandatory requirement that drivers of buses shall only use such public road or such portion of public road.** This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

The BUSES TRAMS AND MINIBUSES ONLY regulatory sign, R140, **indicates that the public road or portion of public road is set aside for the use of buses, trams and minibuses only and imposes a mandatory requirement that drivers of buses shall only use such public road or such portion of public road.** This has the effect that no other class of road user shall use the public road or portion of public road indicated by the sign.

SPEED LIMIT

COLOURS:

PERMANENT

Border: Red retroreflective

Legend & symbol: Black semi-matt
"BMOD"

Background: White retroreflective

TEMPORARY

Border: Red retroreflective

Legend & symbol: Black semi-matt
"BMOD" Background:
Yellow retroreflective

R201-100



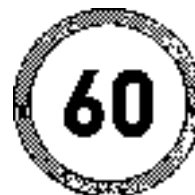
R201-60

For dimensions
ref. Vol4
pages2.4.1
2.4.2

TR201-100



TR201-60



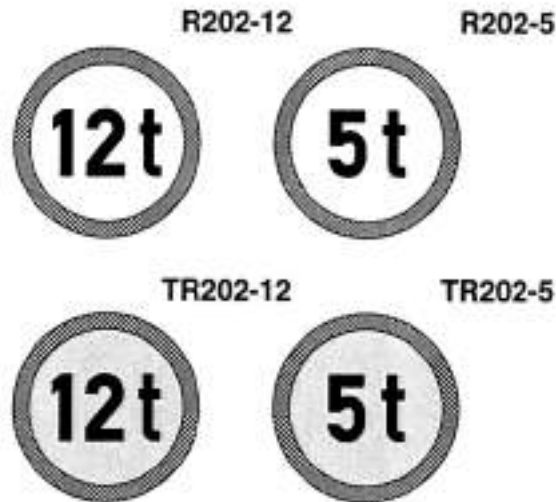
2.4.1 Speed Limit

- 1 The SPEED LIMIT regulatory sign R201 imposes **a mandatory requirement that drivers of vehicles on a public road shall not exceed the speed limit indicated in kilometres per hour, by means of a number on such sign, beyond such sign.** Speed limits should preferably be displayed only in increments of 10 km/h. Sign R201 may be displayed as a SELECTIVE RESTRICTION sign in conjunction with a secondary message indicating a class of vehicle to which the mandatory requirement applies (See Section 2.7).
- 2 Sign R201 may be displayed to indicate the general speed limit, if doubt may exist in the minds of drivers as to the class of road. Due to occasional variations in statutory general speed limit, and to the difficulty which drivers may have in identifying a class of road, it is recommended that all changes in speed limit be indicated using a SPEED LIMIT sign R201. This applies to an increase or decrease in speed limit. When ordering signs it is recommended that the speed limit value be indicated after the sign number e.g. R201 - 100.
- 3 Since R201 sign automatically cancels a different speed limit applicable to a roadway immediately prior to the sign, R201 signs should not be preceded or accompanied by a speed de-restriction sign. In terms of the above, **the use of speed de-restriction signs is not recommended.**
- 4 When it is required to reduce a speed limit, particularly from the statutory maximum speed limit, this should normally be achieved in increments of 20 km/h e.g. 120 km/h to 100 km/h to 80 km/h to 60 km/h. The minimum distance between such signs should be 150 m. A distance of 200 m or more is preferred.
- 5 The value of the speed limit indicated on a roadway must be a realistic safe speed taking into account the roadway alignment, surface condition, traffic volumes and proximity of roadside obstacles including road workmen. In assessing a reduced speed limit authorities should consider the drivers' perception of the roadway conditions. If the reasons for the reduction in speed limit are not obvious consideration should be given to supplementing the speed limit sign with an appropriate message such as a warning sign, or a TOWN or CITY NAME sign GL3. The latter combination need not automatically be located at the town boundary if the area is not subdivided into erven.
- 6 Sign R201 should be displayed on the left-hand side of the roadway at a point where the speed limit is to commence. It is recommended that a second sign be provided on the right-hand side of one-way roadways, including carriageways of a dual carriageway roadway, which are demarcated into two or more lanes.
- 7 Sign R201 may be displayed on the same post as a MINIMUM SPEED regulatory sign R101 with sign R201 above sign R101.
- 8 It is recommended that a sign R201 be located approximately 450 m beyond the end of a freeway on-ramp taper when significant volumes of entering traffic occur and particularly beyond the junction of two freeway systems. When roads with different speed limits intersect it is recommended that appropriate SPEED LIMIT signs R201 be placed 60 m to 200 m beyond the junction on each exit roadway.
- 9 Temporary sign TR201 may be used under the same circumstances as permanent SPEED LIMIT signs. A permanent SPEED LIMIT sign R201 indicating the appropriate general speed limit should be displayed at the end of a roadworks site in which the speed limit has been reduced.
- 10 SPEED LIMIT signs R201 or TR201 may be incorporated into a guidance sign or a HIGH VISIBILITY back-ground to indicate that a route, or lane of a roadway, is subject to a speed limit which is different to other adjacent route(s) or lane(s) (see Section 2.8).
- 11 It is not generally recommended that signs indicating reduced speed limits be displayed for short sections of roadway. If a local condition requires reduced speed the use of a supplementary information plate giving an advisory speed, combined with an appropriate warning sign, is recommended. However, in roadworks situations where localised conditions relating to safety, particularly of workers, is a primary consideration, reduced localised speed limits may be applied in conjunction with an appropriate warning sign.
- 12 Signs R201 and TR201 should be sized in accordance with Table 2.4.

MASS LIMIT

For dimensions
Ref. Vol 4
pages

2.4.3
2.4.4



COLOURS:

PERMANENT

Border:
Legend:

Red retroreflective
Black semi-matt
"BMOD" and
T: DIN "B"

Background:
TEMPORARY

White retroreflective

Border:
Legend

Red retroreflective
Black semi-matt
"BMOD" and
T: DIN "B"

Background:

Yellow retroreflective

2.4.2 Mass Limit

- 1 The MASS LIMIT regulatory sign R202 imposes a **mandatory requirement that drivers of vehicles with a mass in excess of the mass indicated in tonnes, by means of a number on the sign, shall not proceed beyond such sign on a public road. The value indicated in tonnes is the gross vehicle mass (GVM) or gross combination mass (GCM) of the vehicle** (see paragraph 2.4.2.6). Sign R202 may be displayed as a SELECTIVE RESTRICTION sign in conjunction with a secondary message indicating a class of vehicle to which the mandatory requirement applies (see Section 2.7). If all goods vehicles are prohibited NO GOODS VEHICLES sign R229 should be used. If it is required to prohibit only goods vehicles exceeding a certain GVM a NO GOODS VEHICLES OVER INDICATED GVM sign R230 should be used.
- 2 Sign R202 should be displayed to indicate the point on a roadway from where the mass prohibition is to apply. The sign should only be displayed just beyond a point on the roadway where a vehicle exceeding the mass limit indicated may leave the roadway onto an alternative route. When ordering signs it is recommended that the mass limit value be indicated after the sign number e.g. R202- 12 for a PERMANENT mass limit of 12 tonnes.
- 3 The sign should be displayed on the left-hand side of the roadway. On dual-carriageway roadways the sign shall be repeated on the right-hand side of the carriageway. A second sign shall also be used on the right-hand side of a one-way roadway which is demarcated into two or more lanes.
- 4 Sign R202 may be used in conjunction with a range of COMMAND signs or SELECTIVE RESTRICTION COMMAND signs to control access by all heavy vehicles (including buses) to compulsory stops, weigh stations or vehicle inspection areas.
- 5 Temporary regulatory sign TR202 may be used under the same circumstances as permanent MASS LIMIT regulatory signs when temporary roadworks or other conditions require.
- 6 A MASS LIMIT sign R202 may be incorporated into the sign face of a guidance sign or a HIGH VISIBILITY background to indicate that a route, or lane of a roadway, is subject to a vehicle mass limit which is different to other adjacent route(s) or lane(s) (see Section 2.8). When used on a direction sign the overall sign face should include a distance message indicating the distance to the point of prohibition, or a distance message indicating the distance for which the prohibition applies, or an arrow to indicate the lane to which the prohibition applies.
- 7 It may occasionally be necessary to indicate to drivers that a maximum permissible vehicle mass restriction exists some considerable distance away. In this case signs R202 or TR202 may be combined with a SUPPLEMENTARY PLATE sign IN11.4 which should preferably include the name of the geographical location of the restriction and the distance to it, e.g. "At Oliviershoek Pass in 85 km". When displayed in this manner signs R202 or TR202 shall not apply to the point of roadway at which they are located. Such signs should be displayed in advance of an optional route selection point (junction or ramp terminal), or where suitable turning facilities exist (see Section 2.8).
- 8 Signs R202 and TR202 should be sized in accordance with Table 2.4.

AXLE MASSLOAD LIMIT

COLOURS:

PERMANENT

Border: Red retroreflective
 Legend & symbol: Black semi-matt "BMOD" and t:DIN"B"

Background: White retroreflective

TEMPORARY

Border: Red retroreflective
 Legend & symbol: Black semi-matt "BMOD" and t:DIN"B" Background: Yellow retroreflective

R203-2



For dimensions ref. Vol4 page

2.4.5

TR203-2



2.4.3 Axle Massload Limit

- 1 The AXLE MASSLOAD LIMIT regulatory sign R203 imposes **a mandatory requirement that drivers of vehicles with an axle massload in excess of the mass indicated in tonnes by means of a number on such sign, shall not proceed beyond such sign on a public road. The value indicated in tonnes is the gross axle massload (GA) or the gross axle unit massload (GAU) of the vehicle.**
- 2 Sign R203 should be displayed to indicate the point on a roadway from where the axle load prohibition is to apply. The sign should only be displayed beyond a point on the roadway where a vehicle exceeding the maximum permissible axle load indicated may leave the roadway onto an alternative route or where the

vehicle may turn around. When ordering signs it is recommended that the sign be ordered stating the axle load limit after the sign number e.g. R203 - 2 for a PERMANENT 2 tonne axle load limit sign.

- 3 This sign should be displayed on the left-hand side of the roadway.
- 4 Temporary regulatory sign TR203 may be used under the same circumstances as permanent AXLE MASSLOAD LIMIT regulatory signs when temporary road- works or other conditions require.
- 5 Signs R203 and TR203 should be sized in accordance with Table 2.4.

HEIGHT LIMIT

For dimensions
ref. Vol4
page

2.4.6



COLOURS:

PERMANENT

Border: Red retroreflective
Legend & symbol: Black semi-matt
"BMOD"

Background: White retroreflective

TEMPORARY

Border: Red retroreflective
Legend & symbol: Black semi-matt
"BMOD"

Background: Yellow retroreflective

2.4.4 Height Limit

- 1 The HEIGHT LIMIT regulatory sign R204 imposes a **mandatory requirement that drivers of vehicles on a public road shall not proceed beyond the sign or drive under a height gauge or structure to which the sign is attached unless the height of the vehicle, including any load thereon, and, in the case of a height gauge, any radio antenna attached to the vehicle, is less than the clearance height indicated in metres by means of a number on such sign.**
- 2 When required in terms of paragraph 2.4.4 sign R204 should be displayed on, and immediately in advance of:
 - (a) a height gauge located ahead of a railway crossing over which overhead electrical power cables are installed;
 - and
 - (b) an overhead structure.
- 3 When displayed on a height gauge or an overhead structure sign R204 shall be flanked by two OVERHEAD DANGER PLATE signs W415 (see Subsection 3.5.9 and Volume 2, Chapter 3 and Chapter 7).
- 4 When displayed immediately in advance of a height gauge or structure, sign R204 should be displayed on the left-hand side of the roadway.
- 5 Sign R204 shall be used when the clearance height over any part of the full width of roadway is less than 4,7 m and is recommended for use when the clearance height over any part of the full width of the roadway is less than 5,2 m. The actual clearance height, less a safety allowance of at least 75 mm, should be shown to two decimal places of a metre, and rounded down to the second decimal.
- 6 Advance warning of the height limit should be given by the use of the HEIGHT RESTRICTED warning sign W320 as provided in Subsection 3.4.16. A typical sign arrangement for a height restricted site is given in Volume2.
- 7 Temporary regulatory sign TR204 may be used under the same circumstances as permanent HEIGHT LIMIT regulatory signs when temporary roadworks or other conditions require.
- 8 It may occasionally be necessary to indicate to drivers that a height limit exists some considerable distance away. In this case signs R204 or TR204 may be combined with a SUPPLEMENTARY PLATE sign IN11.4 which should preferably include the name of the geographical location of the restriction and the distance to it e.g. "At Nottingham Road in 8 km". When displayed in this manner, signs R204 or TR204 shall not apply to the point of roadway at which they are located. Such signs should be displayed in advance of an optional route selection point (junction or ramp terminal), or where suitable turning facilities exist (see Section 2.8).
- 9 Signs R204 and TR204 should be sized in accordance with Table 2.4.

LENGTH LIMIT

COLOURS:

PERMANENT

Border: Red retroreflective

Legend & symbol: Black semi-matt
"BMOD" and
m:"DIN"B"

Background: White retroreflective

TEMPORARY

Border: Red retroreflective

Legend & symbol: Black semi-matt
"BMOD" and
m:"DIN"B"

Background: Yellow retroreflective

For dimensions
ref. Vol4
page

2.4.7

R205



TR205



2.4.5 Length Limit

- 1 The LENGTH LIMIT sign R205 imposes **a mandatory requirement that drivers of vehicles which are in excess of the length indicated in metres by means of a number on such sign shall not proceed beyond such sign on a public road. The value indicated in metres on such sign is the maximum length of the vehicle or combination of vehicles, including any projections.**
- 2 Sign R205 should be displayed to indicate the point on a roadway from where the length limit is to apply. The sign should only be displayed just beyond a point on the roadway where a vehicle exceeding the maximum permissible length may leave the roadway onto an alternative route.
- 3 The sign should be displayed on the left-hand side of the roadway.
- 4 Sign R205 should be used to indicate where the road alignment and/or width is such that long vehicles will overhang the inner edge of the roadway on left-hand curves and the centre line on right-hand curves to the extent that the vehicle will risk touching side obstructions or cause an obstruction to oncoming traffic. The maximum permissible length to be displayed should be determined by an engineering assessment and should be rounded down to the full metre length indication closest to the assessed value.
- 5 Advance warning of the length limit should be given by the use of LENGTH RESTRICTED warning sign W321 as provided in Subsection 3.4.17.
- 6 Temporary regulatory sign TR205 may be used under the same circumstances as permanent LENGTH LIMIT regulatory signs when temporary roadworks or other conditions require.
- 7 It may occasionally be necessary to indicate to drivers that a length limit exists some considerable distance away (see Subsection 2.4.2 paragraph 2.4.2.7).
- 8 Signs R205 and TR205 should be sized in accordance with Table 2.4.

NO EXCESSIVE NOISE

For dimensions
ref. Vol4
page

2.4.8

R206



COLOURS:

PERMANENT

Border:

Red retroreflective

Symbol:

Black semi-matt

Background:

White retroreflective

TEMPORARY

Border:

Red retroreflective

Symbol:

Black semi-matt Yellow

Background:

retroreflective

2.4.6 No Excessive Noise

- 1 The NO EXCESSIVE NOISE regulatory sign R206 imposes a **mandatory requirement that drivers shall NOT sound their vehicle sounding device or hooter for 100m after passing sign, AND sign R206 imposes a mandatory requirement that drivers shall not drive a vehicle beyond such sign, if such vehicle is emitting noise in excess of noise levels prescribed.**
- 2 Sign R206 should be displayed on the left-hand side of the roadway approximately 50 m in advance of the place for which the prohibition is being implemented. The sign may be used in the vicinity of places of worship, hospitals or homes for invalids or the aged. If a more general restriction is to be imposed in terms of prescribed noise levels, sign R206 should be located on the left side of the roadway just beyond a point on the roadway where a driver of a vehicle exceeding the prescribed noise level may leave the roadway onto an alternative route.
- 3 Regulations may be promulgated or amended from time to time to prescribe acceptable noise levels including those emitted by vehicles and motor vehicles. Sign R206 should not be used in terms of such regulations if there is no intention to enforce compliance with the regulations.
- 4 If it is required to impose the excessive noise prohibition for distances greater than 100 m sign R206 should be supplemented by an appropriate SUPPLEMENTARY PLATE sign IN11.2.
- 5 A temporary version of the NO EXCESSIVE NOISE regulatory sign is unlikely to be required.
- 6 Sign R206 should be sized in accordance with Table 2.4.

NO HITCH-HIKING

For dimensions
ref. Vol4
pages

2.4.9

R207



COLOURS:

PERMANENT

Border:

Red retroreflective

Symbol:

Black semi-matt White

Background:

retroreflective

TEMPORARY

Border:

Red retroreflective

Symbol:

Black semi-matt Yellow

Background:

retroreflective

2.4.7 No Hitch-hiking

- 1 The NO HITCH-HIKING regulatory sign R207 imposes a **mandatory requirement that pedestrians shall NOT attempt to secure a lift from a passing vehicle AND sign R207 imposes a mandatory requirement that drivers of vehicles shall NOT pick-up passengers. The prohibition on hitch-hiking is effective for a distance of 500 m beyond such sign.**
- 2 Sign R207 should be displayed on the left-hand side of the roadway to indicate the point from which the prohibition on hitch-hiking shall apply.
- 3 If the length of prohibition required exceeds 500 m the sign shall be repeated at intervals of 500 m. The first sign in the sequence may carry a SUPPLEMENTARY PLATE sign IN11.2 indicating "For 5 km".
- 4 If the section of roadway subject to a prohibition on hitch-hiking is known to have points of pedestrian activity care should be taken to ensure that R207 signs are placed in such a manner that pedestrians can reasonably be expected to see them as well as drivers. This may require signs at closer spacings than 500 m.
- 5 The use of freeways by persons on foot is prohibited in South Africa by the Road Traffic Act, Act No 93 of 1996, except:
 - (a) within an area indicated by an appropriate road traffic sign;
 - (b) for a cause beyond their control;
 Sign R207 should not therefore be used on freeways.
- 6 A temporary version of sign R207 is unlikely to be required.
- 7 Sign R207 should be sized in accordance with Table 2.4.

NO UNAUTHORISED VEHICLES

COLOURS:

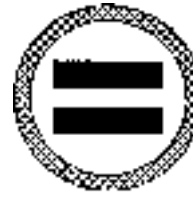
PERMANENT

Border: Red retroreflective
 Symbol: Black semi-matt White
 Background: retroreflective

TEMPORARY

Border: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

R208



For dimensions
 ref. Vo14
 page

2.4.10

TR208



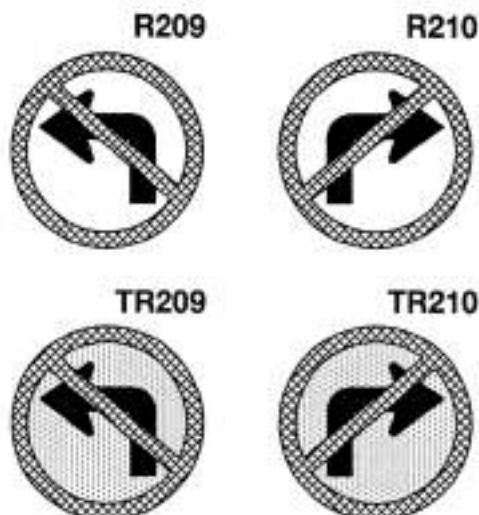
2.4.8 No Unauthorised Vehicles,

- 1 The NO UNAUTHORISED VEHICLES regulatory sign R208 imposes ***amandatory requirement that drivers of unauthorised vehicles shall NOT proceed beyond such sign.*** Use of the sign in its temporary form is particularly appropriate to temporary road closures as a result of roadworks.
- 2 Sign R208 should be used to indicate that a roadway or entrance way is closed to general traffic. **In the event of prosecution the onus shall rest with the driver of a vehicle to prove that he has authority from the responsible authority to proceed beyond signs R208 or TR208. This authorisation should be indicated by the display of an appropriate identity/authorisation disc or other device.** Authorised traffic may be permitted to proceed beyond the sign in order to gain access to private property or a work site.
- 3 It is recommended that signs be located on both the left-hand and right-hand sides of the roadway or entrance.
- 4 Temporary regulatory sign TR208 may be used under the same circumstances as a permanent NO UNAUTHORISED VEHICLES regulatory sign when temporary roadworks or other conditions require that a roadway or entrance be closed to normal traffic other than authorised vehicles. In such a case authorised vehicles will normally include construction vehicles and those belonging to local residents.
- 5 Sign TR208 should not be used to indicate closure of a portion of a roadway such as a shoulder or lane if traffic flow is maintained through the section of roadway.
- 6 When used to effect a temporary road closure to unauthorised vehicles, sign TR208 should be combined with a T-JUNCTION CHEVRON hazard marker sign TW409, a DEAD END/ROAD CLOSED CHEVRON hazard marker sign TW410 or a TEMPORARY BARRICADE hazard marker sign TW411.
- 7 Signs R208 and TR208 should be sized in accordance with the provisions of Table 2.4.

NO LEFT TURN AHEAD - NO RIGHT TURN AHEAD

For dimensions
ref. Vol4
page

2.4.11
2.4.12



COLOURS:

PERMANENT

Border & diagonal: Red retroreflective

Arrow: Black semi-matt

Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective

Arrow: Black semi-matt

Background: Yellow retroreflective

2.4.9 No Left Turn Ahead and No Right Turn Ahead

- 1 The NO LEFT TURN AHEAD and NO RIGHT TURN AHEAD regulatory signs R209 and R210 impose a **mandatory requirement that drivers of vehicles shall NOT turn to the left or the right as the case may be, at the junction or entrance ahead.** Signs R209 and R210 may be displayed as SELECTIVE RESTRICTION signs in conjunction with a secondary message indicating a class of vehicle to which the mandatory requirement applies OR the time of day for which it applies (see Section 2.7). If the prohibition requires a vehicle class secondary message but it only applies during certain hours the use of a variable message road traffic sign is recommended so that the appropriate sign shall only be displayed when the prohibition applies and at all other times NO SIGN should be visible.
- 2 Signs R209 and R210 shall only be displayed in advance, on an approach to a junction where traffic is prohibited from making a turn in the direction indicated. When used, signs R209 and R210 may be followed by NO LEFT TURN or NO RIGHT TURN signs R211 or R212 at the junction, although signing of one way roadways should preferably be by use of ONE WAY ROADWAY signs R4.1 or R4.2 (see Subsection 2.2.5).
- 3 Signs R209 and R210 may be used in advance of an intersecting one-way cross-road where ONE WAY ROADWAY signs R4.1 or R4.2 are displayed when it is important that drivers become aware that the intersecting roadway ahead is a one-way roadway well in advance of the junction. In addition signs R209 and R210 may be used to reduce congestion or collisions by prohibiting left or right turn movements even though the intersecting roadway is not part of a one-way system.
- 4 The signs should normally be displayed on the left hand side of the approach roadway between 15 m and 30 m from the junction to which they apply, provided that in the case of a one-way approach roadway it may be more appropriate to locate the sign on the right-hand side of the roadway.
- 5 It may be advisable to elevate the signs above the normal mounting height to improve visibility and thereby correct lane selection in a one-way system. Care shall be taken not to locate a sign so that a public access point lies between the sign and the junction ahead.
- 6 Temporary regulatory signs TR209 and TR210 may be used under the same circumstances as permanent NO LEFT TURN AHEAD and NO RIGHT TURN AHEAD regulatory signs particularly if a temporary detour is required during roadworks or other conditions.
- 7 Signs R209, R210, TR209, and TR210 should be sized in accordance with Table 2.4.

NO LEFT TURN – NO RIGHT TURN

COLOURS:

PERMANENT

Border & diagonal: Red retroreflective
 Arrow: Black semi-matt
 Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective
 Arrow: Black semi-matt
 Background: Yellow retroreflective

For dimensions
 ref. Vol 4
 pages



2.4.13
 2.4.14

2.4.10 No Left Turn and No Right Turn

- 1 The NO LEFT TURN and NO RIGHT TURN regulatory signs R211 and R212 impose **a mandatory requirement that drivers of vehicles shall NOT turn to the left or to the right as the case may be.**
- 2 Signs R211 and R212 may be displayed at a junction or entrance to indicate that a turn to the left or a turn to the right is prohibited, as the case may be. Signs R211 and R212 should normally be preceded by NO LEFT TURN AHEAD and NO RIGHT TURN AHEAD signs R209 and R210 respectively.
- 3 Signs R211 and R212 should not be used at junctions of one way streets in place of ONE WAY ROADWAY signs R4.1 or R4.2. However, if some particular condition exists whereby signs R4.1 or R4.2 are deemed inadequate on their own, signs R211 or R212 may be used to supplement them.
- 4 A NO LEFT TURN sign R211 may be used at a

channelised junction when a left turning roadway (sliproad) is provided. It may be used to prohibit left turns beyond the turning roadway at the actual junction of the principal intersecting roadways in addition to MANDATORY DIRECTION ARROW road markings RMB. This point will normally be the corner of a kerbed channelising island. When used under these circumstances this kerbed island should have a side length on the approach controlled by the sign R211 of at least 6 m.

- 5 Temporary regulatory signs TR211 and TR212 may be used under the same circumstances as permanent NO LEFT TURN and NO RIGHT TURN regulatory signs during roadworks or other temporary conditions where a detour of traffic may be required.
- 6 Signs R211, R212, TR211 and TR212 should be sized in accordance with Table 2.4.

NO U-TURN

COLOURS:

PERMANENT

Border & diagonal: Red retroreflective
 Arrow: Black semi-matt
 Background: White retroreflective

For dimensions
 ref. Vol4
 page

R213



2.4.15

2.4.11 No U-Turn

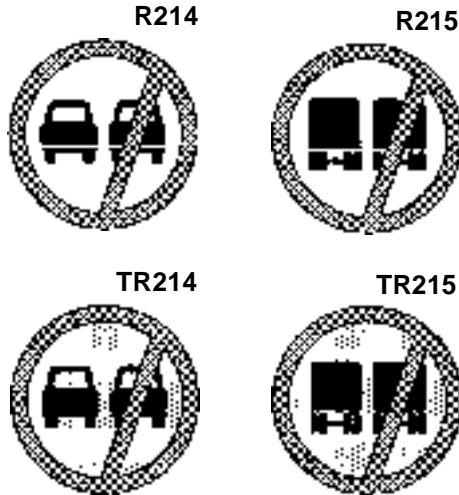
- 1 The NO U-TURN regulatory sign R213 imposes **a mandatory requirement that drivers of vehicles shall NOT turn their vehicles so as to face in the opposite direction.** This has the effect that drivers shall not turn their vehicles to face in the opposite direction within a public road, a junction or an opening in a median island of a dual carriageway roadway.
- 2 Sign R213 may be displayed in advance of a junction or on the far side of a junction. When placed in advance of a junction or median island opening it should be located within 15 m of the junction or median island opening.

- 3 Sign R213 should be located with care to ensure that the sign will be easily seen by drivers who may attempt to undertake a U-turn manoeuvre. The sign will commonly be located on a median island when such islands exist. It may be advisable to elevate the signs above the normal mounting height to improve visibility.
- 4 A temporary version of sign R213 is unlikely to be required.
- 5 Sign R213 should be sized in accordance with Table 2.4.

NO OVERTAKING

For dimensions
ref. Vol4
pages

2.4.16
2.4.17



COLOURS:
PERMANENT

Border: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

Border: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

2.4.12 No Overtaking

- The NO OVERTAKING regulatory sign R214 ALL VEHICLES, - imposes a **mandatory requirement that drivers of vehicles shall NOT overtake another vehicle travelling in the same direction**, and the NO OVERTAKING regulatory sign R215- GOODS VEHICLES BY GOODS VEHICLES, - imposes a **mandatory requirement that drivers of goods vehicles shall NOT overtake another goods vehicle travelling in the same direction. The prohibition on overtaking indicated by signs R214 and R215 is effective for a distance of 500m beyond such signs.** Signs R214 and R215 may be displayed as SELECTIVE RESTRICTION signs in conjunction with a secondary message (see Section 2.7).
- On permanently surfaced roadways a NO OVERTAKING LINE marking RM1 or a NO CROSSING LINE marking RM2 will commonly be used to prohibit vehicles from travelling on the right-hand side of the line in order to overtake other vehicles travelling in the same direction. Such line markings should be used when it is deemed unsafe to permit overtaking manoeuvres. For additional emphasis, or where the overtaking of one vehicle by another vehicle, without the need to cross a barrier line is also to be prohibited i.e. - on a steep or congested multi-lane roadway, the relevant sign should be displayed on the left side of the roadway at the beginning of the prohibition section.
- The signs may be displayed on gravel-surfaced roadways to indicate an overtaking prohibition which cannot be indicated by means of a barrier line marking. Determination of sections of such roadways where overtaking could be hazardous may be made by the same techniques which are used to determine the need for a barrier line marking (see Chapter 7).
- Sign R214 should be displayed to indicate a general overtaking prohibition applicable to all classes of vehicle permitted to use the roadway and travelling in the same direction.
- Sign R215 may be displayed to indicate a prohibition on goods vehicles overtaking goods vehicles. Use of the sign should be restricted to roadways with two lanes for traffic travelling in the same direction where the roadway gradient will result in a very slow overtaking manoeuvre of a goods vehicle by another goods vehicle. A more costly, but preferred, method of signing this requirement may be achieved by the use of regulatory signs such as the MINIMUM SPEED sign R101 (or TR101) or the NO GOODS VEHICLES sign R229 (or TR229) on a diagrammatic guidance sign (see Subsection 2.8.4).
- If the length of prohibition required exceeds 500 m the appropriate sign shall be repeated at intervals of 500 m. In such cases the first sign in the sequence may carry a SUPPLEMENTARY PLATE sign IN11.2 indicating "For 3 km". A similar message may be displayed on subsequent signs. The distance value should be reduced as appropriate (see Subsection 2.8.2 and Chapter 5).
- Temporary regulatory signs TR214 and TR215 may be used under the same circumstances as permanent NO OVERTAKING regulatory signs when roadworks or other conditions create circumstances which make it unsafe to permit overtaking.
- Care should be taken not to use these signs unnecessarily. Such practices will only result in disrespect for the prohibition with the result that signs used correctly in the interests of road safety may not be observed.
- Signs R214, R215, TR214 and TR215 should be sized in accordance with Table 2.4.

NO PARKING

COLOURS:

PERMANENT

Border & diagonal: Red retroreflective
 Legend: Black semi-matt,
 Background: Blue retroreflective

TEMPORARY

Border & diagonal: Red retroreflective
 Legend: Black semi-matt,
 Background: Yellow retroreflective

R216



For dimensions
 Ref. Vol4
 pages

2.4.18

TR216



2.4.13 No Parking

- 1 The NO PARKING regulatory sign R216 imposes a **mandatory requirement that drivers of vehicles shall NOT park their vehicles at any time on any portion of a public road, as indicated by such sign.** The applicability of sign R216 may be modified by being displayed as a SELECTIVE RESTRICTION sign in conjunction with a secondary message indicating a class of vehicle, or the time or times of day, or the days of the week or the circumstances/activities for which the parking prohibition applies (see Section 2.7). It should be noted that this does not classify the sign as a temporary sign (see paragraph 2.4.13.12).
 - 2 Sign R216 should be displayed where parking is prohibited for all vehicles at all times of day and night, OR in conjunction with a SELECTIVE RESTRICTION NO STOPPING sign **which overrides the NO PARKING sign** during the periods of applicability of the SELECTIVE RESTRICTION NO STOPPING sign. The NO PARKING sign is therefore applicable at all other times.
 - 3 The sign should only be used on sections of roadway where total prohibition of parking is considered essential, such as:
 - (a) on both sides of a roadway less than 5,5 m wide;
 - (b) on one or both sides of a narrow roadway with a width of more than 5,5 m;
 - (c) on one side of a roadway less than 9 m wide;
 - (d) on one or both sides of a busy two-lane two-way roadway;
 - (e) on the approaches to a major (multi-lane) junction;
 - (f) on the approaches to a signal-controlled junction.

If the prohibition on parking is only for certain times of day or for a certain class of vehicle a SELECTIVE RESTRICTION version of R216 shall be used.
 - 4 Sign R216 may be supplemented by a NO PARKING LINE marking RM13 as detailed in Chapter 7.
 - 5 Legislation makes provision for a range of general parking prohibitions. Legislation also empowers local authorities to make by-laws which may place further general prohibitions on the stopping or parking of vehicles. Such general provisions need not be indicated by sign R216 although they may be over-ruled by the display of an appropriate road traffic sign.
 - 6 The Act and relevant by-laws shall be carefully studied
- 7 NO PARKING signs R216 shall be located at the beginning and end of each section of public road and on the side of such section to which they are to be applicable. Such a section of roadway shall not include any junction with another public road. If the length of the section exceeds 150m additional R216 signs shall be provided so that the maximum distance between signs does not exceed 150 m.
 - 8 When the length of a section of public road between junctions does not exceed 75 m parking may be prohibited for such section by displaying a single R216 sign in the approximate centre of the section subject to the provisions of paragraph 2.4.13.10.
 - 9 R216 signs may be located on any other portion of public road to which their significance is required to be applied.
 - 10 Sign R216 shall be displayed at right angles to the direction of travel of traffic in a roadway and shall show the same sign on the reverse side.
 - 11 If a section of roadway is signed with NO STOPPING regulatory signs R217, that section of roadway shall not also be signed with NO PARKING regulatory signs R216. This requirement does not apply to the use of SELECTIVE RESTRICTION versions of signs R216 and R217.
 - 12 Temporary regulatory sign TR216 may be used under the same circumstances as permanent NO PARKING regulatory signs when temporary roadworks or adjacent building construction sites require. Temporary NO PARKING signs may be required where normal parking is no longer possible for any reason on a temporary basis. These signs should also form part of the equipment carried by local authority units, police units or security force units who may on occasions have to erect temporary barricades at emergency sites. In the event that permanent PARKING signs are not removed, temporary NO PARKING signs shall supersede any such signs.
 - 13 Temporary NO PARKING signs TR216 may be used with SUPPLEMENTARY PLATE signs TIN11.4 to override other parking signs on a temporary basis. The

SUPPLEMENTARY PLATES may display such messages as "During Building Operations" or "During Sports Meeting".

- 14 The extent of the area/section of public road to which NO PARKING or SELECTIVE RESTRICTION NO PARKING signs may be applicable may be clarified by the display of arrow symbols on SUPPLEMENTARY PLATE sign IN11.5 mounted below the signs and at 90° to the sign R216. The arrow symbols should face the roadway and should point inwards from the limit of the NO PARKING area. The arrow symbol below an intermediate R216 sign on a section of public road over 150 m in length should point in both directions, as should the arrow symbol when used on a sign placed approximately in the centre of a section of public road that does not exceed 75 m between junctions.

- 15 A minimum sign size of 450 mm diameter may be used in urban areas. Miniature R216 signs of 150 mm diameter may be used in their SELECTIVE RESTRICTION form on parking meter posts to supplement the message of the full size signs provided in the section of public road.
- 16 It should be noted that **BUS STOP RESERVATION sign R325 and MINIBUS STOP RESERVATION sign R326, as regulatory-signs override the provisions of NO PARKING sign R216 and NO STOPPING sign R217, without the need to qualify signs R216 or R217 with a supplementary plate indicating "EXCEPT BUSES"**.

NO STOPPING

For dimensions
ref. Vo14
page

2.4.19



COLOURS:

PERMANENT

Border & diagonal: Red retroreflective

Legend: Black semi-matt,

Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective

Legend: Black semi-matt,

Background: Yellow retroreflective

2.4.14 No Stopping

- 1 The NO STOPPING regulatory sign R217 imposes a **mandatory requirement that drivers of vehicles shall NOT stop their vehicles at any time on any portion of public road, as indicated by such sign**. Such a requirement does not apply if a driver is acting in compliance with a traffic signal or the signal of a traffic officer or to avoid a collision. The applicability of sign R217 may be modified by being displayed as a SELECTIVE RESTRICTION sign in conjunction with a secondary message indicating a class of vehicle, or the time or times of day, or the days of the week, or circumstances/activities for which the stopping prohibition applies (see Section 2.7). It should be noted that this does not classify the sign as a temporary sign (see paragraph 2.4.14.12).
- 2 Sign R217 should be displayed where stopping is prohibited for all vehicles at all times of day and night.
- 3 The sign should only be used on sections of roadway where the total prohibition of stopping is considered essential, such as:
- a narrow roadway carrying heavy traffic during both day and night;
 - the kerb-side of a lane that is allocated exclusively to a single-turning movement;
 - a short section of urban arterial road where it may be hazardous for vehicles to stop for purposes such as setting down or picking up passengers;
- (d) within 6m of any tunnel, subway, or bridge;
- (e) where it is necessary to increase traffic capacity; or
- (f) where sight distances will be adversely affected by vehicles stopping.
- 4 Sign R217, may be supplemented by a NO STOPPING LINE marking RM12 as detailed in Chapter 7.
- 5 Legislation and relevant by-laws should be carefully studied before designing for the provision of NO STOPPING signs R217 or their "part-time" SELECTIVE RESTRICTION variants. It should be noted that if NO STOPPING signs are displayed with NO PARKING signs the NO STOPPING sign overrides the NO PARKING sign. Thus a SELECTIVE RESTRICTION NO STOPPING sign overrides a (24 hr) NO PARKING sign during the periods of applicability of the SELECTIVE RESTRICTION NO STOPPING sign.
- 6 It is not necessary to supplement a NO STOPPING sign with a NO PARKING sign R216. This requirement does not apply to the use of SELECTIVE RESTRICTION versions of signs R216 and R217.
- 7 See also paragraphs 2.4.13.5, 2.4.13.7 to 2.4.13.10 and 2.4.13.12 to 2.4.13.16.

NO PEDESTRIANS

COLOURS:

PERMANENT

Border & diagonal: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

R218



For dimensions
 ref. Vol4
 page
 2.4.20

TR218



2.4.15 No Pedestrians

- 1 The NO PEDESTRIANS regulatory sign imposes a **mandatory requirement that pedestrians shall NOT proceed beyond such sign.**
- 2 Sign R218 should be displayed at the beginning of, and at entrances to, a roadway, lane or path to indicate that pedestrians are prohibited from using that roadway, lane or path. Sign R218 may be used to indicate to pedestrians that they should not cross a roadway at the point indicated by the sign. In the latter case it is recommended that additional signs be displayed to guide pedestrians to a safe crossing point. If sign R218 is used in this manner at a junction, the junction should not be marked with PEDESTRIAN CROSSING LINE marking RTM3 at that point.
- 3 A NO PEDESTRIANS sign shall not be displayed between a roadway and a pedestrian way at the commencement of a pedestrian way, when such a point

is signed with PEDESTRIANS ONLY sign R110 or one of the CYCLISTS AND PEDESTRIANS ONLY signs R112 to R115 (see Subsections 2.3.6 and 2.3.8).

- 4 A NO PEDESTRIANS sign shall not be displayed at the beginning of a cyclist only facility which is already signed with a CYCLISTS ONLY sign R111 (see Subsection 2.3.7).
- 5 Temporary regulatory sign TR218 may be used under the same circumstances as a permanent NO PEDESTRIANS regulatory sign when temporary roadworks or other conditions require that a roadway, lane or path normally available to pedestrians is temporarily not available.
- 6 Signs R218 and TR218 should be sized according to the provisions of Table 2.4.

NO CYCLISTS

For dimensions
ref. Vol4
page

2.4.21

R219



TR219



COLOURS:

PERMANENT

Border & diagonal: Red retroreflective

Symbol: Black semi-matt

Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective

Symbol: Black semi-matt

Background: Yellow retroreflective

2.4.16 No Cyclists

- 1 The NO CYCLISTS regulatory sign R219 imposes a **mandatory requirement that cyclists shall NOT proceed beyond such sign.**
- 2 Sign R219 should be displayed at the beginning of, and at entrances to, a roadway, lane or path to indicate that cyclists are prohibited from using that roadway, lane or path.
- 3 The sign should normally be used at the beginning of roadways where, for safety reasons, it is desirable to prohibit bicycle traffic.
- 4 A NO CYCLISTS sign shall not be displayed between a roadway and a cycle way at the commencement of a cycle way when such a point is signed with a CYCLISTS ONLY sign R111 or one of the CYCLISTS AND PEDESTRIANS ONLY signs R112 to R115 (see Subsections 2.3.7 and 2.3.8).
- 5 A NO CYCLISTS sign shall not be displayed at the beginning of a pedestrian only footpath which is already signed with a PEDESTRIANS ONLY sign R110 (see Subsection 2.3.6).
- 6 Temporary regulatory sign TR219 may be used under the same circumstances as a permanent NO CYCLISTS regulatory sign when temporary roadworks or other conditions require that a roadway, lane or path normally available to cyclists is temporarily not available.
- 7 Signs R219 and TR219 should be sized according to the provisions of Table 2.4.

NO CYCLISTS AND PEDESTRIANS

COLOURS:

PERMANENT

Border & diagonal: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

For dimensions
 ref. Vo14
 page

2.4.22

R220



TR220



2.4.17 No Cyclists and Pedestrians

- 1 The NO CYCLISTS AND PEDESTRIANS regulatory sign R220 imposes **a mandatory requirement that cyclists and pedestrians shall NOT proceed beyond such sign.**
- 2 Sign R220 should be displayed at the beginning of, and at entrances to, a roadway, lane or path to indicate that cyclists AND pedestrians are prohibited from using the roadway, lane or path. Sign R220 may be used to indicate to cyclists and pedestrians that they should not cross a roadway at the point indicated by the sign. In the latter case it is recommended that additional signs be displayed to guide cyclists and pedestrians to a safe crossing point.
- 3 A NO CYCLISTS AND PEDESTRIANS sign shall not be displayed between a roadway and a shared cyclist and pedestrian way at its commencement when such a point is signed with one of the CYCLISTS AND PEDESTRIANS ONLY signs R112 to R115.
- 4 Temporary regulatory sign TR220 may be used under the same circumstances as a permanent NO CYCLISTS AND PEDESTRIANS regulatory sign when temporary roadworks or other conditions require that a roadway, lane or path normally available to cyclists and pedestrians is temporarily not available.
- 5 Signs R220 and TR220 should be sized according to the provisions of Table 2.4.

2.4.19 Other Prohibition Signs

- 1 Other PROHIBITION regulatory signs referring to specific classes of vehicle impose **a mandatory requirement that drivers of the class of vehicle indicated on such sign shall NOT proceed beyond the sign.** The signs may be used as the primary component of a SELECTIVE RESTRICTION sign in which case their prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).
- 2 The signs shown on the following pages cover the following classes of vehicle:
 - R222 NO MOTOR CYCLES**
 - R223 NO MOTOR CARS**
 - R224 NO TAXIS**
 - R224 NO MINIBUSES**
 - R226 NO MIDI-BUSES**
 - R227 NO BUSES**
 - R228 NO DELIVERY VEHICLES** „
 - R229 NO GOODS VEHICLES**
 - R230 NO GOODS VEHICLES OVER INDICATED GVM**
 - R231 NO CONSTRUCTION VEHICLES**
 - R232 NO VEHICLES CONVEYING DANGEROUS GOODS**
 - R233 NO ABNORMAL VEHICLES**
 - R234 NO RICKSHAWS**
 - R235 NO TOUR BUSES**
 - R236 NO AGRICULTURAL VEHICLES**
 - R237 NO ANIMAL- DRAWN VEHICLES**
 - R238 NO HORSES AND RIDERS**
 - R239 WIDTH LIMIT**
 - R240 NO TOWED VEHICLES**
 - R241 NO HAWKERS**
 - R242 NO MOTOR VEHICLES**
- 3 Any of the above signs, when used, should be displayed at the beginning of or at the entrance to a section of roadway, lane or path to indicate that the class of vehicle shown on the sign is prohibited from using such roadway, lane or path.
- 4 Sign R222 NO MOTOR CYCLES may be used:
 - (a) at the entrance to pedestrian lanes;
 - (b) at the entrance to a separate cycle way;
 - (c) where for road safety reasons it is desirable to prohibit motor cycle traffic on a particular section of roadway e.g. due to narrow traffic lanes.
- 5 It may be necessary to give motor cyclists an advance indication of a prohibition on motor cycles ahead of them on the road. Sign R222 may be incorporated into a map-type or diagrammatic sign with a distance indication to the point of prohibition. The minimum advance indication should be an R222 sign combined with a SUPPLEMENTARY PLATE sign IN11.3 giving the distance in metres to the point of prohibition. The point at which motor cyclists should leave the roadway in question may be signed using a MOTOR CYCLES KEEP LEFT SELECTIVE RESTRICTION sign R103-562 (see Section 2.7). It may be necessary to provide special guidance signs to help motor cyclists reach a parallel or suitable alternative route.

Sign R222 should be displayed on the left side of the roadway. On one way carriageways the sign should be repeated on the right-hand side of the roadway.
- 6 The use of signs R222 to R242 is likely to be rare. However, as the provision of facilities, segregated by vehicle class, becomes more appropriate to major transport terminals and -other transport and complex commercial development, the use of such signs will enable improved control and direction of traffic by class of vehicle.
- 7 Prohibition signs R222 to R242 may be incorporated into the signface of a guidance sign to indicate that a route, or lane of a roadway is prohibited to the class of vehicle displayed on the sign. When used on a direction sign the overall signface should include a distance message indicating the distance to the point of prohibition, and thereby to an alternative route.
- 8 Signs R222 to R242 should be used with care to ensure that they do not become over-utilised or incorrectly utilised, thereby reducing their effectiveness when re- ally required.
- 9 Temporary regulatory versions of signs R222 to R242 are unlikely to be required. Should temporary versions be required those should be numbered in the TR200 series from TR222 to TR242 and should be manufactured with a black semi-matt symbol on a yellow retroreflective background and a red retroreflective circular border and diagonal slash. Such signs have the significance that **for a temporary period the class of vehicle indicated must NOT proceed beyond the sign.**
- 10 The size and positioning of signs R222 to R242 and TR222 to TR242 should follow the general requirements for regulatory signs. They should be sized in accordance with Table 2.4.
- 11 This Subsection includes details on three signs specific to public transport vehicles, namely, NO MINIBUSES sign R225, NO MIDI-BUSES sign R226 and NO BUSES sign R227. The relevant feature used to classify these vehicles for signing purposes is the number of seats. Details of these classifications are given in Chapter 10. Signing for the control of buses has occurred for some time. However, control of mini-buses is as yet not fully developed and midi-buses are new additions to the range of public transport vehicle types. Whilst the signs provided conform to the general principles of the sign group, their application is likely to develop with time (see also Volume 2).
- 12 Signs R228 to R233 refer to various classes of goods vehicle. Signs R228, R231, and R232 refer to specific types of heavy vehicle and apply to all such vehicles over 3500 kg mass. If it is required to link a specific mass limit to these signs this may be achieved by using a SELECTIVE RESTRICTION sign incorporating a MASS LIMIT sign R202 as the primary message and the appropriate vehicle class symbol as a secondary message (see Section 2.7).
- 13 The NO GOODS VEHICLES sign R229 refers to all trucks over a mass of 3500 kg including those covered by signs R228, R231 and R232 but excluding buses. NO GOODS VEHICLES OVER INDICATED GVM sign R230 similarly applies to all trucks, but not buses, which exceed the mass indicated on the sign.

Sign R222 should be displayed on the left side of the roadway. On one way carriageways the sign should be

NO MOTOR CYCLES

NO MOTOR CARS

For dimensions
ref. Vol. 4
page

2.4.23

R222



COLOURS:
PERMANENT

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

TR222



For dimensions
ref. Vol. 4
page

2.4.24

R223



COLOURS:
PERMANENT

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

TR223



The NO MOTOR CYCLES regulatory sign R222 imposes a **mandatory requirement that drivers of motor cycles shall NOT proceed beyond such sign** (see paragraph 2.4.19.3).

The NO MOTOR CARS regulatory sign R223 imposes a **mandatory requirement that drivers of motor cars shall NOT proceed beyond such sign**. The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).

NO TAXIS

NO MINIBUSES

For dimensions
ref. Vol. 4
page

2.3.25

R224



COLOURS:
PERMANENT

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

TR224



For dimensions
ref. Vol. 4
page

2.4.26

R225



COLOURS:
PERMANENT

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

TR225



The NO TAXIS regulatory sign R224 imposes a **mandatory requirement that drivers of taxis shall NOT proceed beyond such sign**. The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).

The NO MINIBUSES regulatory sign R225 imposes a **mandatory requirement that drivers of minibuses shall NOT proceed beyond such sign**. The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).

NO MIDI-BUSES

R226



For dimensions
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page

2.4.27

TR226



COLOURS:
PERMANENT
Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective
TEMPORARY
Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

The NO MIDI-BUSES regulatory sign R226 imposes **a mandatory requirement that drivers of midi-buses shall NOT proceed beyond such sign**. The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).

NO BUSES

R227



For dimensions
Ref. Vol 4
page

2.4.28

TR227



COLOURS:
PERMANENT
Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective
TEMPORARY
Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

The NO BUSES regulatory sign R227 imposes **a mandatory requirement that drivers of buses shall NOT proceed beyond such sign**. The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).

NO DELIVERY VEHICLES

R228



For dimensions
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page

2.4.29

TR228



COLOURS:
PERMANENT
Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective
TEMPORARY
Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

The NO DELIVERY VEHICLES regulatory sign R228 imposes **a mandatory requirement that drivers of delivery vehicles shall NOT proceed beyond such sign**. The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).

NO GOODS VEHICLES

R229



For dimensions
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page

2.4.30

TR229



COLOURS:
PERMANENT
Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective
TEMPORARY
Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

The NO GOODS VEHICLES regulatory sign R229 imposes **a mandatory requirement that drivers of goods vehicles shall NOT proceed beyond such sign**. The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).

NO GOODS VEHICLES OVER INDICATED GVM

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page

2.4.31

COLOURS:
PERMANENT

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Legend: White retroreflective
"B MOD" ("t" in DIN
1451 Style "B")

Background: White retroreflective

TEMPORARY
Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Legend: White retroreflective
"B MOD" ("t" in DIN
1451 Style "B")

Background: Yellow retroreflective

R230



TR230



NO CONSTRUCTION VEHICLES

For dimensions
ref. Vol. 4
page

2.4.32

COLOURS:
PERMANENT

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY
Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

R231



TR231



The NO GOODS VEHICLE OVER INDICATED GVM regulatory sign R230 imposes a **mandatory requirement that drivers of goods vehicles over the indicated GVM (or GCM) shall NOT proceed beyond such sign**. The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).

The NO CONSTRUCTION VEHICLES regulatory sign R231 imposes a **mandatory requirement that drivers of construction vehicles shall NOT proceed beyond such sign**. The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).

NO VEHICLES CONVEYING DANGEROUS GOODS

For dimensions
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page

2.4.33

COLOURS:
PERMANENT

Border & diagonal: Red retroreflective
Symbol: Black semi-matt and orange
retroreflective
Background: White retroreflective

TEMPORARY
Border & diagonal: Red retroreflective
Symbol: Black semi-matt and orange
retroreflective

R232



TR232



NO ABNORMAL VEHICLES

For dimensions
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page

2.4.34

COLOURS:
PERMANENT

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY
Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

R233



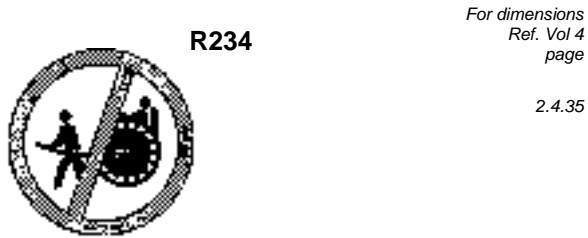
TR233



The NO VEHICLES CONVEYING DANGEROUS GOODS regulatory sign R232 imposes a **mandatory requirement that drivers of vehicles conveying dangerous goods shall NOT proceed beyond such sign**. The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).

The NO ABNORMAL VEHICLES regulatory sign R233 imposes a **mandatory requirement that drivers of abnormal vehicles shall NOT proceed beyond such sign**. The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be carried in some way (see Subsection 2.1.4 and Section 2.7).

NO RICKSHAWS



R234

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page

2.4.35



TR234

COLOURS:
PERMANENT

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

The NO RICKSHAWS regulatory sign R234 imposes a **mandatory requirement that drivers of rickshaws shall NOT proceed beyond such sign**. The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).

NO TOUR BUSES



R235

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page

2.4.36



TR235

COLOURS:
PERMANENT

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Legend: Black semi-matt
DIN"A"

Background: White retroreflective
TEMPORARY

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Legend: Black semi-matt
DIN"A"

Background: Yellow retroreflective

The NO TOUR BUSES regulatory sign R235 imposes a **mandatory requirement that drivers of tour buses shall NOT proceed beyond such sign**. The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).

NO AGRICULTURAL VEHICLES



R236

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2.4.37



TR236

COLOURS:
PERMANENT

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

The NO AGRICULTURAL VEHICLES regulatory sign R236 imposes a **mandatory requirement that drivers of agricultural vehicles shall NOT proceed beyond such sign**. The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).

NO ANIMAL-DRAWN VEHICLES



R237

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2.4.38



TR237

COLOURS:
PERMANENT

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

The NO ANIMAL-DRAWN VEHICLES regulatory sign R237 imposes a **mandatory requirement that drivers of animal-drawn vehicles shall NOT proceed beyond such sign**. The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).

NO HORSES AND RIDERS

WIDTH LIMIT

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R238



2.4.39

COLOURS:
PERMANENT

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

TR238



For dimensions
Ref. Vol 4
page

R239



2.4.29

COLOURS:

PERMANENT

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Legend: "B MOD" ("m" in DIN 1451 Style "B")

Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Legend: "B MOD" ("m" in DIN 1451 Style "B")

Background: Yellow retroreflective

TR239



The NO HORSES regulatory sign R238 imposes a **mandatory requirement that horses and their riders, whether mounted on the horses or leading them, shall NOT proceed beyond such sign.** The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).

The WIDTH LIMIT regulatory sign R239 imposes a **mandatory requirement that drivers of vehicles shall NOT proceed beyond the sign if the width of their vehicle is equal to or greater than that indicated in metres by means of a number on such sign.** Sign R239 should be displayed in a similar manner to sign R204 (see Subsection 2.4.4).

NO TOWED VEHICLES

NO HAWKERS

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R240



2.4.41

COLOURS:
PERMANENT

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

TR240



For dimensions
ref. Vol. 4
page

R241



2.4.42

COLOURS:
PERMANENT

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

Border & diagonal: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

TR241



The NO TOWED VEHICLES regulatory sign R240 imposes a **mandatory requirement that drivers of any vehicle which is towing another vehicle, including a light trailer, caravan, heavy goods trailer or a broken down vehicle, shall NOT proceed beyond the sign.** The sign may be used as the primary component of a SELECTIVE RESTRICTION sign in which case its prohibition applicability will be varied in some way (see Subsection 2.1.4 and Section 2.7).

The NO HAWKERS regulatory sign R241 imposes a **mandatory requirement that hawkers shall NOT conduct their business within 75 metres on either side of such a sign within a public road AND sign R241 imposes a mandatory requirement that drivers of vehicles shall NOT stop, stand or park to service or do business with a hawkker within 75 metres on either side of such a sign.**

COLOURS:

PERMANENT

Border & symbols: White retroreflective or semi-matt

Background: Blue retroreflective or semi-matt

TEMPORARY

Border & symbols: Black semi-matt

Background: Yellow retroreflective

R301



R301-P



For dimensions
ref. Vol. 4
page

2.5.1

TR301



TR301-P

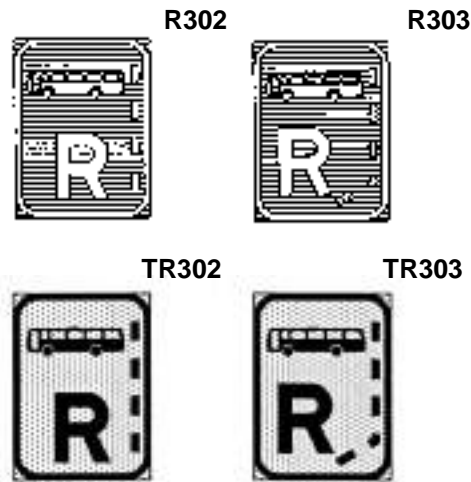


2.5.1 Bus Reservation

- 1 The BUS RESERVATION sign R301 indicates a **conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for buses only**. Sign R301-P indicates a **conditional requirement to drivers of vehicles that a parking area is reserved for buses only**. This has the effect that no other class of road user shall use the public road, portion of public road or parking area indicated by the signs BUT it should be noted that the use of sign R301 does NOT make it mandatory for drivers of buses to use the roadway, access point or other traffic facility. Such a requirement is the function of BUSES ONLY command regulatory sign R121. Signs R301 and R301-P may be displayed as SELECTIVE RESTRICTION signs with a secondary message indicating that the reservation only applies for certain times of day and/or for certain days of the week. (See Subsection 2.5.2 for BUS LANE RESERVATION signs R302 and R303).
- 2 Sign R301 should be displayed to indicate the commencement of the reservation of a roadway, or the reservation of access to an area set aside for use by buses where passenger loading or disembarking occurs, such as a bus terminus, or other transport termini where vehicle segregation occurs by class. **An R3 • "NO ENTRY • EXCEPT BUSES" sign combination shall not be used in such situations.**
- 3 The provision of BUS PARKING RESERVATION signs R301-P or TR301-P, in terms of lengths of sections of public road between junctions, shall conform to the requirements for NO PARKING signs R216 and TR216 as indicated in Subsection 2.4.13, particularly paragraphs 2.4.13.7 and 2.4.13.8. When parking is reserved for buses on-street the area reserved shall be demarcated by either parking bay or exclusive parking bay road markings.
- 4 If it is required to control access to an off-street area set aside for the exclusive parking or holding of buses i.e. passengers do not get on or off the bus in the area, sign R301-P should be used.
- 5 Signs R301 and R301-P shall apply to facilities reserved for the exclusive use of buses with a gross vehicle mass of more than 3500 kg and a seating capacity in excess of 45 seats. If it is required to reserve an area for a range of passenger vehicles including buses and those of a lesser mass than 3500 kg and/or a lesser seating capacity, HIGH OCCUPANCY VEHICLE RESERVATION signs R320 or R32D-P, displaying the LOWER limit of number of passengers carried, may be used.
- 6 Temporary versions of signs R301 and R301-P may be used under the same circumstances as the permanent versions when road construction necessitates special temporary arrangements for buses.
- 7 Signs should be sized in accordance with Table 2.4.
- 8 In this category of regulatory sign a significant number of vehicle classes have been catered for by the provision of exclusive symbols. Many of these apply to vehicle types used to provide passenger transport (several others refer to types of heavy vehicle). The various types of passenger transport vehicle are defined in legislation. The classification differences are largely based on the seating capacity of each vehicle type.
- 9 A wide range of possible uses exist for SELECTIVE RESTRICTION signs related to passenger transport vehicle classes. Typical examples are:
 - (a) a sign to indicate the maximum number of buses which shall use a bus parking area could be provided by a SELECTIVE RESTRICTION sign combination of a primary message sign BUS PARKING RESERVATION with a secondary message sign displaying the MAXIMUM NUMBER R301-P-536;
 - (b) a sign to indicate that access to a bus terminus is reserved for buses operated by a specific operator could be provided by a SELECTIVE RESTRICTION sign combination of a primary message BUS RESERVATION with a secondary message sign displaying the OPERATOR IDENTITY by LOGO or TEXT, sign R301-530 or R301-531.
- 10 Details given in this subsection and Subsection 2.5.2 are equally applicable to other classes of passenger transport vehicle. Other RESERVATION class signs applicable to passenger transport operations are covered collectively in Subsection 2.5.8. Examples of the application of passenger transport signing are given in Volume 2, Chapter 8.

BUS LANE RESERVATION

For dimensions
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pages
2.5.2
2.5.3

COLOURS:
PERMANENT

Border & symbols: White retroreflective
or semi-matt
Lane Lines: Yellow retroreflective
or semi-matt
Background: Blue retroreflective or
semi-matt

TEMPORARY

Border & symbols: Black semi-matt
Background: Yellow retroreflective

2.5.2 Bus Lane Reservation

- 1 The BUS LANE RESERVATION regulatory signs R302 and R303 indicate **a conditional requirement to drivers of vehicles that the portion of public road marked as a bus lane by EXCLUSIVE USE LANE LINE marking RM9 and the BUS word marking GM7 is reserved for buses only. Sign R303 indicates the beginning of such a bus lane.** These signs have the effect that no other class of road user shall use the portion of public road indicated by signs and markings as a bus lane. Signs R302 and R303 will commonly be qualified by a secondary message as SELECTIVE RESTRICTION signs, indicating the time(s) of day for which the signs are applicable.
- 2 Sign R303 shall be used in conjunction with EXCLUSIVE USE LANE LINE marking RM9 and other relevant road markings to indicate the commencement of a bus lane where, on a route, a bus lane has not been marked, or designated by sign prior to the point in question. It shall not be necessary to repeat sign R303 after discontinuities in a bus lane due to intersecting side roads (see Chapter 7, in particular Subsection 7.2.13). Sign 302 shall be positioned at suitable intervals along the bus lane to adequately indicate to drivers of other classes of vehicle entering the roadway at junctions or entrances, that the lane is reserved for buses. The distance between repeat signs R302 should not exceed 250 m.
- 3 Bus lanes will most commonly be located on the left side of a roadway because of the passenger boarding and alighting requirements. Signs R302 and R303 will therefore be the most commonly specified BUS LANE RESERVATION signs. However, under certain circumstances a right side bus lane may be required. Such a lane should be signed using BUS LANE - RIGHT sign R348. Other reservation signs for exclusive passenger transport lanes are covered in Subsection 2.5.8 and in Volume 2, Chapter 8.
- 4 When a bus lane is located on the left side of a roadway which is otherwise operating as a one-way roadway for traffic moving in the opposite direction to that of the buses it is commonly referred to as a "contra-flow" bus

lane. This type of lane has proven to be difficult to operate safely, particularly on initial installation, and particularly for pedestrians. It is therefore advocated that such a traffic arrangement should be signed as a two-way roadway. To achieve this, the following basic signing steps are recommended:

- (a) ONE WAY sign R4 should not be displayed for the bus lane roadway;
 - (b) BUS LANE signs R302/R303 and EXCLUSIVE USE LANE LINE marking RM9, with BUS word marking GM7, should be used to identify the bus lane to bus drivers and pedestrians;
 - (c) BUS LANE - RIGHT sign R348 should be mounted on the reverse side of all appropriate R302/R303 signs to identify the lane to drivers travelling in normal traffic (in the opposite direction), and to pedestrians.
- In addition signing will be required on intersecting side roads, according to their individual lane configurations, to prohibit turns when appropriate and to enhance further pedestrian awareness of the bus lane a range of diagrammatic signs in the GS700 series is available (see Chapter 4 and Volume 2, Chapter 8).
- 5 Vehicles other than buses may only cross a bus lane if they cannot otherwise enter or leave premises adjacent to such lane, and then only when it is safe to do so. It is normal practice to stop the EXCLUSIVE USE LANE LINE marking RM9 a sufficient distance in advance of an intersecting side road into which vehicles may turn from the road in which the bus lane is marked. This "ending" of a bus lane can be made clearer to potential turning traffic by means of END OF EXCLUSIVE USE LANE ARROW markings WM11, WM12 or WM13, positioned immediately beyond the end of marking RM9.
 - 6 Temporary versions of signs R302 and R303 may be used under the same circumstances as the permanent versions when road construction necessitates special temporary arrangements for buses.
 - 7 Signs should be sized in accordance with Table 2.4.

COLOURS:

PERMANENT

Border & symbols: White retroreflective or semi-matt

Lane Lines: Yellow retroreflective or semi-matt

Background: Blue retroreflective or semi-matt

TEMPORARY

Border & symbols: Black semi-matt

Background: Yellow retroreflective

R304



R304-P



For dimensions
ref. Vol. 4
page

2.5.4

TR304



TR304-P



2.5.3 Cycle Reservation

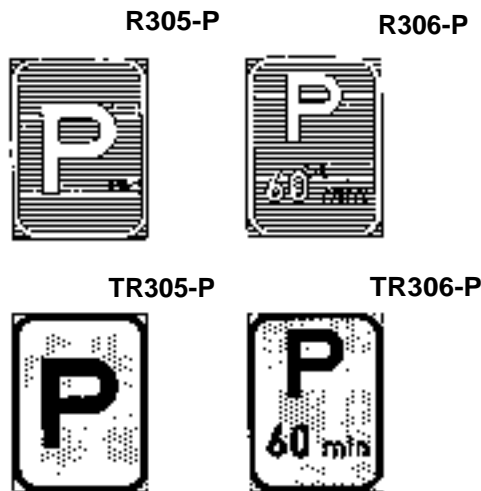
- 1 The CYCLE LANE RESERVATION regulatory sign R304 indicates **a conditional requirement to drivers of vehicles that the portion of public roadway marked by EXCLUSIVE USE LANE LINE marking RM9 and BICYCLE SYMBOL marking GM6.1 is reserved for cyclists only. Sign R304-P indicates a conditional requirement to cyclists and drivers that a parking area is reserved for cyclists only.** This has the effect that no other class of road user shall use the public road, portion of public road or parking area indicated by the signs, BUT it should be noted that the use of sign R304 does not make it mandatory for cyclists to use the reserved area. Such a requirement is the function of CYCLISTS ONLY command regulatory sign R111. Signs R304 and R304-P may be displayed as SELECTIVE RESTRICTION signs with a secondary message indicating that the reservation only applies for example to certain times of day and/or to certain days of the week.
- 2 Sign R304 should be used in conjunction with EXCLUSIVE USE LANE LINE marking RM9 and BICYCLE SYMBOL marking GM6.1 to indicate a BICYCLE LANE reservation. Sign R304 and symbol GM6.1 shall be positioned at regular intervals along the bicycle lane to adequately indicate to drivers of vehicles that the lane is reserved for bicycles. The distance between repeat signs should not exceed 250 m.
- 3 The treatment of a BICYCLE LANE at an intersecting side road requires careful consideration in terms of:
 - (a) volumes of cyclists;
 - (b) volumes of turning and/or entering traffic;
 - (c) other options available to reduce or minimise the effects of conflicts.

If potential conflicts between cyclists and turning or entering traffic are likely to be high consideration should be given to routing the bicycle lane behind the kerb line at the junction and to providing a formal bicycle crossing of the side road using BICYCLE CROSSING LINES marking GM5. When this option is exercised the use of END OF EXCLUSIVE USE LANE ARROW markings WM11, WM12 or WM13, positioned immediately beyond the end of marking RM9, may be considered.
- 4 Parking for cycles will normally be provided off-street in which case sign R304-P should be displayed in the centre of the area or length of frontage reserved for cycle parking. According to the layout of the cycle parking R304-P signs may be mounted back-to-back or on four sides of a central support or at each end of a linear layout. In the latter case the use of arrow SUPPLEMENTARY PLATE signs IN11.5 is recommended.
- 5 When parking is reserved on-street for cycles the area reserved shall be demarcated by either parking bay or exclusive parking bay road markings.
- 6 The use of a temporary version of signs R304 and R304-P is unlikely but if required such signs may be provided in the temporary sign colour code of black symbols and border on a yellow background and numbered R304 and TR304-P respectively.
- 7 Signs R304 and R304-P should be sized in accordance with Table 2.4.

PARKING AND LIMITED PARKING RESERVATION

For dimensions
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pages

2.5.5
2.5.6



COLOURS:

PERMANENT

Border & symbols: White retroreflective or semi-matt
Legend: White retroreflective or semi-matt DIN B
Background: Blue retroreflective or semi-matt

TEMPORARY

Border & symbols: Black semi-matt
Legend: Black semi-matt
DIN B
Background: Yellow retroreflective or semi-matt

2.5.4 Parking and Limited Parking

- 1 The PARKING RESERVATION regulatory sign R305-P indicates a conditional requirement to drivers of vehicles that a parking area is reserved for use only upon payment of the tariff indicated at such parking area. LIMITED PARKING RESERVATION regulatory sign R306-P indicates a conditional requirement to drivers of vehicles that a parking area is reserved for use only upon payment of the tariff indicated at such parking area subject also to a maximum duration of stay. Signs R305-P and R306-P may commonly be displayed as SELECTIVE RESTRICTION signs in conjunction with a secondary or tertiary message indicating that the reservation is only applicable for certain times of day and/or certain days of the week (see Section 2.7).
- 2 Sign R305-P should only be displayed when the parking available to any vehicle of less than 3500 kg gross vehicle mass is of unlimited time duration AND a charge is levied in relation to the duration of stay. Failure to pay the required fee shall constitute an offence.
- 3 Sign R306-P should be displayed when it is considered necessary to restrict the maximum duration of parking in the area to the time period indicated on the sign. Such parking may incur a charge or be free, but failure to leave the area concerned within the maximum time period stated shall constitute an offence. Signs of this nature may be utilised to cater for a turn-over of parking in business areas where parking supply is less than demand. The period indicated on the sign should be displayed in minutes subject to a minimum time display of ten minutes.
- 4 Signs R305-P and R306-P should only be displayed in relation to portions of public road or other areas where parking bays have been demarcated on the roadway surface.
- 5 PARKING or LIMITED PARKING RESERVATION signs R305-P and R306-P shall be located at the beginning and end of each section of public road or other area, and on the side of such section to which they are to be applicable, in addition to PARKING BAY markings RM6. Such a section of roadway shall not include any junction with another public road. If the length of the section exceeds 150 m additional R305-P or R306-P signs shall be provided so that the maximum distance between signs does not exceed 150 m.
- 6 When the section of public road between junctions does not exceed 75 m parking or limited parking may be designated for the section by displaying a single R305-P or R306-P sign in the approximate centre of the section in addition to PARKING BAY markings RM6 subject to the provisions of paragraph 2.5.4.7.
- 7 Signs R305-P and R306-P shall be displayed at 90° to the direction of travel of traffic in a roadway and shall show the same sign on the reverse side.
- 8 Temporary regulatory signs TR305-P and TR306-P may be used under the same circumstances as permanent PARKING and LIMITED PARKING RESERVATION regulatory signs when urban building operations require the provision of temporary parking on cleared sites or other areas not normally used for parking. Temporary signs TR305-P and TR306-P may be considered for use at special short term events which utilize parking areas not normally subject to limits or reservations.
- 9 The extent of the area/section of public road to which PARKING and LIMITED PARKING RESERVATION signs and the wide range of SELECTIVE RESTRICTION derivatives may be applicable may be clarified by the display of arrow SUPPLEMENTARY PLATE signs IN1.5 below the signs and at 90° to the signs. The arrow symbols should face the roadway and should point inwards from the limit of the parking or limited parking area. The arrow symbol below an intermediate R305-P or R306-P sign on a section of public road over 150 m in length should point in both directions, as should the arrow symbol when used on a sign placed approximately in the centre of a section of public road that does not exceed 75 m between junctions.
- 10 Signs should be sized in accordance with Table 2.4. Care should be taken to ensure that all legend is legible to drivers from a reasonable distance.
- 11 See Subsections 2.4.13 and 2.4.14 on NO PARKING sign R216 and NO STOPPING sign R217 respectively. Any rules of practice applicable to signs R216 or R217 not specifically detailed here shall apply *mutatis mutandis* to the use of signs R305-P, R306-P, TR305-P and TR306-P.
- 12 If parking is not to be regulated by the application of time related charges and/or maximum durations of stay signs R305-P, R306-P, TR305-P and TR306-P should not be used. To identify an area as uncontrolled parking a SERVICE FACILITY PARKING sign GF15 displaying a white "P" symbol on a brown background should be used.

2.5.6 Other Reservation Signs

- 1 Other RESERVATION regulatory signs referring to specific classes of vehicle displaying the letter "R" indicate **a conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for the class of vehicle indicated by symbol on the sign only**, and signs displaying the letter "P" indicate **a conditional requirement to drivers of vehicles that a parking area is reserved for the class of vehicle indicated by symbol on the sign only**.
- 2 Signs R307 to R323 and R307-P to R323-P also have the effect that no other class of road user shall use the public road, portion of public road or parking area indicated by the signs. These signs may be displayed as SELECTIVE RESTRICTION signs in conjunction with a secondary message indicating that the reservation is only applicable for certain times of day and/or certain days of the week (see Section 2.7).
- 3 The following signs shown on subsequent pages cover reservations appropriate to various classes of vehicle:

R307	MOTOR CYCLE RESERVATION
R308	MOTOR CAR RESERVATION
R312	DELIVERY VEHICLE RESERVATION
R313	GOODS VEHICLE RESERVATION
R314	GOODS VEHICLE OVER INDICATED GVM RESERVATION
R315	CONSTRUCTION VEHICLE RESERVATION
R316	VEHICLE CONVEYING DANGEROUS GOODS RESERVATION
R317	ABNORMAL VEHICLE RESERVATION
R318	RICKSHAW RESERVATION
R321	EMERGENCY VEHICLE RESERVATION
R322	POLICE VEHICLE RESERVATION
R323	DISABLED PERSONS VEHICLE RESERVATION
- 4 The following signs shown on subsequent pages cover parking reservations appropriate to various classes of vehicle:

R307-P	MOTOR CYCLE PARKING RESERVATION
R308-P	MOTOR CAR PARKING RESERVATION
R312-P	DELIVERY VEHICLE PARKING RESERVATION
R313-P	GOODS VEHICLE PARKING RESERVATION
R314-P	GOODS VEHICLE OVER INDICATED GVM PARKING RESERVATION
R315-P	CONSTRUCTION VEHICLE PARKING RESERVATION
R316-P	DANGEROUS GOODS VEHICLE PARKING RESERVATION
R317-P	ABNORMAL VEHICLE PARKING RESERVATION
R318-P	RICKSHAW PARKING RESERVATION
R321-P	EMERGENCY VEHICLE PARKING RESERVATION
R322-P	POLICE VEHICLE PARKING RESERVATION
R323-P	DISABLED PERSONS VEHICLE PARKING RESERVATION
- 5 All other reservation signs related in one way or another to the provision of passenger transport are described and detailed in Subsections 2.5.8 and 2.5.9.
- 6 Signs R307 to R323 are not likely to be required to designate reserved portions of roadway except in unusual circumstances. Should the need arise their effect and application shall be the same as for the BUS RESERVATION regulatory sign R301. Signs R307 to R323 may, however, be used to identify points of access, for vehicles of the class indicated to areas reserved for the individual class of vehicle. Examples of such applications are:
 - (a) DELIVERY VEHICLE RESERVATION sign R312 at the entrance to the service area for a block of shops or large shopping centre;
 - (b) EMERGENCY VEHICLE RESERVATION sign R321 at an exclusive ambulance access to a casualty handling facility at a hospital;
 - (c) POLICE VEHICLE RESERVATION sign R322 at an exclusive police vehicle access to a police station or other security facility.
 - (d) MOTOR CYCLE RESERVATION sign R307 is only likely to be required in exceptional circumstances where an exclusive roadway has been provided for motor cycles to lead them to a motor cycle parking area. In such an instance sign R307 should be displayed on the left-hand side of such an access roadway.

An R3 "NO ENTRY - EXCEPT" sign combination shall not be used in such situations.
- 7 Signs R307-P to R323-P may be displayed in the same manner as signs R305-P or R306-P to identify parking reserved for the class of vehicle displayed on the sign. A charge or time limit may or may not be related to such signs. The indication of a duration of stay limit, or time of day, or day of the week applicability, in addition to the principal sign, shall classify the sign as a SELECTIVE RESTRICTION sign (see Section 2.7). It shall be an offence to fail to comply with the symbolic parking classification indicated on the sign. Since the use of an appropriate symbol places a reservation on the use of the area concerned, the use of a SERVICE FACILITY PARKING sign GF15 (as indicated in paragraph 2.5.4.12) is NOT appropriate for specific classes of vehicle.
- 8 Signs R307-P to R323-P should only be displayed in relation to portions of public road or other areas where parking bays have been demarcated on the roadway surface.
- 9 PARKING RESERVATION signs R307-P to R323-P shall be located, as appropriate to the class of vehicle concerned, at the beginning and end of each section of public road, or other area, and on the side of such section to which they are to be applicable, in addition to appropriate road markings. Such a section of roadway shall not include any junction with another public road. If the length of section exceeds 150 m additional R307-P to R323-P signs shall be provided, as appropriate, so that the maximum distance between signs does not exceed 150 m.
- 10 When the section of public road between junctions does not exceed 75 m parking or limited parking may be

- designated for the section by displaying a single R307- P to R323-P sign in the approximate centre of the section in addition to appropriate road markings subject to the provisions of paragraph 2.5.6.9.
- 11 Signs R307-P to R323-P shall be displayed at goo to the direction of travel of traffic in a roadway and shall show the same sign on the reverse side.
 - 12 The extent of the area/section of public road to which signs R307-P to R323-P and the wide range of SELECTIVE RESTRICTION derivatives may be applicable may be clarified by the display of arrow SUPPLEMENTARY PLATE signs IN11.5 below the signs and at 90° to the signs. The arrow symbols should face the road- way and should point inwards from the limit of the area. The arrow symbol below an intermediate sign on a section of public road over 150 m in length should point in both directions, as should the arrow symbol when used on a sign placed approximately in the centre of a section of public road that does not exceed 75 m between junctions.
 - 13 See Subsections 2.4.13 and 2.4.14 on NO PARKING sign R216 and NO STOPPING sign R217 respectively. Any rules of practice applicable to signs R216 or R217 not specifically detailed here apply *mutatis mutandis* to the use of signs R307-P to R323-P.
 - 14 Reservation signs R307 to R323 may be incorporated into the signface of a guidance sign to indicate that a route, or lane of a roadway, is reserved for use by the class of vehicle indicated. When used on a direction sign the overall guidance signface should include a distance message indicating the distance to the point where the reservation commences.
 - 15 Signs R307 to R323 should be used with care to ensure that they do not become over-utilized or incorrectly utilized, thereby reducing their effectiveness when really required.
 - 16 Temporary regulatory versions of signs R307 to R323 or R307-P to R323-P are unlikely to be required. Should temporary versions be required these should be numbered in the TR300 series from TR307 to TR323 and TR307-P to TR323-P and should be manufactured with black semi-matt symbols and border on a yellow retroreflective background. Such signs have the significance that **for a temporary period due to roadworks, adjacent building operations, or other traffic deviation or detour use of the roadway, portion of roadway, or other road traffic facility such as a parking area is reserved for the class of vehicle indicated by symbol on the sign and no person shall drive or park any other class of vehicle in the area so reserved.**
 - 17 The size and positioning of signs R307 to R323, TR307 to TR323, R307-P to R323-P and TR307-P to TR323-P should follow the general requirements for regulatory signs as given in Chapter 1, Table 2.4 and Volume 2.
 - 18 Signs R312 to R317 refer to various classes of goods vehicle, all of which apply to vehicles over 3 500kg gross vehicle mass or gross combination mass (GVM/GCM). The GOODS VEHICLE OVER INDICATED GVM RESERVATION signs R314, TR314, R314-P and TR314-P apply only to goods vehicles of a GVM or GCM greater than that indicated on the sign.
 - 19 The use of SELECTIVE RESTRICTION variants of all signs covered by this subsection is permitted.

MOTOR CYCLE RESERVATION

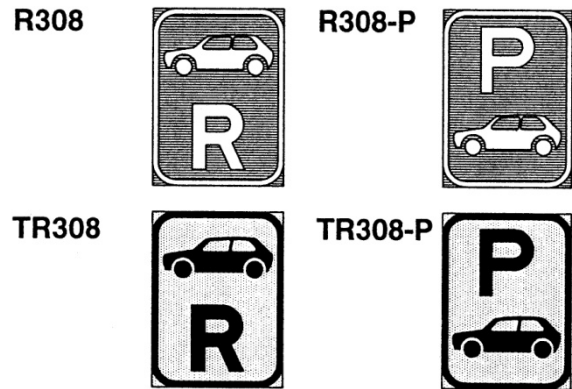
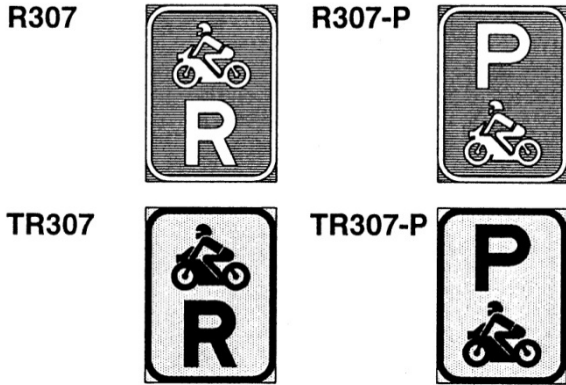
MOTOR CAR RESERVATION

COLOURS:
 PERMANENT
 Border & symbols: White retroreflective or semi-matt
 Background: Blue retroreflective or semi-matt
 TEMPORARY
 Border & symbols: Black semi-matt
 Background: Yellow retroreflective

For dimensions
 ref. Vol. 4
 page
 2.5.7

COLOURS:
 PERMANENT
 Border & symbols: White retroreflective or semi-matt
 Background: Blue retroreflective or semi-matt
 TEMPORARY
 Border & symbols: Black semi-matt
 Background: Yellow retroreflective

For dimensions
 ref. Vol. 4
 page
 2.5.8



The MOTOR CYCLE RESERVATION regulatory sign R307 indicates a **conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for motor cycles only**. Sign R307-P indicates a **conditional requirement to drivers of vehicles that a parking area is reserved for motor cycles only**.

The MOTOR CAR RESERVATION regulatory sign R308 indicates a **conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for motor cars only**. Sign R308-P indicates a **conditional requirement to drivers of vehicles that a parking area is reserved for motor cars only**.

DELIVERY VEHICLE RESERVATION

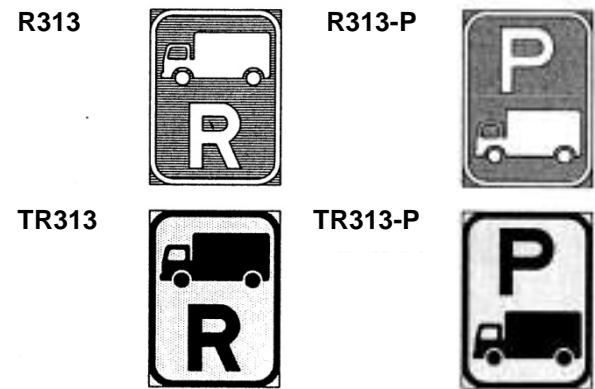
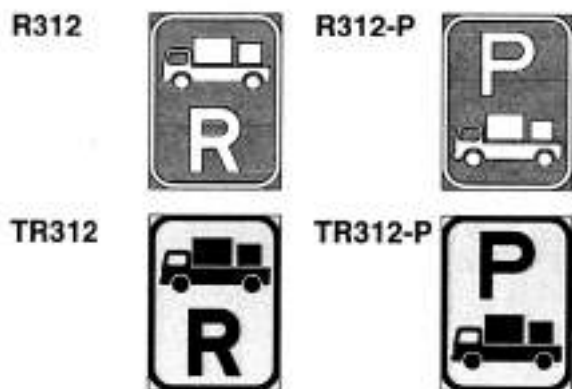
GOODS VEHICLE RESERVATION

COLOURS:
 PERMANENT
 Border & symbols: White retroreflective or semi-matt
 Background: Blue retroreflective or semi-matt
 TEMPORARY
 Border & symbols: Black semi-matt
 Background: Yellow retroreflective

For dimensions
 ref. Vol. 4
 page
 2.5.12

COLOURS:
 PERMANENT
 Border & symbols: White retroreflective or semi-matt
 Background: Blue retroreflective or semi-matt
 TEMPORARY
 Border & symbols: Black semi-matt
 Background: Yellow retroreflective

For dimensions
 ref. Vol. 4
 page
 2.5.13



The DELIVERY VEHICLE RESERVATION sign R312 indicates a **conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for delivery vehicles only**. Sign R312-P indicates a **conditional requirement to drivers of vehicles that a parking area is reserved for delivery vehicles only**.

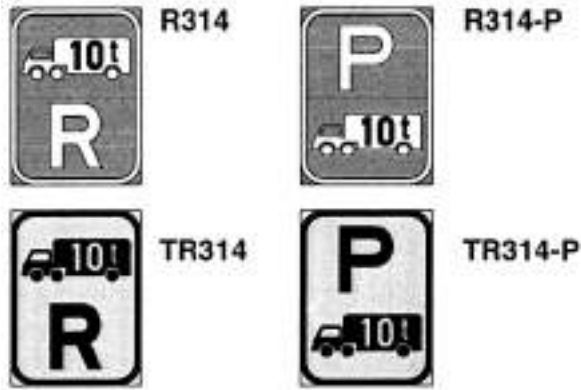
The GOODS VEHICLE RESERVATION regulatory sign R313 indicates a **conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for goods vehicles only**. Sign R313-P indicates a **conditional requirement to drivers of vehicles that a parking area is reserved for goods vehicles only**.

GOODS VEHICLES OVER INDICATED GVM RESERVATION

For dimensions ref. Vol. 4 page 2.5.14

COLOURS:
 PERMANENT
 Border & symbols: White retroreflective or semi-matt
 Legend: Black semi-matt or blue retroreflective; Numerals: "B" MOD and t: DIN "B"
 Background: Blue retroreflective or semi-matt

TEMPORARY
 Border & symbols: Black semi-matt
 Legend: White or yellow retroreflective;
 Numerals: "B" MOD and t: DIN "B"
 Background: Yellow retroreflective



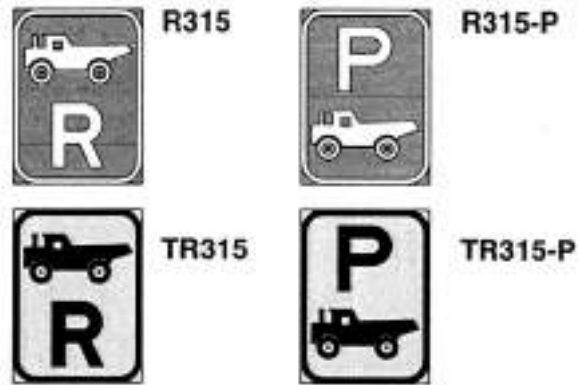
The GOODS VEHICLES OVER INDICATED GVM RESERVATION regulatory sign R314 indicates a **conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for goods vehicles over the indicated GVM only**. Sign R314-P indicates a **conditional requirement to drivers of vehicles that a parking area is reserved for goods vehicles over the indicated GVM only**.

CONSTRUCTION VEHICLE RESERVATION

For dimensions ref. Vol. 4 page 2.5.15

COLOURS:
 PERMANENT
 Border & symbols: White retroreflective or semi-matt
 Background: Blue retroreflective or semi-matt

TEMPORARY
 Border & symbols: Black semi-matt
 Background: Yellow retroreflective



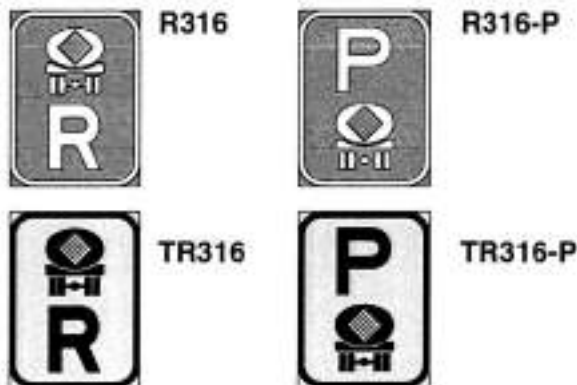
The CONSTRUCTION VEHICLE RESERVATION regulatory sign R315 indicates a **conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for construction vehicles only**. Sign R315-P indicates a **conditional requirement to drivers of vehicles that a parking area is reserved for construction vehicles only**.

VEHICLE CONVEYING DANGEROUS GOODS RESERVATION

For dimensions ref. Vol. 4 page 2.5.16

COLOURS:
 PERMANENT
 Border: White retroreflective or semi-matt
 Symbol: White retroreflective or semi-matt and orange retroreflective
 Background: Blue retroreflective or semi-matt

TEMPORARY
 Border: Black semi-matt
 Symbol: Black semi-matt and orange retroreflective
 Background: Yellow retroreflective



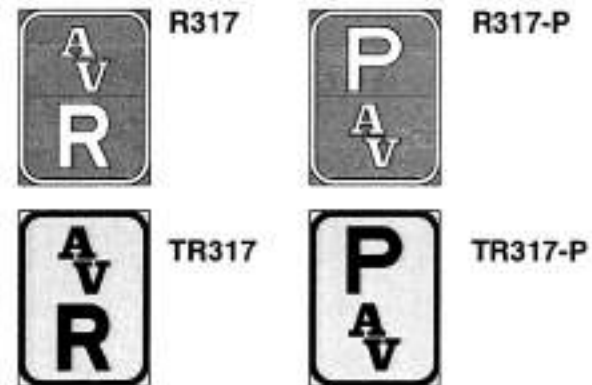
The VEHICLES CONVEYING DANGEROUS GOODS RESERVATION regulatory sign R316 indicates a **conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for vehicles conveying dangerous goods only**. Sign R316-P indicates a **conditional requirement to drivers of vehicles that a parking area is reserved for vehicles conveying dangerous goods only**.

ABNORMAL VEHICLE RESERVATION

For dimensions ref. Vol. 4 page 2.5.17

COLOURS:
 PERMANENT
 Border & symbols: White retroreflective or semi-matt
 Background: Blue retroreflective or semi-matt

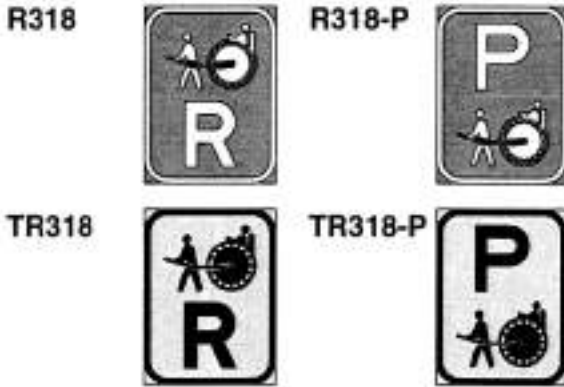
TEMPORARY
 Border & symbols: Black semi-matt
 Background: Yellow retroreflective



The ABNORMAL VEHICLE RESERVATION regulatory sign R317 indicates a **conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for abnormal vehicles only**. Sign R317-P indicates a **conditional requirement to drivers of vehicles that a parking area is reserved for abnormal vehicles only**.

RICKSHAW RESERVATION

COLOURS: For dimensions
 PERMANENT ref. Vol. 4
 Border & symbols: *White retroreflective or semi-matt* page
 Background: *Blue retroreflective or semi-matt* 2.5.18
 TEMPORARY
 Border & symbols: *Black semi-matt*
 Background: *Yellow retroreflective*



The RICKSHAW RESERVATION regulatory sign R318 indicates **a conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for rickshaws only.** Sign R318-P indicates **a conditional requirement to drivers of vehicles that a parking area is reserved for rickshaws only.**

EMERGENCY VEHICLE RESERVATION

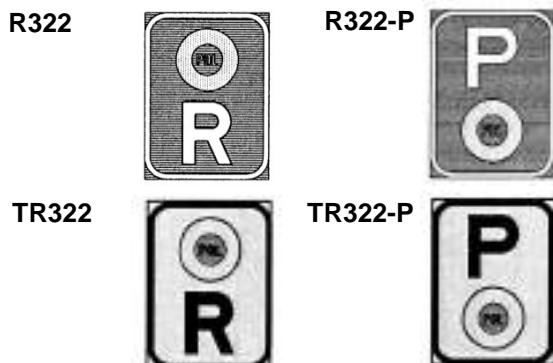
COLOURS: For dimensions
 PERMANENT ref. Vol. 4
 Border: *White retroreflective or semi-matt* page
 Symbols: *White retroreflective or semi-matt and white and red retroreflective* 2.5.21
 Background: *Blue retroreflective or semi-matt*
 TEMPORARY
 Border: *Black semi-matt*
 Symbols: *Black semi-matt and white and red retroreflective*
 Background: *Yellow retroreflective*



The EMERGENCY VEHICLE RESERVATION regulatory sign R321 indicates **a conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for emergency vehicles only.** Sign R321-P indicates **a conditional requirement to drivers of vehicles that a parking area is reserved for emergency vehicles only.**

POLICE VEHICLE RESERVATION

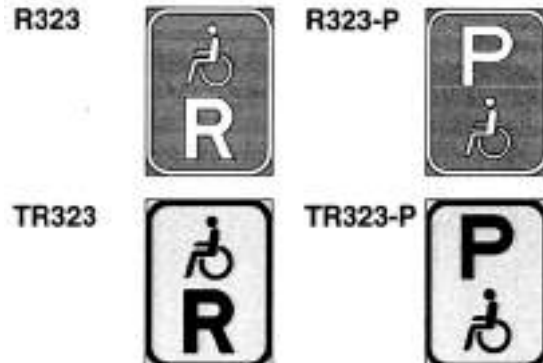
COLOURS: For dimensions
 PERMANENT ref.
 Border: *White retroreflective or semi-matt* Vol. 4
 Symbols: *White retroreflective or semi-matt and yellow and blue retroreflective* page
 Background: *Blue retroreflective or semi-matt* 2.5.22
 TEMPORARY
 Border: *Black semi-matt*
 Symbols: *Black semi-matt and yellow and blue retroreflective*
 Background: *Yellow retroreflective*



The POLICE VEHICLE RESERVATION regulatory sign R322 indicates **a conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for police vehicles only.** Sign R322-P indicates **a conditional requirement to drivers of vehicles that a parking area is reserved for police vehicles only.**

DISABLED PERSONS VEHICLE RESERVATION

COLOURS: For dimensions
 PERMANENT ref. Vol. 4
 Border & symbol: *White retroreflective or semi-matt* page
 Lane Lines: *Yellow retroreflective or semi-matt*
 Background: *Blue retroreflective or semi-matt* 2.5.2
 TEMPORARY
 Border & symbol: *Black semi-matt*
 Background: *Yellow retroreflective*

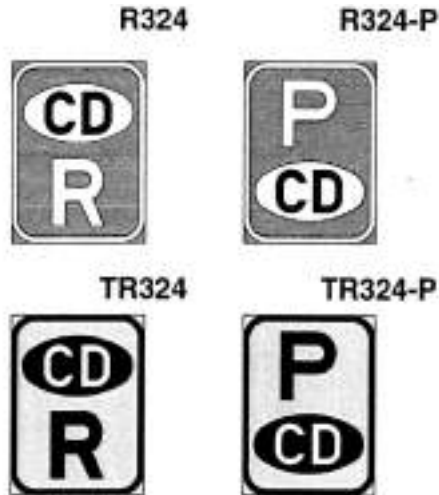


The DISABLED PERSONS VEHICLE RESERVATION regulatory sign R323 indicates **a conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for disabled persons vehicles only.** Sign R323-P indicates **a conditional requirement to drivers of vehicles that a parking area is reserved for disabled persons vehicles only.**

AUTHORISED VEHICLE RESERVATION (e.g. CORPS DIPLOMATIQUE)

For dimensions
ref. Vol. 4
page

2.5.24



COLOURS:

PERMANENT

Border: White retroreflective or semi-matt

Symbols: White retroreflective or semi-matt

Legend: Black semi-matt or blue retroreflective "B MOD" or full colour

Background: Blue retroreflective or semi-matt

TEMPORARY

Border: Black semi-matt

Symbols: Black semi-matt

Legend: Yellow or white retroreflective "B MOD" or full colour

Background: Yellow retroreflective

2.5.7 Authorised Vehicle Reservation

- 1 The AUTHORISED VEHICLE RESERVATION regulatory sign R324 indicates **a conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for vehicles of the organisation indicated by means of a crest, logo or other device, on such sign, only.** Sign TR324-P indicates **a conditional requirement to drivers of vehicles that a parking area is reserved for vehicles of the organisation indicated by means of a crest, logo or other device, on such sign, only.** This has the effect that a vehicle without such crest, logo or other device, shall not use the public road, portion of public road or parking area indicated by the signs.
- 2 The term "Authorised Vehicle" is deemed to cover any vehicles which an organisation wishes to have access to an area under its control. These vehicles shall preferably be identified by a crest, logo or other device as depicted on the sign e.g. diplomatic vehicles.
- 3 It is most unlikely that these signs will be used on public roadways. Their use is most likely to be relevant to private property to which the public has access such as transport termini or a university campus.
- 4 A range of symbols may be used within the oval shape provided including:
 - (a) town crests;
 - (b) provincial or national crests;
 - (c) departmental crests; or
 - (d) logos.
- 5 The identifying symbol may be indicated in black on white or in full colour on the white oval background.
- 6 The use of signs R324 and R324-P shall conform to the practices applicable to other reservation signs in this section (see Subsection 2.5.6.)

2.5.8 Other Passenger Transport Reservation Signs

- 1 This chapter provides a range of regulatory signs suitable for the control and operation of passenger transport vehicles and thereby various facilities such as dedicated roadways, exclusive use lanes, stops, ranks, termini, stations, and parking areas. The context of the term "passenger transport" is covered as widely as possible in this section and includes details of reservation signs for the following classes of "passenger transport" vehicle, and for signs indicating exclusive lanes and other facilities shared by more than one class of vehicle (details of bus reservation signs are covered in Subsections 2.5.1 and 2.5.2):
 - (a) taxi;
 - (b) minibus;
 - (c) midi-bus;
 - (d) tour bus;
 - (e) high occupancy vehicle;
 - (f) tram or light rail vehicle.
- 2 TAXI and HIGH OCCUPANCY VEHICLE signs have been included because, although these classes of vehicle do not represent formal public transport vehicles, they tend to be subjected to similar on-street controls as passenger transport vehicles.
- 3 Signs are included for TRAMS, or light rail vehicles, to assist designers deal with the basic signing concepts and to illustrate that such basic signing can conform to the principles evolved within the road traffic sign system and covered by this Manual.
- 4 The control and operation of passenger transport vehicles is likely to utilize signs from the CONTROL (Section 2.2), COMMAND (Section 2.3), PROHIBITION (Section 2.4), and RESERVATION categories of regulatory sign. The most frequently used signs for passenger transport purposes will be those in the RESERVATION category. For this reason the following paragraphs are included in this section of the chapter. The principles involved are, however, equally appropriate to the use of public transport signs covered in other sections (see also Volume 2, Chapter 8). Signs relevant to the control and operation of passenger transport are listed below in relation to their vehicle class:
 - (a) BUS- R121, R227, R301, R301-P, R302, R303, R325, R348, (R)560;
 - (b) TAXI- R118, R224, R309, R309-P, (R)564;
 - (c) MINIBUS - R119, R225, R310, R31Q-P, R326, (R)565;
 - (d) MIDI-BUS- R120, R226, R311, R311-P, (R)566;
 - (e) TOUR BUS- R129, R235, R319, R319-P, (R)574;
 - (f) HIGH OCCUPANCY VEHICLE- R320, R32Q-P, R336, R337, R352, (R)575;
 - (g) TRAM- R137, R338, R339, R340, R341, (R)583;
 - (h) shared facilities- R134, R138, R139, R327, R327-P, R328, R329, R342, R343, R344, R345, R346, R347, R349, R350, R351.
- 5 Demand has occurred for the use of reserved facilities such as bus lanes for more than one class of vehicle. The normal rule restricting regulatory signs to the display of only one symbol per signface (Subsection 2.1.4), has therefore been waived to permit the use of signs relating to passenger transport vehicles to display up to three symbols per signface. Signs displaying two symbols are of a standard size, whereas those displaying three symbols are increased in size for a given application as indicated in Table 2.7 (see also Subsection 2.3.14).
- 6 Identification of a passenger transport lane is provided for by the use of one of a number of signs together with the EXCLUSIVE USE LANE LINE marking RM9 (RM9 marking can also be used to demarcate a BICYCLE LANE). In addition the word markings BUS (GM7), Tram (GM7), or the HIGH OCCUPANCY VEHICLE symbol GM6.* are recommended to be marked within the lane at positions where reservation signs have been erected. The use of the word "BUS" is recommended whether the lane is for buses only or for shared use by buses and other classes of vehicle.
- 7 If the lane or other facility is intended for HIGH OCCUPANCY VEHICLES it will not be necessary to use multi-symbol signs, since high occupancy vehicles, by definition, may include all classes of vehicle with the indicated number of seats occupied.
- 8 Public transport RESERVATION regulatory signs displaying the letter "R" indicate a conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for the class of vehicle indicated by symbol on the sign only, and signs displaying the letter "P" indicate a conditional requirement to drivers of vehicles that a parking area is reserved for the class of vehicle indicated by symbol on the sign only.
- 9 These signs also have the effect that no other class of road user shall use the public road, portion of public road or parking area indicated by the signs. These signs may be displayed as SELECTIVE RESTRICTION signs in conjunction with a secondary message indicating, for instance, that the reservation is only applicable for certain times of the day and/or certain days of the week (see Section 2.7).
- 10 The following signs, shown on subsequent pages, cover reservations appropriate to the indicated classes and groups of vehicles (bus reservation signs are covered in Subsection 2.5.1 and 2.5.2 and passenger transport "stop" reservation signs in Subsection 2.5.9):

R309	TAXI RESERVATION
R310	MINIBUS RESERVATION
R311	MIDI-BUS RESERVATION
R319	TOUR BUS RESERVATION
R320	HIGH OCCUPANCY VEHICLE RESERVATION
R327	shared BUS/MIDI-BUS/MINIBUS RESERVATION
R328	shared BUS/MIDI-BUS/MINIBUS LANE RESERVATION
R329	start of shared BUS/MIDI-BUS/MINIBUS RESERVATION
R330	BUS AND MIDI-BUS RESERVATION
R331	BUS AND MIDI-BUS LANE RESERVATION
R332	start of BUS AND MIDI-BUS LANE RESERVATION
R333	BUS, MIDI-BUS, MINIBUS RESERVATION
R334	BUS, MIDI-BUS, MINIBUS LANE RESERVATION
R335	start of BUS, MIDI-BUS AND MINIBUS LANE RESERVATION
R336	HIGH OCCUPANCY VEHICLE LANE RESERVATION

- R337 start of HIGH OCCUPANCY VEHICLE LANE RESERVATION
- R338 TRAM RESERVATION
- R339 TRAM LANE RESERVATION
- R340 start of TRAM LANE RESERVATION
- R342 shared BUS/TRAM RESERVATION
- R343 shared BUS/TRAM LANE RESERVATION
- R344 start of shared BUS/TRAM LANE RESERVATION
- R345 shared BUS/TRAM/MINIBUS RESERVATION
- R346 BUS/TRAM/MINIBUS LANE RESERVATION
- R347 start of shared BUS/TRAM/MINIBUS LANE RESERVATION
- R348 BUS LANE - RIGHT RESERVATION
- R349 shared BUS/MIDI-BUS/MINIBUS LANE- RIGHT RESERVATION
- R350 shared BUS/TRAM LANE - RIGHT RESERVATION
- R351 shared BUS/TRAM/MINIBUS LANE RESERVATION
- R352 MIDI-BUS RESERVATION
- R353 AUTHORISED PASSENGER TRANSPORT VEHICLE LANE RESERVATION
- R354 AUTHORISED PASSENGER TRANSPORT VEHICLE LANE- RIGHT RESERVATION.
- 11 The following signs shown on subsequent pages cover parking reservations appropriate to various classes and groups of passenger transport vehicle (bus parking reservation signs are covered in Subsection 2.5.1):
- R309-P TAXI PARKING RESERVATION
- R310-P MINIBUS PERKING RESERVATION
- R311-P MIDI-BUS PARKING RESERVATION
- R319-P TOUR BUS PARKING RESERVATION
- R320-P HIGH OCCUPANCY VEHICLE RESERVATION
- R327-P shared BUS/MIDI-BUS/MINIBUS PARKING RESERVATION
- 12 It is likely that relatively complex combinations of class of vehicle/lane use configurations will develop involving signs of the types discussed. Additional requirements may be provided for in the form of:
- (a) SELECTIVE RESTRICTION signs - involving the combination of a SECONDARY regulatory message with the PRIMARY sign message, such as the time of applicability of the PRIMARY message (the secondary message sign shall use the same colours as the primary sign above it);
- (b) INFORMATION-by the use of SUPPLEMENTARY PLATE sign IN11 (using a white background and black border and message).
- 13 Bus lanes are often in environments which are also used by pedestrians and may in fact operate in an unexpected lane. If such a lane is opened to other classes of vehicle such as minibuses it is likely that their operating speed will increase. This will increase the risk for pedestrians and the treatment of crossing points must be undertaken with great care. It is therefore recommended that pedestrian cross routes be clearly identified to drivers of the passenger transport vehicles and that the numbers and design of points at which pedestrians may cross be strictly controlled. If necessary specific signs may be necessary to warn pedestrians of the circumstances (see Chapter 4 and Volume 2, Chapter 8). Ultimately it may be necessary to erect pedestrian barriers or screens between crossing points.

TABLE 2.7

PASSENGER TRANSPORT SIGN SIZES

TABLE 2.7

	Operating speed (km/h)			Stopping/ Parking	Overhead Signs
	100 or more	70 to 90	60		
Circular sign diameter Two symbols (mm)	1200	900	600	450	1600
Circular sign diameter Three symbols (mm)	1600	1200	900	600	2000 ⁽²⁾
Rectangular sign- H x W Two symbols (mm)	1200 x 900	900x 675	600x 450	450x 340	1600 x 1200
Rectangular sign- H x W Three symbols (mm)	1500 ⁽²⁾ x 900	1125 ⁽²⁾ x 675	750 ⁽²⁾ x 450	565 ⁽²⁾ x 340	2000 ⁽²⁾ x 1200

NOTES:

(1) See Table 2.4 for standard regulatory sign sizes.

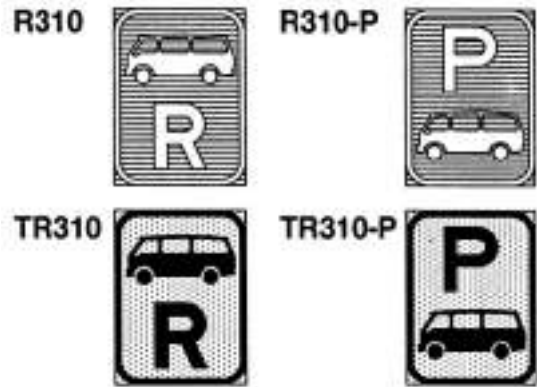
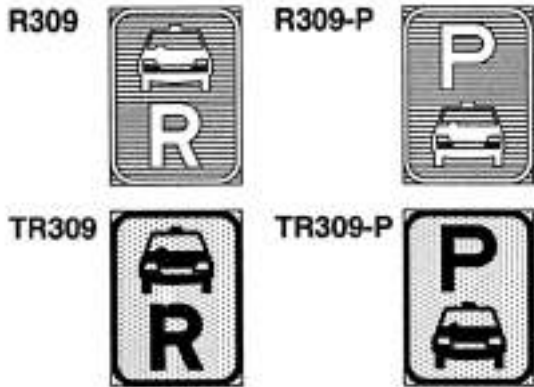
(2) These are non-standard sign sizes.

TAXI RESERVATION

MINIBUS RESERVATION

COLOURS: *For dimensions ref. Vol. 4 page*
 PERMANENT
 Border & symbols: *White retroreflective or semi-matt*
 Background: *Blue retroreflective or semi-matt*
 TEMPORARY
 Border & symbols: *Black semi-matt*
 Background: *Yellow retroreflective* 2.5.9

COLOURS: *For dimensions ref. Vol. 4 page*
 PERMANENT
 Border & symbols: *White retroreflective or semi-matt*
 Background: *Blue retroreflective or semi-matt*
 TEMPORARY
 Border & symbols: *Black semi-matt*
 Background: *Yellow retroreflective* 2.5.10



The TAXI RESERVATION regulatory sign R309 indicates a **conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for taxis only**. Sign R309-P indicates a **conditional requirement to drivers of vehicles that a parking area is reserved for taxis only**.

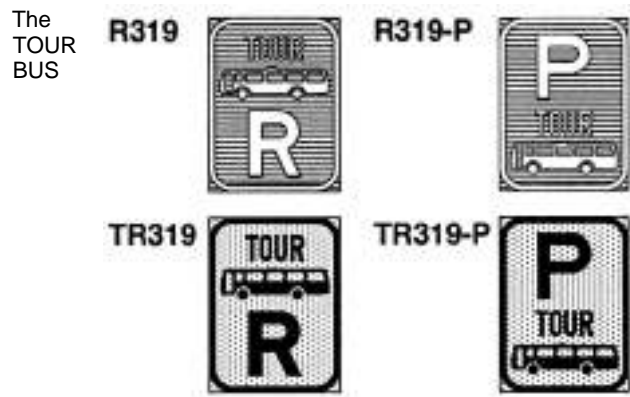
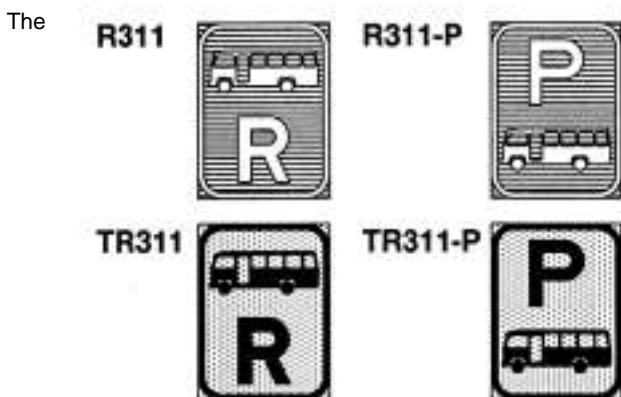
The MINIBUS RESERVATION regulatory sign R310 indicates a **conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for minibuses only**. Sign R310-P indicates a **conditional requirement to drivers of vehicles that a parking area is reserved for minibuses only**.

MIDI-BUS RESERVATION

TOUR BUS RESERVATION

COLOURS: *For dimensions ref. Vol. 4 page*
 PERMANENT
 Border & symbols: *White retroreflective or semi-matt*
 Background: *Blue retroreflective or semi-matt*
 TEMPORARY
 Border & symbols: *Black semi-matt*
 Background: *Yellow retroreflective* 2.5.11

COLOURS: *For dimensions ref. Vol. 4 page*
 PERMANENT
 Border: *White retroreflective or semi-matt*
 Symbols: *White retroreflective or semi-matt (Legend)DIN "A"*
 Background: *Blue retroreflective or semi-matt*
 TEMPORARY
 Border: *Black semi-matt*
 Symbols: *Black semi-matt (Legend) DIN "A"*
 Background: *Yellow retroreflective* 2.5.19



The MIDIBUS RESERVATION regulatory sign R311 indicates a **conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for midi-buses only**. Sign R311-P indicates a **conditional requirement to drivers of vehicles that a parking area is reserved for midi-buses only**.

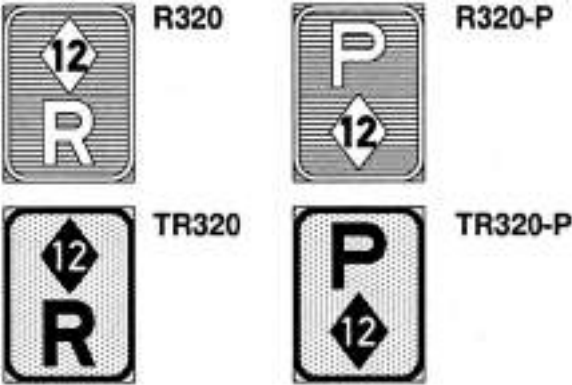
The TOUR BUS RESERVATION regulatory sign R319 indicates a **conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for tour buses only**. Sign R319-P indicates a **conditional requirement to drivers of vehicles that a parking area is reserved for tour buses only**.

HIGH OCCUPANCY VEHICLE RESERVATION

For dimensions ref. Vol. 4 page 2.5.20

COLOURS:
PERMANENT
 Border: White retroreflective or semi-matt
 Symbols: White retroreflective or semi-matt
 Legend: Black semi-matt or blue retroreflective "B MOD"
 Background: Blue retroreflective or semi-matt

TEMPORARY
 Border: Black semi-matt
 Symbols: Black semi-matt and yellow retroreflective
 Legend: White or yellow retroreflective "B MOD"
 Background: Yellow retroreflective



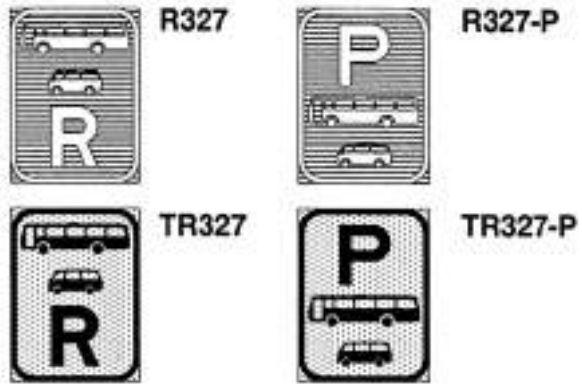
The HIGH OCCUPANCY VEHICLE RESERVATION regulatory sign indicates a **conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for high occupancy vehicles only**. Sign R320-P indicates a **conditional requirement to drivers of vehicles that a parking area is reserved for high occupancy vehicles only**.

BUS AND MINIBUS RESERVATION

For dimensions ref. Vol. 4 page 2.5.27

COLOURS:
PERMANENT
 Border & symbols: White retroreflective or semi-matt
 Background: Blue retroreflective or semi-matt

TEMPORARY
 Border & symbols: Black semi-matt
 Background: Yellow retroreflective



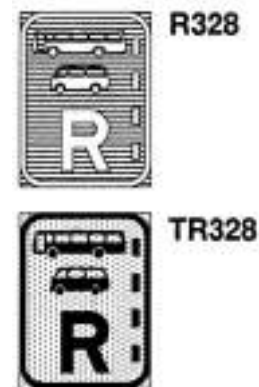
The BUS AND MINIBUS RESERVATION regulatory sign R327 indicates a **conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for buses and minibuses only**. Sign R327-P indicates a **conditional requirement to drivers of vehicles that a parking area is reserved for buses and minibuses only**.

BUS AND MINIBUS LANE RESERVATION

For dimensions ref. Vol. 4 page 2.5.28

COLOURS:
PERMANENT
 Border & symbols: White retroreflective or semi-matt Lane
 Lines: Yellow retroreflective or semi-matt
 Background: Blue retroreflective or semi-matt

TEMPORARY
 Border & symbols: Black semi-matt
 Background: Yellow retroreflective



The BUS AND MINIBUS LANE RESERVATION regulatory sign R328 indicates a **conditional requirement to drivers of vehicles that the portion of public road marked as a bus and minibus lane by EXCLUSIVE USE LANE LINE marking RM9 and the BUS word marking RM17.2 is reserved for buses and minibuses only**.

START OF BUS AND MINIBUS LANE RESERVATION

For dimensions ref. Vol. 4 page 2.5.29

COLOURS:
PERMANENT
 Border & symbols: White retroreflective or semi-matt Lane
 Lines: Yellow retroreflective or semi-matt
 Background: Blue retroreflective or semi-matt

TEMPORARY
 Border & symbols: Black semi-matt
 Background: Yellow retroreflective

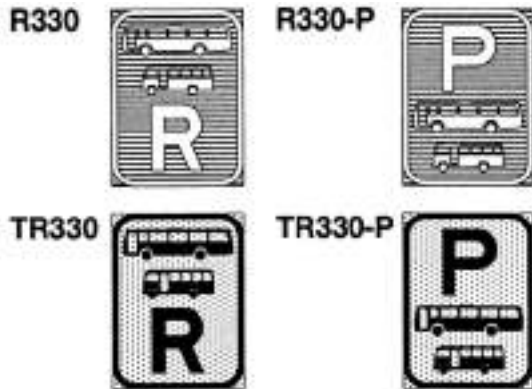


The START OF BUS AND MINIBUS LANE RESERVATION regulatory sign R329 indicates **the beginning of a conditional requirement to drivers of vehicles that the portion of public road marked as a bus and minibus lane by EXCLUSIVE USE LANE LINE marking RM9 and the BUS word marking RM17.2 is reserved for buses and minibuses only**.

BUS AND MIDI-BUS RESERVATION

COLOURS: *For dimensions*
 PERMANENT *ref. Vol. 4 page*
 Border & symbols: *White retroreflective or semi-matt*
 Background: *Blue retroreflective or semi-matt* 2.5.30

TEMPORARY
 Border & symbols: *Black semi-matt*
 Background: *Yellow retroreflective*



The BUS AND MIDI-BUS RESERVATION regulatory sign R330 indicates **a conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for buses and midi-buses only.** Sign R330-P indicates **a conditional requirement to drivers of vehicles that a parking area is reserved for buses and midi-buses only.**

BUS AND MIDI-BUS LANE RESERVATION

COLOURS: *For dimensions*
 PERMANENT *ref. Vol. 4 page*
 Border & symbols: *White retroreflective or semi-matt Lane*
 Lines: *Yellow retroreflective or semi-matt*
 Background: *Blue retroreflective or semi-matt* 2.5.31

TEMPORARY
 Border & symbols: *Black semi-matt*
 Background: *Yellow retroreflective*

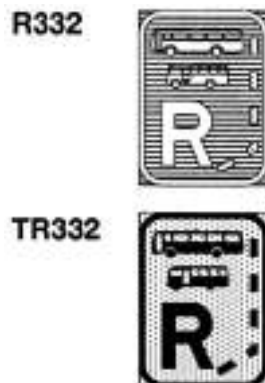


The BUS AND MIDI-BUS LANE RESERVATION regulatory sign R331 indicates **a conditional requirement to drivers of vehicles that the portion of public road marked as a bus and midi-bus lane by EXCLUSIVE USE LANE LINE marking RM9 and the BUS word marking RM17.2 is reserved for buses and midi-buses only.**

START OF BUS AND MIDI-BUS LANE RESERVATION

COLOURS: *For dimensions*
 PERMANENT *ref. Vol. 4 page*
 Border & symbols: *White retroreflective or semi-matt Lane*
 Lines: *Yellow retroreflective or semi-matt*
 Background: *Blue retroreflective or semi-matt* 2.5.32

TEMPORARY
 Border & symbols: *Black semi-matt*
 Background: *Yellow retroreflective*



The START OF BUS AND MIDI-BUS LANE RESERVATION regulatory sign R332 indicates **the beginning of a conditional requirement to drivers of vehicles that the portion of public road marked as a bus and midi-bus lane by EXCLUSIVE USE LANE LINE marking RM9 and the BUS word marking RM17.2**

BUS, MIDI-BUS AND MINIBUS RESERVATION

COLOURS: *For dimensions*
 PERMANENT *ref. Vol. 4 page*
 Border & symbols: *White retroreflective or semi-matt*
 Background: *Blue retroreflective or semi-matt* 2.5.33



The BUS, MIDI-BUS AND MINIBUS RESERVATION regulatory sign R333 indicates **a conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for buses, midi-buses and minibuses only.** Sign R327-P indicates **a conditional requirement to drivers of vehicles that a parking area is reserved for buses, midi-buses and minibuses only.** Temporary versions of these signs, numbered TR333 and TR333-P may be used if required.

BUS, MIDI-BUS AND MINIBUS LANE RESERVATION

For dimensions ref. Vol. 4 page 2.5.34

COLOURS:
PERMANENT
Border & symbols: White retroreflective or semi-matt
Lane Lines: Yellow retroreflective or semi-matt
Background: Blue retroreflective or semi-matt



R334

The BUS, MIDI-BUS AND MINIBUS LANE RESERVATION regulatory sign R334 indicates a **conditional requirement to drivers of vehicles that the portion of public road marked as a bus, midi-bus and minibus lane by EXCLUSIVE USE LANE LINE marking RM9 and the BUS word marking RM17.2 is reserved for buses, midi-buses and minibuses only. Temporary versions of these signs, numbered TR334 and TR334-P may be used if required.**

START OF BUS, MIDI-BUS AND MINIBUS LANE RESERVATION

For dimensions ref. Vol. 4 page 2.5.35

COLOURS:
PERMANENT
Border & symbols: White retroreflective or semi-matt
Lane Lines: Yellow retroreflective or semi-matt
Background: Blue retroreflective or semi-matt



R335

The START OF BUS, MIDI-BUS AND MINIBUS LANE RESERVATION regulatory sign R335 indicates **the beginning of a conditional requirement to drivers of vehicles that the portion of public road marked as a bus, midi-bus and minibus lane by EXCLUSIVE USE LANE LINE marking RM9 and the BUS word marking RM17.2. Temporary versions of these signs, numbered TR335 and TR335-P may be used if required.**

HIGH OCCUPANCY VEHICLE LANE RESERVATION

For dimensions ref. Vol. 4 page 2.5.36

COLOURS:
PERMANENT
Border & symbols: White retroreflective or semi-matt
Legend: Black semi-matt or blue retroreflective "B MOD"
Lane Line: Yellow retroreflective or semi-matt
Background: Blue retroreflective or semi-matt
TEMPORARY
Border & symbols: Black semi-matt
Legend: White or yellow retroreflective "BMOD"
Background:



R336



TR336

The HIGH OCCUPANCY VEHICLE LANE (HOV) RESERVATION regulatory sign R336 indicates a **conditional requirement to drivers of vehicles that the portion of public road marked as a high occupancy vehicle lane by EXCLUSIVE USE LANE LINE marking RM9 and HOV symbol marking RM17.4 is reserved for high occupancy vehicles only.**

START OF HIGH OCCUPANCY VEHICLE LANE RESERVATION

For dimensions ref. Vol. 4 page 2.5.37

COLOURS:
PERMANENT
Border & symbols: White retroreflective or semi-matt
Legend: Black semi-matt or blue retroreflective "B MOD"
Lane Lines: Yellow retroreflective or semi-matt
Background: Blue retroreflective or semi-matt
TEMPORARY
Border & symbols: Black semi-matt
Legend: White or yellow retroreflective "BMOD"



R337



TR337

The START OF HIGH OCCUPANCY VEHICLE LANE RESERVATION regulatory sign R337 indicates **the beginning of a conditional requirement to drivers of vehicles that the portion of public road marked as a high occupancy vehicle lane by EXCLUSIVE USE LANE LINE marking RM9 and HOV symbol marking RM17.4 is reserved for high occupancy vehicles only.**

TRAM RESERVATION

TRAM LANE RESERVATION

COLOURS:
PERMANENT
Border & symbols: White retroreflective or semi-matt
Background: Blue retroreflective or semi-matt

For dimensions
ref. Vol. 4
page
2.5.38

R338



The TRAM RESERVATION regulatory sign R338 indicates **a conditional requirement to drivers that the public road or portion of public road is reserved for trams only.**

COLOURS:
PERMANENT
Border & symbols: White retroreflective or semi-matt
Lane Lines: Yellow retroreflective or semi-matt
Background: Blue retroreflective or semi-matt

For dimensions
ref. Vol. 4
page
2.5.39

R339



The TRAM LANE RESERVATION regulatory sign R339 indicates **a conditional requirement to drivers of vehicles that the portion of public road marked as a tram lane by EXCLUSIVE USE LANE LINE marking RM9 and the TRAM word marking RM17.2 is reserved for trams only.**

START OF TRAM LANE RESERVATION

BUS AND TRAM RESERVATION

COLOURS:
PERMANENT
Border & symbols: White retroreflective or semi-matt
Lane Lines: Yellow retroreflective or semi-matt
Background: Blue retroreflective or semi-matt

For dimensions
ref. Vol. 4
page
2.5.40

R340



The START OF TRAM LANE RESERVATION regulatory sign R340 indicates **the beginning of a conditional requirement to drivers of vehicles that the portion of public road marked as a tram lane by EXCLUSIVE USE LANE LINE marking RM9 and the TRAM word marking RM17.2 is reserved for trams only.**

COLOURS:
PERMANENT
Border & symbols: White retroreflective or semi-matt
Background: Blue retroreflective or semi-matt

For dimensions
ref. Vol. 4
page
2.5.42

R342



The BUS AND TRAM RESERVATION regulatory sign R342 indicates **a conditional requirement to drivers of vehicles that the public road or portion of public road is reserved for buses and trams only.**

BUS AND TRAM LANE RESERVATION

For dimensions
ref. Vol. 4
page

2.5.43

COLOURS:
PERMANENT

Border & symbols: White retroreflective or semi-matt Lane
Lines: Yellow retroreflective or semi-matt
Background: Blue retroreflective or semi-matt



R343

The BUS AND TRAM LANE RESERVATION regulatory sign R343 indicates **a conditional requirement to drivers of vehicles that the portion of public road marked as a bus and tram lane by EXCLUSIVE USE LANE LINE marking RM9 and the BUS and TRAM word markings RM17.2 is reserved for buses and trams only.**

START OF BUS AND TRAM LANE RESERVATION

For dimensions
ref. Vol. 4
page

2.5.44

COLOURS:
PERMANENT

Border & symbols: White retroreflective or semi-matt Lane
Lines: Yellow retroreflective or semi-matt
Background: Blue retroreflective or semi-matt



R344

The START OF BUS AND TRAM LANE RESERVATION regulatory sign R344 indicates **the beginning of a conditional requirement to drivers of vehicles that the portion of public road marked as a bus and tram lane by EXCLUSIVE USE LANE LINE marking RM9 and the BUS and TRAM word markings RM17.2 is reserved for buses and trams only.**

BUS, TRAM AND MINIBUS RESERVATION

For dimensions
ref. Vol. 4
page

2.5.45

COLOURS:
PERMANENT

Border & symbols: White retroreflective or semi-matt
Background: Blue retroreflective or semi-matt



R345

The BUS, TRAM AND MINIBUS RESERVATION regulatory sign R345 indicates **a conditional requirement to drivers of vehicles that the portion of public road marked as a bus and minibus and tram lane by EXCLUSIVE USE LANE LINE marking RM9 and the BUS and TRAM word markings RM17.2 is reserved for buses and minibuses and trams only.**

BUS, TRAM AND MINIBUS LANE RESERVATION

For dimensions
ref. Vol. 4
page

2.5.46

COLOURS:
PERMANENT

Border & symbols: White retroreflective or semi-matt Lane
Lines: Yellow retroreflective or semi-matt
Background: Blue retroreflective or semi-matt

TEMPORARY

Border & symbols: Black semi-matt
Background: Yellow retroreflective



R346

The BUS, TRAM AND MINIBUS LANE RESERVATION regulatory sign R346 indicates **a conditional requirement to drivers of vehicles that the portion of public road marked as a bus and minibus and tram lane by EXCLUSIVE USE LANE LINE marking RM9 and the BUS word marking RM17.2 is reserved for buses and minibuses and trams only.**

START OF BUS, TRAM AND MINIBUS LANE RESERVATION

COLOURS:		For dimensions
PERMANENT		ref. Vol. 4
Border & symbols:	White retroreflective or semi-matt Lane	page
Lines:	Yellow retroreflective or semi-matt	2.5.47
Background:	Blue retroreflective or semi-matt	

R347



The START OF BUS, TRAM AND MINIBUS LANE RESERVATION regulatory sign R347 indicates **the beginning of a conditional requirement to drivers of vehicles that the portion of public road marked as a bus and mini-bus and tram lane by EXCLUSIVE USE LANE LINE marking RM9 and the BUS word marking RM17.2 is reserved for buses and minibuses and trams only.**

BUS LANE - RIGHT RESERVATION

COLOURS:		For dimensions
PERMANENT		ref. Vol. 4
Border & symbols:	White retroreflective or semi-matt Lane	page
Lines:	Yellow retroreflective or semi-matt	2.5.48
Background:	Blue retroreflective or semi-matt	
TEMPORARY		2.5.48
Border & symbols:	Black semi-matt	
Background:	Yellow retroreflective	

R348



TR348



The BUS LANE - RIGHT RESERVATION regulatory sign R348 indicates **the same conditional requirement as sign R302 for a bus lane located on the right side of a public road or portion of public road.**

BUS AND MINIBUS LANE - RIGHT RESERVATION

COLOURS:		For dimensions
PERMANENT		ref. Vol. 4
Border & symbol:	White retroreflective or semi-matt	page
Lane Lines:	Yellow retroreflective or semi-matt	
Background:	Blue retroreflective or semi-matt	2.5.49
TEMPORARY		
Border & symbol:	Black semi-matt	
Background:	Yellow retroreflective	

R349



TR349



The BUS AND MINIBUS LANE- RIGHT RESERVATION regulatory sign R349 indicates **the same conditional requirement as sign R328 for a bus and minibus lane located on the right side of a public road or portion of public road.**

BUS AND TRAM LANE - RIGHT RESERVATION

COLOURS:		For dimensions
PERMANENT		ref. Vol. 4
Border and symbols:	White retroreflective or semi-matt	page
Lane Lines:	Yellow retroreflective or semi-matt	2.5.50
Background:	Blue retroreflective or semi-matt	

R350



The BUS AND TRAM LANE - RIGHT RESERVATION regulatory sign R350 indicates **the same conditional requirement as sign R343 for a bus and tram lane located on the right side of a public road or portion of public road.**

BUS, TRAM AND MINIBUS LANE-RIGHT RESERVATION

For dimensions ref. Vol. 4 page	COLOURS: PERMANENT Border & symbol: White retroreflective or semi-matt Lane Lines: Yellow retroreflective or semi-matt Background: Blue retroreflective or semi-matt
2.5.51	TEMPORARY Border & symbol: Black semi-matt Background: Yellow retroreflective



R351

The BUS, TRAM AND MINIBUS AND TRAM LANE - RIGHT RESERVATION regulatory sign R351 indicates **the same conditional requirement as sign R346 for a bus, tram and minibus lane located on the right side of a public road or portion of public road.**

HIGH OCCUPANCY VEHICLE LANE-RIGHT RESERVATION

For dimensions ref. Vol. 4 page	COLOURS: PERMANENT Border & symbols: White retroreflective or semi-matt Legend: Black semi-matt or blue retroreflective "B MOD"
2.5.52	Lane Lines: Yellow retroreflective or semi-matt Background: Blue retroreflective or semi-matt
	TEMPORARY Border & symbols: Black semi-matt Legend: White or yellow retroreflective "BMOD"
	Background: Yellow retroreflective



R352



TR352

The HIGH OCCUPANCY VEHICLE LANE - RIGHT RESERVATION regulatory sign R352 indicates **the same conditional requirement as sign R336 for a high occupancy vehicle lane located on the right side of a public road or portion of public road.**

AUTHORISED PASSENGER TRANSPORT VEHICLE LANE RESERVATION

For dimensions ref. Vol. 4 page	COLOURS: PERMANENT Border & symbols: White retroreflective or semi-matt Lane Lines: Yellow retroreflective or semi-matt Background: Blue retroreflective or semi-matt
2.5.53	TEMPORARY Border & symbols: Black semi-matt Background: Yellow retroreflective



R353

The AUTHORISED PASSENGER TRANSPORT VEHICLE LANE RESERVATION regulatory sign R353 indicates **a conditional requirement to drivers of vehicles that the portion of public road marked as a passenger transport lane by EXCLUSIVE USE LANE LINE marking RM9 is reserved for authorised passenger transport vehicles only.** Authorisation as a passenger transport vehicle can only be granted by the road authority responsible for the public road.

AUTHORISED PASSENGER TRANSPORT VEHICLE LANE – RIGHT RESERVATION

For dimensions ref. Vol. 4 page	COLOURS: PERMANENT Border & symbols: White retroreflective or semi-matt Lane Lines: Yellow retroreflective or semi-matt Background: Blue retroreflective or semi-matt
2.5.54	TEMPORARY Border & symbols: Black semi-matt Background: Yellow retroreflective



R354

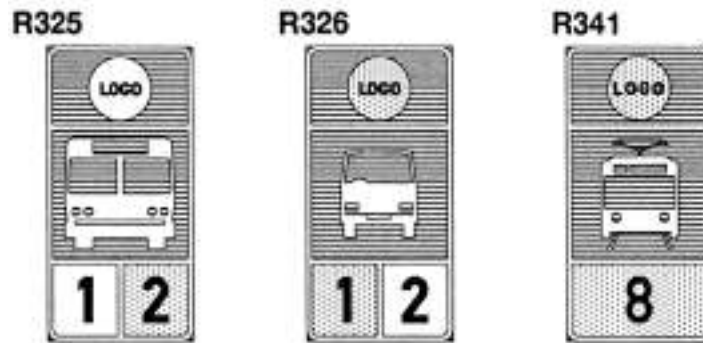
The AUTHORISED PASSENGER TRANSPORT VEHICLE LANE- RIGHT RESERVATION regulatory sign R354 indicates **the same conditional requirement as sign R353 for a passenger transport lane located on the right side of a public road or portion of public road.**

"STOP" RESERVATION

COLOURS:

PERMANENT

Border, symbol: White retroreflective or semi-matt
 Logo: Full colour
 Lower Panel: Colour coded
 Background: Blue retroreflective or semi-matt



For dimensions
 ref. Vol. 4
 page

2.5.27

2.5.9 "Stop" Reservation

- The BUS STOP RESERVATION regulatory sign R325, MINIBUS STOP RESERVATION regulatory sign R326 and TRAM STOP RESERVATION regulatory sign R341 indicate a conditional requirement to drivers of vehicles that the portion of public road is reserved as a stop for passenger transport vehicles of the organisation indicated by means of a crest, logo or other device on such sign, only. Signs R325, R326 and R341 also indicate a conditional requirement to drivers of passenger transport vehicles that they shall only stop at such signs for the purpose of immediately taking-up or dropping-off passengers and shall not wait or park at such sign unless it is marked "TERMINUS".

In addition the following apply:

- if one or more buses, minibuses or trams are already stopped at the relevant sign, subsequent vehicles may also stop in a safe manner and in compliance with any other relevant road traffic sign (see paragraph 2.5.9.3);
 - if the stop is shared with another class or classes of passenger transport vehicle drivers of such other class(es) of vehicle may also stop at signs R325, R326 and R341 as appropriate;
 - an authority may designate signs R325, R326 or R341 as a "TERMINUS" by modifying the "zone" panel in which case the driver may await a scheduled departure time.
- Signs R325, R326 and R341 should be located on the left side of a roadway. The principles involved in the siting of passenger transport stops are outside the scope of this Manual. However, care should be taken to minimise obstruction to other traffic when siting stops. When bus or minibus stops are located on rural roadways sufficient space shall be available for vehicles to pull well clear of the travelled way without significantly slowing so that following traffic is obstructed. Such stops shall be located where a stopped vehicle will be clearly visible. In rural situations consideration should be given to the use of information BUS STOP AHEAD sign IN16 (see Chapter 5, Subsection 5.2.11).
 - It should be noted that **BUS STOP (R325), MINIBUS STOP (R326) and TRAM STOP (R341) RESERVATION signs override the provisions of other regulatory signs such as NO PARKING sign R216 and NO STOPPING sign R217, without the**

need to qualify R216 and R217 signs with a supplementary plate such as are indicating "EXCEPT BUSES". A "STOP" RESERVATION sign indicating "TERMINUS" shall not be placed in a NO PARKING or NO STOPPING zone.

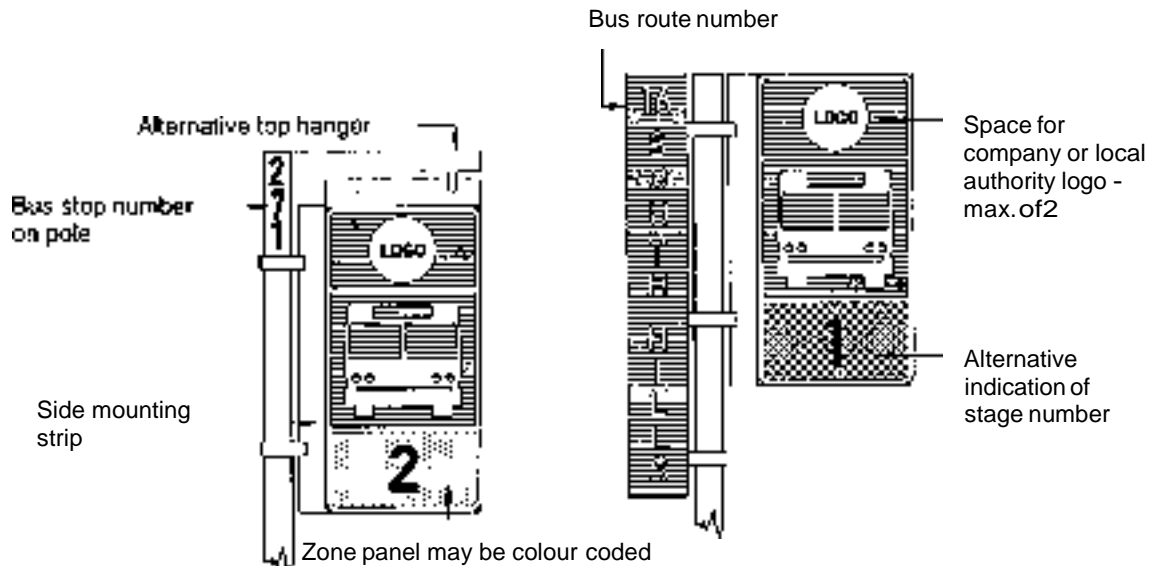
- The sizing of BUS STOP, MINIBUS STOP and TRAM STOP RESERVATION signs is unique. The sizes range from a minimum height x width of 450 mm x 225 mm suitable for low speed uncluttered urban environments including bus termini, to a maximum height x width of 1200 mm x 600 mm suitable for rural high speed roads.
- Signs R325, R326 and R341 may be supplemented by additional plates according to the policies of individual passenger transport authorities without affecting the legal significance of the basic sign. The standard format of signs R325, R326 and R341 shall convey the following information:
 - in the top panel: the identity of the operator for exclusive bus stops, preferably in the form of a compact LOGO which may use the standard colours of the operator, OR two logos if two operators share the stop, OR words such as "ALL BUSES" if all operators on a route may use the stop. (The letter size appropriate to text in this panel is 35 mm in the DIN 1451 Style "A" lettering);
 - in the central panel: the approved front view pictogram of a bus, minibus or tram;
 - in the lower panel: INFORMATION relating to one or more of the typical factors relevant to scheduled passenger transport operation such as zone, stage, fare, stop number or service name or number; the amount of information should be limited to ensure adequate legibility; the use of colour coding for the background of this panel is recommended.
- In designing a passenger transport stop signing system it is recommended that the information to be displayed in this lower panel be carefully considered to avoid unnecessary clutter on the sign. Typical examples of how information may be given are illustrated in Figure 2.9. The amount of information to be given in this lower panel may dictate the overall sign size to be used throughout a system or part of a system.
- "STOP" RESERVATION signs shall be manufactured

so that both faces of the sign display the same information and shall be mounted at 90° to the direction of travel of traffic to be visible to pedestrians and drivers.

- 8 According to individual policies, passenger transport authorities may combine one or more services, operators or modes of transport at one stopping place. If for instance buses and minibuses are to be permitted to stop at the same place BUS STOP RESERVATION sign R325 and MINIBUS STOP RESERVATION sign R326 should be mounted on the same post (see Figure 2.10).

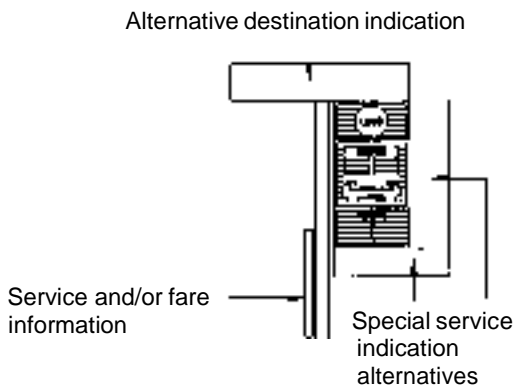
- 9 When laybys are provided, sign R325, R326 or R341 should be located at the forward, or exit, end of the layby. If the layby is designed for multiple stops it is recommended that signs be located at the correct intervals to accommodate several vehicles at the desired spacing.

- 10 A temporary version of signs R325, R326 and R341 are unlikely to be required. If a need does arise due to temporary bus route relocation as a result of roadworks or building operations a temporary version TR325 may be provided in the appropriate black on yellow colours,

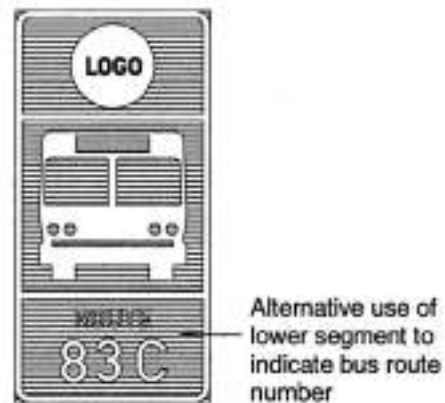


Detail 2.9.1 Alternative Hanging Methods

Detail 2.9.2 Destination Indication at Terminal



Detail 2.9.3 Alternative Methods of Providing Additional Passenger Information



Detail 2.9.4 Route Number Alternative

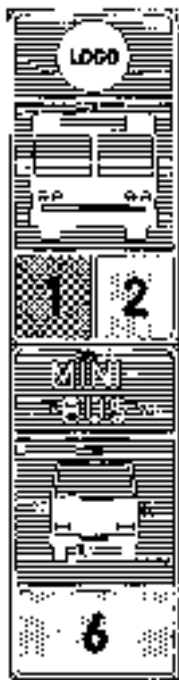
Fig. 2.9 Supplementary Bus Stop Information

OR the standard signs may be temporarily relocated from the original route.

- 11 Holding areas, where taking up or dropping off passengers is not permitted either on-street or off-street, shall not be signed using signs R325, R326 or R341. BUS or MINIBUS PARKING RESERVATION signs R301-P or R310-P should be used for such areas (see Subsection 2.5.1).
- 12 "STOP" RESERVATION signs are appropriate for use within termini and modal transfer points. Access to termini should be reserved for relevant classes of vehicle only by BUS RESERVATION sign R301, MINI-BUS RESERVATION sign R310 or TRAM RESERVATION sign R338 (see Subsections 2.5.1 and 5.2.12).
- 13 The degree to which the activities of minibus drivers will require to be regulated in the future is not clear. It is anticipated that there will be an eventual need to designate some if not all stopping places for minibuses in the interests of safety and traffic management and control. Sign R326 has been provided for this purpose.

Until such time as a formalisation of minibus operators with respect to the use of stopping places is required if ever, it is recommended that the top panel contain the text "MINIBUS" in two lines of lettering 42 mm high and in the DIN 1451 Style "B". This may be replaced by an appropriate LOGO if stopping places are allocated on an operator basis.

- 14 If it is required to provide on-street RANKS where minibuses may wait to take-up passengers MINIBUS RESERVATION sign-R310 should be used in preference to sign R326. Sign R310 may be used as a SELECTIVE RESTRICTION sign with a LIMITED NUMBE.RS secondary message sign (R)536 indicating the max1mum number of minibuses which may occupy the rank at one time.
- 15 Examples of the combined display of BUS STOP RESERVATION sign R325, MINIBUS STOP RESERVATION sign R326 and TRAM STOP RESERVATION sign R341 are given in Figure 2.10.



Detail2.10.1 Shared Stop Signs (VERTICAL MOUNTING)



Detail2.10.2 Shared Stop Signs (SIDE-BY-SIDE MOUNTING)

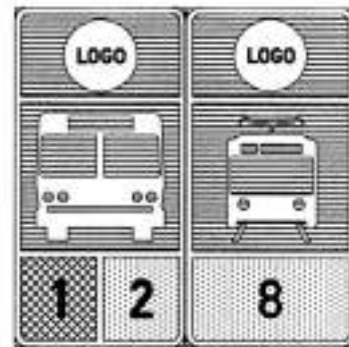


Fig. 2.10 Examples of Minibus Stop And Bus Stop Sign Combinations

DUAL CARRIAGEWAY FREEWAY BEGINS

COLOURS:
PERMANENT

Border: Red retroreflective
Symbol: White retroreflective
Background: Blue retroreflective

R401

For dimensions
ref. Vol. 4
page



2.6.1

2.6.1 Dual Carriageway Freeway Begins

- 1 The DUAL CARRIAGEWAY FREEWAY BEGINS regulatory sign R401 indicates **to drivers of vehicles that a dual carriageway freeway begins and that specific legislation becomes applicable on the section of public road beyond such sign.** This has the effect that in South Africa drivers shall comply with a comprehensive range of regulations given in the Road Traffic Act, specific to the use of dual carriageway freeways.
- 2 **It should be noted that it is the display of sign R401 or R402 which designates a roadway as a freeway in terms of legislation.** For signing purposes a dual carriageway freeway is designated as a Class A1 road and a single carriageway freeway as a Class A2 road (see Chapter 1).
- 3 Sign R401 should be displayed on the left-hand side of on-ramps to a Class A1 freeway provided that if the on-ramp has more than one lane a second sign may be displayed on the right-hand side of the on-ramp roadway. Sign R401 should also be displayed on the left and right-hand sides of a freeway carriageway when this roadway is created from the continuation of a lower class road, including a Class A2 freeway. In this latter case sign R401 may be displayed with a SUPPLEMENTARY PLATE sign IN11.3, in advance of this point to indicate the distance to the start of the Class A1 freeway.
- 4 The special provisions relating to freeways which are brought in to force by sign R401 are covered in legislation. The following items are a summary of the provisions of this legislation. **For full details the legislation must be consulted.**

(a) No person shall operate on a freeway:

- (i) a vehicle drawn by an animal;
- (ii) a pedal cycle;
- (iii) a motorcycle with a cylinder capacity not exceeding 50 cm³ (c.c.) or which is propelled by electric power;
- (v) a motor tricycle;
- (v) a vehicle with a mass not greater than 230 kg and specially designed, constructed or adapted for the use of a person suffering from a physical defect or disability; or
- (vi) a tractor;

(b) No person shall :

- (i) be on a freeway on foot except :
 - (1) within an area reserved for the stopping or parking of vehicles by an appropriate road traffic sign (this provision could include rest areas within the freeway reservation);
 - (2) for a cause beyond the control of that person; or
- (ii) leave or allow an animal to be on a freeway, except in or on a motor vehicle or within an area reserved for the stopping or parking of vehicles by an appropriate road traffic sign, **OR leave an animal in a place where it may stray onto a freeway;**
- (iii) stop a vehicle on a freeway, except:
 - (1) in compliance with a road traffic sign or a direction given by a traffic officer;
 - (2) within an area reserved for the stopping or parking of vehicles by an appropriate road traffic sign;
 - (3) for a cause beyond the control of that person; or
- (iv) give a hand signal when driving a motor vehicle on a freeway, except for a cause beyond the control of the driver;
- (v) cause a vehicle on a freeway to travel in reverse, except:
 - (1) in compliance with a direction given by a traffic officer;
 - (2) within an area reserved for the stopping or parking of vehicles by an appropriate road traffic sign; or
 - (3) for a cause beyond the control of that person;
- (vi) cross the median between carriageways of a divided freeway, and/or cause a vehicle to execute a U-turn on a freeway except :
 - (1) in compliance with a direction given by a traffic officer; or
 - (2) for a cause beyond the control of that person;

- (vi) cause a vehicle to travel on a roadway shoulder of a freeway in order to pass a slower moving vehicle.
- 5 The requirements listed in paragraph 2.6.1.4 may be varied during the course of temporary roadworks. It is recommended that the status of freeway be retained under roadworks conditions to maintain those of the requirements listed which are necessary even under temporary conditions. Any other variations may be indicated by relevant additional temporary signs. If roadworks require that one carriageway of a Class A1 freeway is closed to traffic and that the other carriageway shall operate with two-way traffic, the use of a temporary Class A2 freeway sign TR402 is recommended. The additional use of a temporary FREEWAY (CLASS A1) DE-RESTRICTION sign TR601 is optional (see Section 2.9).
 - 6 The speed limit applicable to a freeway is catered for under the general speed limit legislation as amended from time to time. If it is required, a lower speed limit may be indicated by an appropriate version of the SPEED LIMIT sign -R201 or TR201. A speed de-restriction sign shall not be used for this purpose.
 - 7 Sign R401 should be sized in accordance with Table 2.4. in Section 2.1.

SINGLE CARRIAGEWAY FREEWAY BEGINS

For dimensions
ref. Vol. 4
page

2.6.2

R402



TR402



COLOURS:

PERMANENT

Border:

Symbol:

Background:

TEMPORARY

Border:

Symbol:

Background:

Red retroreflective
White retroreflective
Blue retroreflective

Red retroreflective
Black semi-matt
Yellow retroreflective

2.6.2 Single Carriageway Freeway Begins

- 1 The SINGLE CARRIAGEWAY FREEWAY BEGINS regulatory sign R401 indicates **to drivers of vehicles that a single carriageway freeway begins and that specific legislation becomes applicable on the section of public road beyond such sign**. This has the effect that drivers shall comply with a comprehensive range of regulations given in the Road Traffic Act, specific to the use of single carriageway freeways.
- 2 It should be noted that it is the display of sign R401 or R402 which designates a roadway as a freeway in terms of legislation. For signing purposes a single carriageway freeway is designated as a Class A2 road and a dual carriageway freeway as a Class A1 road (see Chapter 1).
- 3 Sign R402 should be displayed on the left-hand side of on-ramps to a single carriageway freeway and at the commencement of this class of road when it is formed as a continuation of a lower class roadway, or of a Class A1 freeway. Sign R402 may be displayed with a SUPPLEMENTARY PLATE sign IN11.3, in advance of the start of the Class A2 freeway, either on a Class A1 freeway or on a Class B roadway, to indicate the distance to the start of the Class A2 freeway.
- 4 The provisions of paragraph 2.6.1.4 shall apply *mutatis mutandis* to the use of sign R402, with the exception of paragraph 2.6.1.4 (b)(vi) which shall have the requirement for this class of freeway that no person shall cause a vehicle to execute a U-turn.
- 5 The requirements listed in paragraph 2.6.1.4 may be varied during the course of temporary roadworks. It is recommended that freeway status be retained under roadworks conditions to maintain those of the listed requirements which are necessary even under temporary conditions. Any other variations may be indicated by relevant additional temporary signs. Sign TR402 is recommended for use when a Class A1 freeway is operating with two-way traffic on one of its carriageways and the other carriageway is closed to traffic to indicate this downgraded status.
- 6 Signs R402 and TR402 should be sized in accordance with Table 2.4 in Section 2.1.

COLOURS:
PERMANENT

Border: Red retroreflective
Symbol: White retroreflective
Background: Blue retroreflective

R401

For dimensions
ref. Vol. 4
page

2.6.1



2.6.3 Woonerf

- 1 The WOONERF regulatory sign R403 indicates **comprehensive requirements that drivers of vehicles shall:**
 - (a) **not enter the area driving a vehicle with a gross vehicle mass exceeding 3 500 kg and/or more than 10 seats for passengers, except for local access or delivery;**
 - (b) **yield right of way to pedestrians, and children who may be in the roadway;**
 - (c) **observe a general speed limit of 30 km/h within the area unless another speed limit is**

- Indicated by a road sign; and**
- (c) **not enter the area by vehicle and drive through the area to exit at another point or the same point without breaking their journey.**
 - 2 Sign R403 should be displayed on the left side of all points of access to all residential areas declared for the purposes of traffic control as "WOONERF".
 - 3 Sign R403 should be sized in accordance with Table 2.4 in Section 2.1.

2.7 SELECTIVE RESTRICTION SIGNS

2.7.1 General

- 1 A description of SELECTIVE RESTRICTION regulatory signs is given in the introductory Section of this Chapter. Signs of this type have been in use for many years without being specifically designated "selective". The technical review which resulted in the preparation of this edition of the Manual identified a need for a significant and increasing number of signs of this type. A formal grouping within the regulatory sign classification has therefore been created to cater for this need. **The basis of SELECTIVE RESTRICTION signs is that the message given by a CONTROL, COMMAND, PROHIBITION or RESERVATION regulatory sign is NOT universally applicable to either all traffic, or for all times of the day and the night.**
- 2 Each SELECTIVE RESTRICTION sign is a combination of two messages which, with a few exceptions, have already been dealt with individually in the sign descriptions covered earlier in this Chapter. (See Sub-section 2.7.10 for details of exceptions or exclusive secondary messages).
- 3 A SELECTIVE RESTRICTION sign qualifies: the applicability of the PRIMARY message by the introduction of a SECONDARY message. A MANDATORY COM- MAND sign or a MANDATORY PROHIBITION sign may, for instance, be qualified to become applicable selectively for a part of the day (**limit**), or selectively to a specific action (**action**), or to a selective class of vehicle (**object**). A CONDITIONAL RESERVATION sign may be qualified to become applicable for a selective time of day or to a selective class of vehicle.
- 4 In principle there are a great many possible primary and secondary message combinations. In practice, however, very many of these have no practical use. In this Section the signs described are representative examples. All possible message combinations are not covered (see Subsections 2.1.4, 2.7.4 and 2.7.15 to 2.7.20).
- 2 Time limitations on the applicability of a selective restriction sign shall also appear in a horizontal rectangular sign, as secondary messages. Any legend shall be indicated in letters and figures in the DIN 1451 Part2 Styles "A" or "B".
- 3 In a few instances the "secondary" message may in fact be a "tertiary" message e.g. a LIMITED PARKING RESERVATION sign R306-P may be applicable for a maximum duration of stay of 60 minutes AND for a limited time or times of day.

2.7.4 Message Combinations

- 1 It has been stated at the beginning of this Chapter that to regulate is to restrict, and a restriction can be applied in the form of a LIMIT, or it can be applied to an ACTION or an OBJECT. All primary message regulatory signs can be classified into one of these groups. Secondary message regulatory signs used with selective restriction signs can also with very few exceptions be classified into the same groups. The principle exceptions are the EXCLUSIVE SECONDARY MESSAGE signs which relate to circumstances not covered by primary messages, on command, prohibition or reservation signs.
- 2 Selective restriction signs, because they result from a combination of primary and secondary message regulatory signs, can be classified into combinations of the limit, action, and object groups. The most likely of these group combinations are given in Table 2.1 and the example signs for these combinations are covered in Subsections 2.7.15 to 2.7.20.
- 3 The message given by, say, a NO RIGHT TURN AHEAD sign R210 imposes a restriction on the movements, or ACTIONS, of all traffic at the junction ahead. By contrast, the message given by a SELECTIVE RESTRICTION sign which has a NO RIGHT TURN AHEAD primary message sign R210 qualified by the selective secondary message GOODS VEHICLES sign (R)568 imposes a restriction on the movements, or ACTIONS, of a specific class of vehicle, or OBJECT, and NOT all traffic. This sign message combination is therefore classified as an ACTION-OBJECT selective restriction sign message. All selective restriction signs can be classified in this way.
- 4 The significance of a selective restriction sign can be derived from the significance of its primary message segment in the following manner:
 - (a) the significance of a NO RIGHT TURN AHEAD sign R210, as given in Subsection 2.4.9 is that the sign imposes a mandatory requirement upon all drivers NOT to turn right at the junction or entrance ahead; the significance of a NO RIGHT TURN AHEAD for GOODS VEHICLES selective restriction sign R21G-568 is that the sign imposes a mandatory requirement upon drivers of **GOODS VEHICLES** NOT to turn right at the junction or entrance ahead; and,
 - (b) the significance of a LIMITED PARKING sign R306-P, as given in Subsection 2.5.4 is that the sign imposes a parking reservation of **limited duration at all times** of the day and night on the

2.7.2 Primary Messages

- 1 The primary message part of a selective restriction sign shall only utilize an approved regulatory sign as covered in Sections 2.1 to 2.6. The primary message shall appear on a CONTROL sign, or in a circular MANDATORY sign from the COMMAND or PROHIBITION groups, or in a limited number of cases in a vertical rectangular CONDITIONAL sign from the RESERVATION group. The latter type will commonly be required to sign the time limitation of the applicability of RESERVATION or PARKING signs. (See Subsection 2.7.9 with regard to CONTROL SELECTIVE RESTRICTION signs).

2.7.3 Secondary Messages

- 1 The secondary message shall always appear in a horizontal rectangular sign mounted below a circular or vertical rectangular primary message sign. The secondary message will commonly be portrayed by a symbol. Symbols representing a wide range of class of vehicle / have been developed as primary messages. These are also available as secondary messages (see Subsections 2.7.10 and 2.7.14).

use of portions of roadway or other areas set aside for the parking of vehicles, to **vehicles of less than 3500 kg** permissible maximum vehicle mass AND, sign R306-P places an obligation on users of an area so designated to make payment of the relevant charge for parking as may be indicated on parking meters, "Pay and Display" or in some other manner; the significance of a LIMITED PARKING-TIME LIMIT selective restriction sign R30H-P-501 is that the sign imposes a parking reservation of **limited duration between the times indicated** on the use ofetc.; whilst the significance of a LIMITED PARKING-MINIBUS selective restriction sign R306-P-565 is, however, that the sign imposes a parking reservation of limited duration at all times of the day and the night on the use of portions of roadway or other areas set aside for the parking of vehicles to minibuses ... etc.

2.7.5 Sign Descriptions

- 1 SELECTIVE RESTRICTION signs should be described in the same order as they are numbered, namely the primary message followed by the qualifying "selective" secondary message

e.g. NO LEFT TURN - (for) GOODS VEHICLES sign,
or
SPEED LIMIT- (due to) NIGHT-TIME.

2.7.6 Sign Numbering

- 1 The number used to order SELECTIVE RESTRICTION signs shall include the numbers of the two signs to be combined. The regulatory sign class designatory letter "A" may be omitted for the second segment of the number. The numbers should be given in the order "Primary-Secondary" e.g. for a speed limit applicable to buses only the sign number will be R201-5150. The exclusive secondary message signs have a number in the (R)500 series (see Subsection 2.7.10).

2.7.7 Sign Colours

- 1 The colour code of SELECTIVE RESTRICTION signs requires that the secondary message component of the sign use the same border, background and legend/symbol colours as the primary message sign. This requirement applies to both PERMANENT and TEMPORARY signs with the exception that a limited number of full colour symbols retain their colours when used as a secondary message (see Section 2.0 Figure 2.4).

2.7.8 Sign Manufacture

- 1 The meaning to be conveyed to drivers by a SELECTIVE RESTRICTION sign can be very significantly altered if the qualifying secondary message sign becomes detached from the primary message sign. **It is therefore strongly recommended that the sign parts be manufactured from a single piece of material.**
- 2 When a single piece of backing material is used for a circular mandatory selective restriction sign the unused portion of the primary message sign background may adopt the background colour of the primary sign or may be a neutral grey colour (see Subsections 2.7.15 to 2.7.20).

2.7.9 Selective Control Signs

- 1 The use of CONTROL signs as SELECTIVE RESTRICTION signs is not generally recommended. STOP signs R1, and YIELD sign R2 should not be qualified by the use of a secondary message related to a limit, action or object except as covered by Subsection 2.2.1.
- 2 NO ENTRY sign R3 should also not be combined with a secondary message such as a limit (mass limit, or time limit) or an object (bus, taxi, etc.) as this would result in traffic that is not affected by the secondary message driving past such signs. Such a situation should not be permitted to occur as it will result in a lack of credibility of, and disrespect for, the NO ENTRY sign R3.
- 3 The qualification of ONE WAY signs R4.1, R4.2 or R4.3 and PEDESTRIAN PRIORITY sign R5 by a secondary message is unlikely to be required.

2.7.10 Exclusive Secondary Messages

- 1 The exclusive secondary message signs as described in this Subsection and subsequently illustrated in Sub-Section 2.7.11 to 2.7.14 are arranged in the following sub-groups:
 - (a) Time Limit Sub-group;
 - (b) Action Sub-group;
 - (c) Text Message Sub-group;
 - (d) Symbol Sub-group.
- 2 The TIME LIMIT sub-group of exclusive secondary message signs are numbered from (R)501 and the times indicated shall be displayed in the international standard 24-hour manner as depicted in the range of typical examples.
- 3 TIME LIMIT selective restriction signs are widely used in urban areas for the regulation of stopping, parking, and other reservations. The time message to be given may include one, two or three periods of the day and may be varied for weekdays and Saturdays. Such time limit signs are also commonly used with PARKING RESERVATION signs. In certain cases this message may be a "tertiary" message (see paragraph 2.7.3.3).
- 4 The TIME LIMIT sub-group includes signs which refer to DAYTIME, (R)511, and to NIGHTTIME, (R)512. The signing requirement for the use of these exclusive secondary message signs is likely to involve the change of a significant regulation, such as a speed limit, when one time of the day becomes the other. **Such conditions are best dealt with by the use of VARIABLE MESSAGE signs (see Section 2.8 and Chapter 9).** Variable message signs are, however, expensive and SELECTIVE RESTRICTION signs may be used as an alternative. In this way a PERMANENT reduced speed limit may be specified during night-time by means of a SELECTIVE RESTRICTION sign. For the purposes of this sub-group the term "night-time" applies to that period between sunset and sunrise during which it is mandatory that vehicles shall only use a public roadway if their headlamps are switched on.
- 5 The ACTION sub-group signs are numbered from (R)520 and cover the directional qualification of a primary

- message sign so that the resultant SELECTIVE RESTRICTION sign applies to an intersecting roadway, use of the roadway being reserved for a specific class of vehicle. Certain ACTION sub-group signs display an arrow which, if combined for instance with a BUS RESERVATION sign R301, would result in a SELECTIVE RESTRICTION sign numbered H301-520 which would have the significance that travel in the intersecting roadway is reserved for buses in a direction from right to left.
- 6 The ACTION sub-group signs also includes the PAY AND DISPLAY-(R)523 regulatory exclusive secondary message sign which, when used in conjunction with a parking reservation sign in the R300-P series, requires drivers to make the appropriate payment for parking, in whatever manner or amount is prescribed for the area, and to display the receipt received in a visible position on, or within, the vehicle.
- 7 The TEXT MESSAGE sub-group signs are numbered from (R)530 and qualify the applicability of a primary message sign so that the sign is not applicable to all vehicles of the class indicated by the text message. TEXT MESSAGES comprise such statements as:
- (a) the logo of a public transport company;
 - (b) the name of a public transport operator/company;
 - (c) a numerical value such as:
 - (i) a mass limit; or
 - (ii) a maximum number of vehicles;
 - (d) approved messages such as:
 - (i) "up to 125 cc";
 - (ii) "For 5 km".
- 8 **Only approved messages recorded in this Manual shall be used in the manner contemplated by paragraph 2.7.10.7.** A BUS RESERVATION sign R301 combined with an OPERATOR sign (R)531, jointly numbered R301-531, could be used as a SELECTIVE RESTRICTION sign at the access to a bus terminus which is reserved for buses of one specific operator. This sub-group of signs may display the LOGO of the authority or operator in full colour, or may indicate the name of the authority or operator in text, in full or in a commonly used abbreviated form.
- 9 The MAXIMUM NUMBER OF VEHICLES LIMIT text message sign is numbered (R)540 and may be used to qualify the applicability of a primary message sign to put a limit on the number of vehicles which may use the facility covered by the combined SELECTIVE RESTRICTION sign. Use of the sign is appropriate mainly to signs in the RESERVATION category and particularly the parking examples. An appropriate application of such a sign would be in combination with a MINIBUS PARKING RESERVATION sign resulting in a SELECTIVE RESTRICTION sign numbered R310-P-540. Such a sign would have the significance that the number of minibuses which may use the parking area designated by the sign is limited to the number indicated on the sign (R)540. This will enable overcrowding of a facility to be prevented.
- 10 The SYMBOL sub-group signs are numbered from (R)560 and cover various classes of vehicle so that the resultant SELECTIVE RESTRICTION sign applies the regulation given by the primary message only to the OBJECT or class of vehicle depicted in the exclusive secondary message sign. In this way a SPEED LIMIT sign R201 may be made applicable only to VEHICLES CONVEYING DANGEROUS GOODS by means of a sign numbered R201-571.
- 11 It is quite likely that the use of exclusive secondary message signs from these sub-groups may be used with TEMPORARY SELECTIVE RESTRICTION signs during roadworks and/or detours resulting from roadworks or building operations. When used in this manner the signs should be manufactured in the colours appropriate to the primary message sign with which they are combined.
- 12 Examples of some of the various SELECTIVE RESTRICTION combinations of a PRIMARY message sign together with an EXCLUSIVE SECONDARY message sign are illustrated in Subsections 2.7.15 to 2.7.20.

2.7.11 TIME LIMIT Sub-Group

For dimensions
ref. Vol. 4
pages
2.7.1 to 2.7.8

COLOURS:
PERMANENT AND TEMPORARY
As for primary sign EXCEPT
(R)512 - refer to Section 2.0

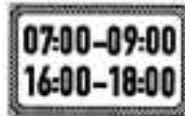
ONE TIME PERIOD

(R)501



TWO TIME PERIODS

(R)502



TWO TIME PERIODS/DAYS

(R)503



THREE TIME PERIODS/DAYS

(R)504



MAXIMUM STAY DURING TWO TIME PERIODS/DAYS

(R)505



MAXIMUM STAY DURING ONE TIME PERIOD

(R)506



(R)507

Number
not
allocated

(R)508

Number
not
allocated

(R)509

Number
not
allocated

(R)510

Number
not
allocated.

DAYTIME
(R)511



NIGHTTIME
(R)512



- 1 The TIME LIMIT exclusive secondary message regulatory signs (R)501 to (R)504 indicate **to drivers of vehicles, when displayed below a primary regulatory sign, that such regulatory sign shall only be applicable for the period, or periods, of time as indicated: provided that different time periods may be displayed for weekdays and/or Saturdays.**
- 2 The TIME LIMIT exclusive secondary message regulatory signs (R)505 to (R)506 indicate **to drivers of vehicles, when displayed below a primary regulatory sign, that the restriction indicated by such regulatory sign is applicable for a**

maximum time period as indicated in hours or minutes, and that such primary regulatory sign shall only be applicable for the period, or periods, of time as indicated: provided that different time periods may be displayed for weekdays and/or Saturdays.

- 3 The TIME LIMIT exclusive secondary message regulatory signs, DAYTIME-(R)511 and NIGHTTIME-(R)512 indicate **to drivers of vehicles, when displayed below a primary regulatory sign, that such regulatory sign shall only be applicable during daytime, or during night-time, respectively.**

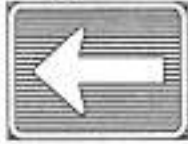
2.7.12 ACTION Sub-Group

COLOURS:
PERMANENT AND TEMPORARY
As for primary sign

For dimensions
ref. Vol. 4
page
2.7.9 to 2.7.12

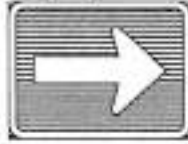
RESERVED MOVEMENT
LEFT BY CLASS

(R)520



RESERVED MOVEMENT
RIGHT BY CLASS

(R)521



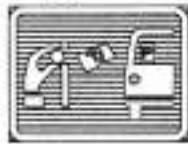
RESERVED MOVEMENT
IN BOTH DIRECTIONS BY
CLASS

(R)522



PAY AND DISPLAY

(R)523



1 The ACTION exclusive secondary message regulatory signs, RESERVED MOVEMENTS TO LEFT AND/OR RIGHT BY VEHICLE CLASS--(R)520, (R)521 and (R)522 indicate *to drivers of vehicles, when displayed below a primary regulatory sign, that the restriction indicated by such regulatory sign is applicable in the direction indicated by the arrow* and PAY AND DISPLAY-(R)52a sign

indicates *to drivers of vehicles, when displayed below a primary parking reservation sign that use of the parking area indicated by such sign is subject to payment by means of a method referred to in legislation, and to the display of the receipt thereof in a visible position on or within the vehicle.*

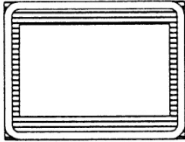
2.7.13 TEXT MESSAGE Sub-Group

For dimensions
ref. Vol. 4
pages
2.7.13 to 2.7.19

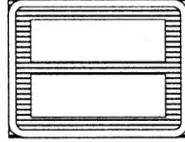
COLOURS:
PERMANENT AND TEMPORARY
As for primary sign

PUBLIC TRANSPORT
OPERATOR (LOGO)

(R)530

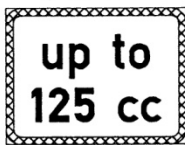
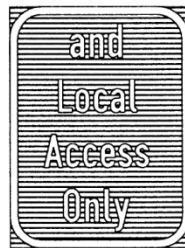
PUBLIC TRANSPORT
OPERATOR (TEXT)

(R)531



MASS LIMIT - TEXT

(R)532

MOTOR CYCLE ENGINE
SIZE LIMIT
(R)533"AND LOCAL ACCESS
ONLY" LIMIT
(R)534

DISTANCE "FOR" LIMIT

(R)535

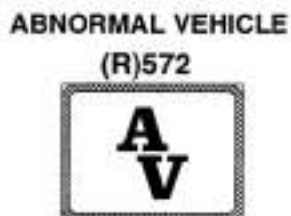
MAXIMUM NUMBER OF
VEHICLES LIMIT
(R)540

- 1 The PUBLIC TRANSPORT OPERATOR exclusive secondary message regulatory signs (R)530 and (R)531 indicate *to the drivers of vehicles, when displayed below a primary regulatory sign, that such regulatory sign shall only be applicable to drivers of vehicles operated by the company, owner or organisation indicated by means of a logo or text.*
- 2 The MASS LIMIT exclusive secondary message regulatory sign (R)532 indicates *to drivers of vehicles, when displayed below a primary regulatory sign, that such regulatory sign shall only be applicable to vehicles with a mass in excess of the mass indicated in tonnes, by means of a number on the sign. The value indicated n tonnes is the gross vehicle mass (GVM) or gross combination mass (GCM) of the vehicle.*
- 3 The MOTORCYCLE ENGINE SIZE LIMIT exclusive secondary message regulatory sign (R)533 indicates *to drivers of motorcycles, when displayed below a primary regulatory sign, that such regulatory sign shall only be applicable to motorcycle with a rated engine capacity up to the capacity indicated in cubic centimetres, by means of a number on the sign.*
- 4 The "AND LOCAL ACCESS ONLY" LIMIT exclusive secondary message regulatory sign (R)534 indicates *to drivers of vehicles, when displayed below a primary reservation sign, that the reservation indicated on such regulatory sign is relaxed to permit access by road users to local public or private property which gains access from the public road or section of public road beyond the sign.*
- 5 The DISTANCE "FOR" LIMIT exclusive secondary message regulatory sign (R)535 indicates *to drivers of vehicles, when displayed below a primary regulatory sign, that the regulation indicated on such regulatory sign is extended to apply for the distance indicated in metres or kilometres, by means of a number on the sign. In the majority of instances a regulatory sign applies to a point or from the point at which it is displayed. A limited number of signs such as NO EXCESSIVE NOISE sign R206, NO HITCH-HIKING sign R207 and NO OVERTAKING signs R214 and R215, include a distance limit on their applicability. Sign (R)535 may be used to extend this limit by the creation of a SELECTIVE RESTRICTION sign. It is recommended that the maximum distance indicated on sign (R)535 be 5 kilometres.*
- 6 The MAXIMUM NUMBER OF VEHICLES LIMIT exclusive secondary message regulatory sign,- (R)540 indicates *to drivers of vehicles, when displayed below a primary reservation sign, that the reservation Indicated on such regulatory sign is further restricted to a maximum number of vehicles as indicated by number on the sign.*

2.7.14 SYMBOL Sub-Group

COLOURS:
PERMANENT AND TEMPORARY
As for primary sign

For dimensions
ref. Vol. 4
pages
2.7.20 to 2.7.34



2.7.14 SYMBOL Sub-Group (Contd.)

For dimensions
ref. Vol. 4
pages
2.7.35 to 2.7.43

COLOURS:
PERMANENT AND TEMPORARY
As for primary sign

HIGH OCCUPANCY
VEHICLE

(R)575



EMERGENCY VEHICLE

(R)576



POLICE VEHICLE

(R)577

DISABLED PERSONS
VEHICLE

(R)578



AUTHORISED VEHICLE

(R)579

AGRICULTURAL
VEHICLE

(R)580

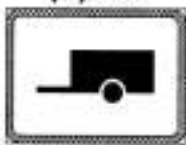
ANIMAL-DRAWN
VEHICLE

(R)581



TOWED VEHICLE

(R)582



TRAM

(R)583



- 1 The SYMBOL exclusive secondary message regulatory signs (R)560 to (R)583 indicate **to drivers of vehicles, when displayed below a primary regulatory sign, that such regulatory sign is applicable only to the class of vehicle depicted by the secondary sign in the range (R)560 to (R)583.**

2.7.15 Limit-Limit Sign Examples

- 1 LIMIT-LIMIT examples of SELECTIVE RESTRICTION regulatory signs comprise primary and secondary message signs both of which relate to limits of one type or another. The addition of the secondary limit message to the primary limit message qualifies the applicability of the primary limit.
- 2 The majority of examples of this category of selective restriction sign which are likely to be required are those related to time limits.
- 3 The examples signs (a) to (f) illustrate that:
 - (a) the SPEED LIMIT is 40 km/h for all vehicles exceeding a MASS LIMIT of 10 tonnes - a PROHIBITION regulatory sign;
 - (b) the SPEED LIMIT is 120 km/h during DAYTIME - a PROHIBITION regulatory sign;
 - (c) the SPEED LIMIT is 100 km/h during NIGHTTIME - a PROHIBITION regulatory sign;
 - (d) vehicles may use the area reserved for PARKING for a MAXIMUM DURATION of 60 minutes within the TIME LIMIT of 09:00 to 16:00 - a PARKING RESERVATION regulatory sign;

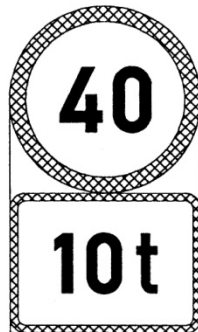
- (e) vehicles may TEMPORARILY use the area reserved for PARKING for a MAXIMUM DURATION of 10 minutes within the TIME LIMITS of 09:00 to 16:00 on weekdays and 08:30 to 13:00 on Saturdays - a TEMPORARY PARKING RESERVATION regulatory sign;
- (f) vehicles exceeding a MASS LIMIT of 5 tonnes shall not proceed beyond the sign within the TIME LIMIT of 18:00 to 08:00 - a PROHIBITION regulatory sign.

- 4 The following points should be borne in mind when using SPEED LIMIT signs:
 - (a) if a speed limit, minimum or maximum, SELECTIVE RESTRICTION sign is to be used it should be placed in close proximity to a speed limit sign appropriate to general traffic so that the required speed differential between classes of vehicle can be more clearly understood.
 - (b) the DAYTIME secondary message sign should not be used if the NIGHTTIME secondary message is not used on a preceding or adjacent sign;
 - (c) refer also to Subsection 2.4.1 with respect to Speed Limits.

(a) R201-532

**SPEED LIMIT -
MASS LIMIT
(GVM or GCM)**

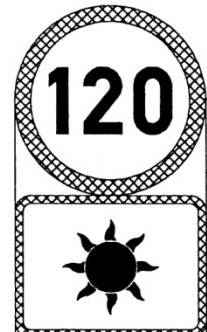
COLOURS:
As for PROHIBITION signs



(b) R201-511

**SPEED LIMIT -
DAYTIME**

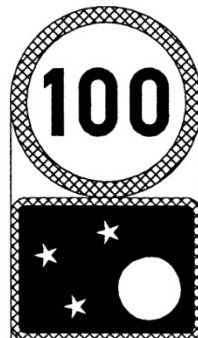
COLOURS:
As for PROHIBITION signs



(c) R201-512

**SPEED LIMIT -
NIGHTTIME**

COLOURS:
As for
PROHIBITION signs



(d) R306-P-501

**LIMITED PARKING -
TIME LIMIT**

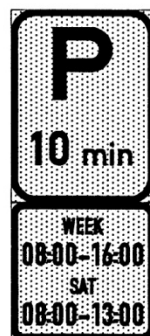
COLOURS:
As for RESERVATION signs



(e) TR306-P-503

**LIMITED PARKING -
TIME LIMIT**

COLOURS:
As for TEMPORARY
RESERVATION signs



(f) R202-501

**MASS LIMIT
(GVM or GCM) -
TIME LIMIT**

COLOURS:
As for PROHIBITION signs

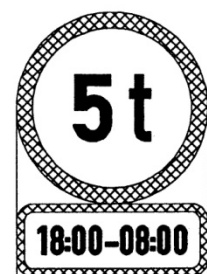


Fig 2.11

Typical Examples of Limit-Limit Signs

2.7.16 Limit-Object Sign Examples

- 1 LIMIT-OBJECT examples of SELECTIVE RESTRICTION regulatory signs comprise primary message signs relating to limits, the applicability of which are qualified by secondary message signs relating to an object being regulated.
- 2 The example signs (a) to (f) illustrate that:
 - (a) a MINIMUM SPEED of 30 km/h applies to all TRUCKS -a COMMAND regulatory sign;
 - (b) the TEMPORARY MINIMUM SPEED of 40 km/h applies to all GOODS VEHICLES – a TEMPORARY COMMAND regulatory sign;
 - (c) the area reserved for PARKING for a MAXIMUM DURATION of 120 minutes is reserved for DELIVERY VEHICLE - a PARKING RESERVATION regulatory sign;
 - (d) the SPEED LIMIT of 60 km/h applies to all GOODS VEHICLES EXCEEDING the MASS INDICATED ONLY regulatory sign -a PROHIBITION sign;
 - (e) for a TEMPORARY period a mandatory requirement exists for VEHICLES EXCEEDING the MASS INDICATED ONLY to proceed beyond the sign which applies ONLY to CONSTRUCTION

VEHICLES - a TEMPORARY COMMAND regulatory sign;

- (f) the TEMPORARY SPEED LIMIT of 50km/h applies to CONSTRUCTION VEHICLES - a TEMPORARY PROHIBITION regulatory sign.
- 3 When these signs are being considered for use the following points should be borne in mind:
 - (a) if a speed limit, minimum or maximum, SELECTIVE RESTRICTION sign is to be used it should be placed in close proximity to a speed limit sign appropriate to general traffic so that the required speed differential between classes of vehicle can be more clearly understood.
 - (b) refer also to Subsection 2.4.1 with respect to Speed Limits;
 - (c) mass prohibition signs should only be used when an alternative route exists for vehicles exceeding the mass indicated on the sign.
- 4 A PROHIBITION regulatory sign incorporating a red slash and an object symbol shall NOT be used as a secondary message sign with SELECTIVE RESTRICTION signs.

(a) R101-568

**MINIMUM SPEED -
GOODS VEHICLES**

COLOURS:
As for COMMAND signs



(b) TR101 - 568

**MINIMUM SPEED -
GOODS VEHICLES
(TEMPORARY)**

COLOURS:
As for TEMPORARY
COMMAND signs



(c) R306-P-567

**LIMITED PARKING -
DELIVERY VEHICLE**

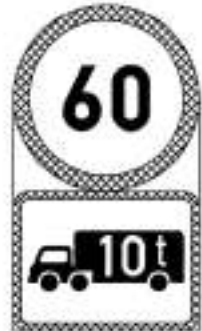
COLOURS:
As for RESERVATION signs



(d) R201-569

**SPEED LIMIT -
GOODS VEHICLE
OVER INDICATED
GVM**

COLOURS:
As for PROHIBITION signs



(e) TR102 - 570

**VEHICLES
EXCEEDING
MASS ONLY -
CONSTRUCTION
VEHICLES
(TEMPORARY)**

COLOURS:
As for TEMPORARY
COMMAND signs



(f) TR201-570

**SPEED LIMIT -
CONSTRUCTION
VEHICLES
(TEMPORARY)**

COLOURS:
As for TEMPORARY
PROHIBITION signs



Fig 2.12

Typical Examples of Limit-Object Signs

2.7.17 Action Limit Sign Examples

- 1 ACTION-LIMIT examples of SELECTIVE RESTRICTION regulatory signs comprise primary message signs relating to actions, which include movements or turns, the applicability of which are qualified by secondary message signs relating to a limit.
- 2 The example signs (a) to (e) illustrate that:
 - (a) a TEMPORARY prohibition on TURNING RIGHT exists for the junction or entrance AHEAD within the TWO TIME LIMITS of 07:00 to 09:00 and 16:00 to 18:00 - a TEMPORARY PROHIBITION regulatory sign;
 - (b) a prohibition that NO UNAUTHORISED VEHICLES shall pass beyond the sign within the TWO TIME LIMITS of 06:30 to 09:00 and 16:00 to 20:00 - a PROHIBITION regulatory sign.
 - (c) a prohibition on STOPPING exists within the TWO

- TIME LIMITS of 07:00 to 09:00 and 16:00 to 18:00 - a PROHIBITION regulatory sign;
- (d) vehicles may use the area reserved for PARKING within the TIME LIMIT of 08:00 to 18:00- a PARKING RESERVATION regulatory sign;
- (e) a mandatory requirement to PROCEED STRAIGHT ON ONLY applies to all vehicles only during DAYTIME - a COMMAND regulatory sign;
- (f) the mandatory requirement to KEEP LEFT applies to all vehicles exceeding a MASS LIMIT of 16 tonnes only - a COMMAND regulatory sign.
- 3 A PROHIBITION regulatory sign incorporating a red slash and an object symbol shall not be used as a secondary message sign with SELECTIVE RESTRICTION signs.

(a) TR210-502

**NO RIGHT TURN
AHEAD - TIME LIMIT
(TEMPORARY)**

*COLOURS:
As for TEMPORARY
PROHIBITION signs*



(b) R208-502

**NO UNAUTHORISED
VEHICLES -
TIME LIMIT**

*COLOURS:
As for PROHIBITION signs*



(c) R217-502

**NO STOPPING -
TIME LIMIT**

*COLOURS:
As for PROHIBITION signs*



(d) R305-P-501

**PARKING -
TIME LIMIT**

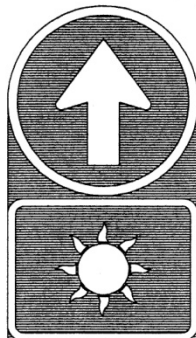
*COLOURS:
As for RESERVATION signs*



(e) R107-511

**PROCEED STRAIGHT
ON ONLY - DAYTIME**

*COLOURS:
As for COMMAND signs*



(f) R103-532

**KEEP LEFT -
MASS LIMIT**

*COLOURS:
As for COMMAND signs*

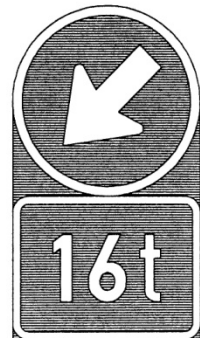


Fig 2.13

Typical Examples of Action-Limit Signs

2.7.18 Action-Object Sign Examples

- 1 ACTION-OBJECT examples of SELECTIVE RESTRICTION regulatory signs comprise primary message signs relating to actions, which include movements or turns, the applicability of which are qualified by a secondary message sign relating to an object being regulated.
- 2 The example signs (a) to (e) illustrate that:
 - (a) a prohibition on TURNING RIGHT exists for BUSES at the junction or entrance AHEAD • a PROHIBITION regulatory sign.
 - (b) the mandatory requirement to TURN LEFT at the junction applies to DELIVERY VEHICLES only - a COMMAND sign;
 - (c) a prohibition exists on the OVERTAKING of GOODS VEHICLES by VEHICLES CONVEYING DANGEROUS GOODS

- DANGEROUS GOODS - a PROHIBITION regulatory sign;
 - (d) the mandatory requirement to KEEP LEFT applies to MOTORCYCLES only a COMMAND regulatory sign;
 - (e) a mandatory requirement to SWITCH HEADLAMPS ON and applies to EMERGENCY VEHICLES only;
 - (f) a TEMPORARY prohibition exists on the OVERTAKING of VEHICLES by BUSES - a PROHIBITION regulatory sign.
- 3 A PROHIBITION regulatory sign incorporating a red slash and an object symbol shall not be used as a secondary message sign with SELECTIVE RESTRICTION signs.

(a) **R210-560**
NO RIGHT TURN AHEAD - BUSES
COLOURS:
As for PROHIBITION signs



(b) **R108-567**
TURN LEFT AHEAD - DELIVERY VEHICLES
COLOURS:
As for COMMAND signs



(c) **R215-571**
NO OVERTAKING - GOODS VEHICLES BY VEHICLES CONVEYING DANGEROUS GOODS
COLOURS:
As for PROHIBITION signs



(d) **R103-562**
KEEP LEFT - MOTOR CYCLES
COLOURS:
As for COMMAND signs



(e) **R133-576**
SWITCH HEADLAMPS ON - EMERGENCY VEHICLES
COLOURS:
As for COMMAND signs



(f) **TR214-560**
NO OVERTAKING - BUSES (TEMPORARY)
COLOURS:
As for TEMPORARY COMMAND signs



Fig 2.14

Typical Examples of Action-Object Signs

2.7.19 Object-Limit Sign Examples

- 1 OBJECT-LIMIT examples of SELECTIVE RESTRICTION regulatory signs comprise primary message signs relating to objects, the applicability of which, are qualified by secondary message signs relating to a limit. Applications of this type of SELECTIVE RESTRICTION SIGN should be restricted to secondary message TIME LIMITS. Speed or mass limits should not be used (see paragraph 2.7.15.3).
- 2 The example signs (a) to (f) illustrate that:
 - (a) a TEMPORARY reservation exists on the use of a roadway or area by CONSTRUCTION VEHICLES within the TIME LIMIT 06:30 to 20:00 - a TEMPORARY RESERVATION regulatory sign;
 - (b) the BUS LANE RESERVATION exists within the TIME LIMIT 16:00 to 19:30 - a RESERVATION regulatory sign;
 - (c) TOUR BUSES and ONLY TOUR BUSES shall use the roadway indicated within the TWO TIME LIMITS 06:00 to 09:00 and 16:00 to 20:00 - a COM- MAND regulatory sign;
 - (d) a TEMPORARY prohibition on PEDESTRIANS exists within the TIME LIMIT 18:00 to 16:00 - a TEMPORARY PROHIBITION regulatory sign;
 - (e) a prohibition on TAXIS exists within the TIME LIMIT 06:00 to 09:00 - a PROHIBITION regulatory sign;
 - (f) a prohibition on ABNORMAL VEHICLES not to proceed past the sign during NIGHTTIME - a PROHIBITION regulatory sign.
- 3 A PROHIBITION regulatory sign incorporating a red slash and an object symbol shall not be used as a secondary message sign with SELECTIVE RESTRICTION signs.

<p>(a) TR315-501</p> <p>CONSTRUCTION VEHICLES RESERVATION - TIME LIMIT (TEMPORARY)</p> <p>COLOURS: As for RESERVATION signs</p>		<p>(b) R302-501</p> <p>BUS LANE RESERVATION - TIME LIMIT</p> <p>COLOURS: As for RESERVATION signs</p>	
<p>(c) R129-502</p> <p>TOUR BUSES ONLY/ MUST-TIME LIMIT</p> <p>COLOURS: As for COMMAND signs</p>		<p>(d) TR218-501</p> <p>NO PEDESTRIANS - TIME LIMIT (TEMPORARY)</p> <p>COLOURS: As for TEMPORARY PROHIBITION signs</p>	
<p>(e) R224-501</p> <p>NO TAXIS - TIME LIMIT</p> <p>COLOURS: As for PROHIBITION signs</p>		<p>(f) R233-512</p> <p>NO ABNORMAL VEHICLES - NIGHTTIME</p> <p>COLOURS: Border: Red retroreflective Symbol: White retroreflective Background: Black semi-matt</p>	

Fig 2.15 Typical Examples of Object-Limit Signs

2.7.2 Limit-Action Sign Examples and Object-Action Sign Examples

- 1 The preceding Subsections 2.7.14 to 2.7.19 show examples of possible SELECTIVE RESTRICTION signs, with various groupings of Limit-Limit, Limit-Object, Action-Limit, Action-Object and Action-Object Primary Message signs with Secondary Message signs. Although it is not envisaged that any other significant groupings will be required, examples of a Limit-Action sign and an Object-Action sign are shown to illustrate the possibilities. It is, however recommended that these groupings only be used after all other avenues of sign groupings have been thoroughly investigated.
- 2 Detail 2.16.1 illustrates an example of a Limit-Action sign namely a REGULATORY sign indicating that PARKING is reserved for use for a TIME LIMIT and upon payment of a tariff, and that such payment be made in a prescribed PAY AND DISPLAY manner.
- 3 Detail 2.16.2 illustrates an example of an Object-Action sign, namely that a RESERVATION exists for an exclusive roadway for use by BUSES ONLY, and that this restriction is RESERVED for MOVEMENTS TO LEFT AND RIGHT.

R306-P-523

COLOURS:
As for RESERVATION signs



**LIMITED PARKING RESERVATION
- PAY AND DISPLAY**

Detail 2.16.1

R301-522

COLOURS:
As for RESERVATION signs



**BUS RESERVATION - RESERVED
MOVEMENT LEFT AND RIGHT**

Detail 2.16.2

Fig 2.16 Typical Examples of Limit-Action and Object-Action Signs

2.8 REGULATORY SIGN COMBINATIONS

2.8.1 General

- 1 Regulatory signs may be combined with other signs or with other devices to improve the comprehension of the message to be passed to drivers, or to improve the overall conspicuity of the regulatory sign. Such applications may be used with both PERMANENT and TEMPORARY regulatory signs.
- 2 Regulatory signs may be supplemented in one or more of the following ways:
 - (a) by combination with a supplementary plate which conveys additional information ONLY, about the regulation being signed, and DOES NOT affect the applicability of the regulatory sign with which it is combined;
 - (b) by display on a HIGH VISIBILITY background;
 - (c) by incorporation into guidance signs, both ground mounted and overhead.
- 3 Circumstances may occur where there is a need to indicate different regulatory requirements at different times of day or under different traffic operating conditions such as bad weather or extreme congestion. Different regulatory messages, often combined with warning messages, may be provided by using VARI- ABLE MESSAGE signs.
- 4 Regulatory signs may occasionally be combined with a flashing light signal to improve the ability of the sign to attract attention, although this use is not likely to be as common as the use of a flashing light with a warning sign. Examples of such situations are:
 - (a) at a temporary road closure particularly where a detour occurs some distance before the point of closure;
 - (b) at rail crossings controlled by a STOP sign R1 in combination with a RAILWAY CROSSING(S) hazard marker W403 or W404 (see Subsection 3.6.4);
 - (c) in addition to a HIGH VISIBILITY background (see Subsection 2.8.3).
- 2 Various Subsections in this Chapter have commented on the use of supplementary plates with regulatory signs and details of their use is covered in Subsection 2.1.8.
- 3 Supplementary plates may be used with PERMANENT or TEMPORARY regulatory signs in one of the following categories:
 - (a) descriptive information by words or symbol;
 - (b) distance for which the regulation occurs;
 - (c) distance to the point where the regulation commences or occurs.
- 4 Many applications will require a text legend. This should be sized in accordance with the operating speed of the road. Text messages should be carefully worded using standardised messages, wherever possible, and should be as brief as possible. In exceptional cases the amount of text may require a large area to be accommodated. In such special cases the regulatory sign may be incorporated into the text sign. The use of HIGH VISIBILITY backgrounds should be considered in such cases (see Volume 2, Chapter 13).
- 5 Supplementary plates should be mounted immediately below the regulatory sign in such a way that the gap between the sign and the plate is in the range 0 mm to 150 mm.
- 6 Supplementary plates may also be used with regulatory signs mounted on HIGH VISIBILITY backgrounds. Examples are given in Subsection 2.8.3.
- 7 **It should be noted that 3-WAY STOP and 4-WAY STOP signs R1.3 and R1.4 are NOT examples of the use of supplementary plates but are derivatives of STOP sign R1.**
- 8 It should also be noted that messages such as "Police" (by symbol), "Customs", or "Toll" ARE supplementary plates as they inform drivers of the reason for the STOP control. This does not affect the applicability or required actions of drivers in response to the general rules of the road which require drivers to act on the instructions of a police officer, traffic officer, customs officer or toll official.

2.8.2 Supplementary Plates

- 1 Details relating to type, size, colour and material of SUPPLEMENTARY PLATE signs IN11 are given in Chapter 5, Subsection 52.6 and details of dimensions are given in Volume 4, Chapter 9.

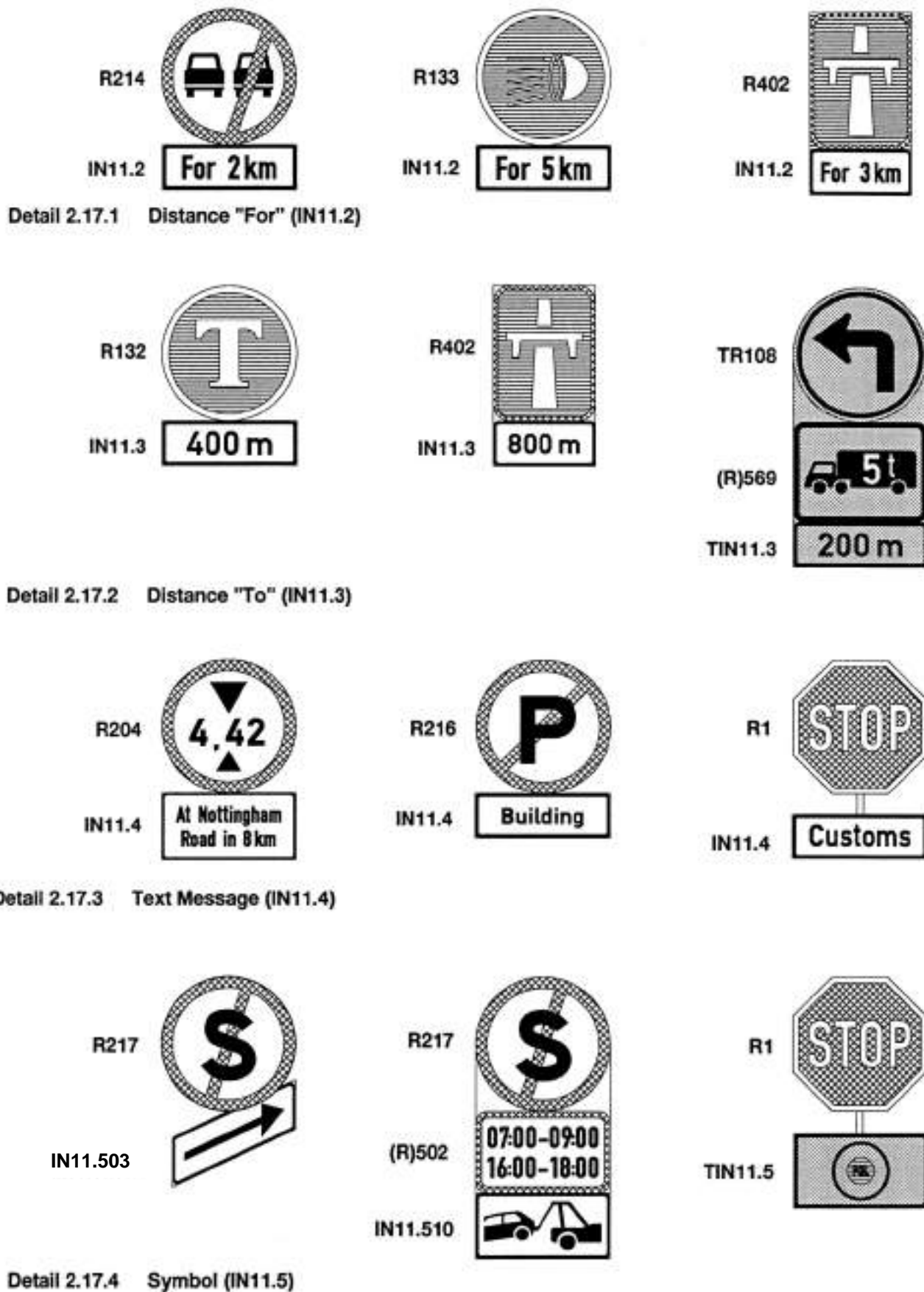


Fig 2.17 Typical Examples of Regulatory Sign Combinations Using Text or Symbol Message Supplementary Plates

2.8.3 High Visibility Background

- 1 Any PERMANENT or TEMPORARY regulatory signs may be incorporated into a HIGH VISIBILITY background to improve the target value of the sign. The use of a high visibility background is particularly appropriate in advance of a physical condition of the roadway which necessitates the implementation of a mandatory regulation not generally applicable to the rest of the roadway e.g. at steep descents, low bridges or at roadworks sites.
- 2 A high visibility background may also be used in very "confused" or "cluttered" environments such as city streets, or where there is a particularly low observance of an existing regulatory sign, in order to improve conspicuity.
- 3 The background of a PERMANENT HIGH VISIBILITY regulatory sign shall be manufactured in white retroreflective material with a red retroreflective border.
- 4 The background of a TEMPORARY HIGH VISIBILITY regulatory sign shall be manufactured in yellow retroreflective material with a black semi-matt border.
- 5 When ordering a HIGH VISIBILITY background the normal regulatory sign number(s) should be quoted, followed by the letters RA-A, RA-B, RB-A, RB-B, RC or RD (see Volume 4, Figures 2.12 to 2.17).
- 6 When a PERMANENT COMMAND sign is used on a PERMANENT HIGH VISIBILITY background a thin black semi-matt border should be placed outside the normal white border of the COMMAND sign. When a TEMPORARY COMMAND sign is used on a TEMPORARY HIGH VISIBILITY background the COMMAND sign black semi-matt border should be increased in width. Dimensional details of these borders are given in Volume 4, Section 2.1.
- 7 If a high visibility sign is used with a regulatory sign in advance of the point or area of prohibition a distance plate shall be added below the sign. Since the distance relates to the applicability of the sign it should be manufactured in the same colours as the main high visibility background above it.
- 8 High visibility applications of regulatory signs may be mounted on the same support as a similar high visibility warning sign. The two signs must, however, be mounted separately with a 150 mm - 200 mm gap between signs. This technique can be effective in bringing to the attention of drivers the reason why a particular regulation has been imposed particularly under circumstances of temporary roadworks.

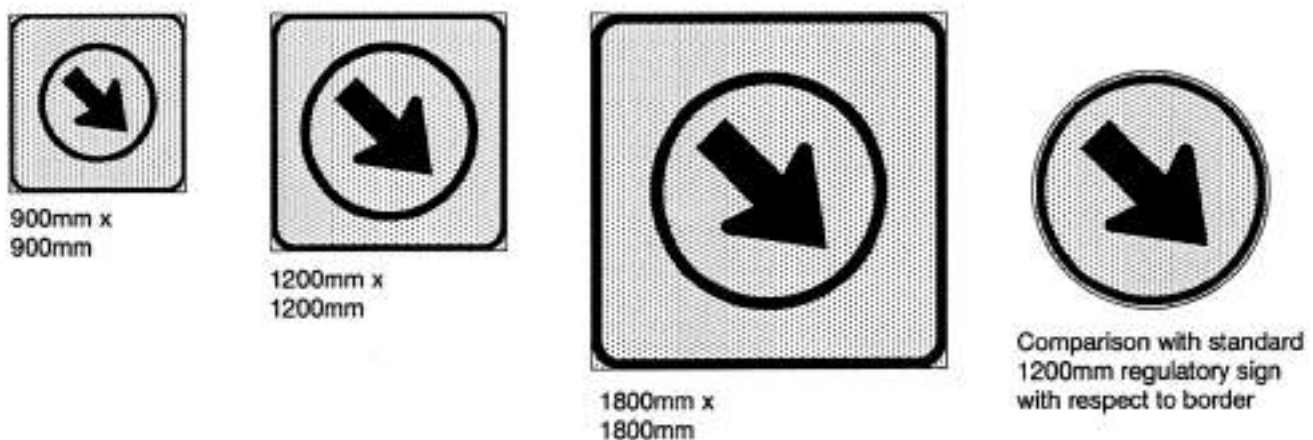
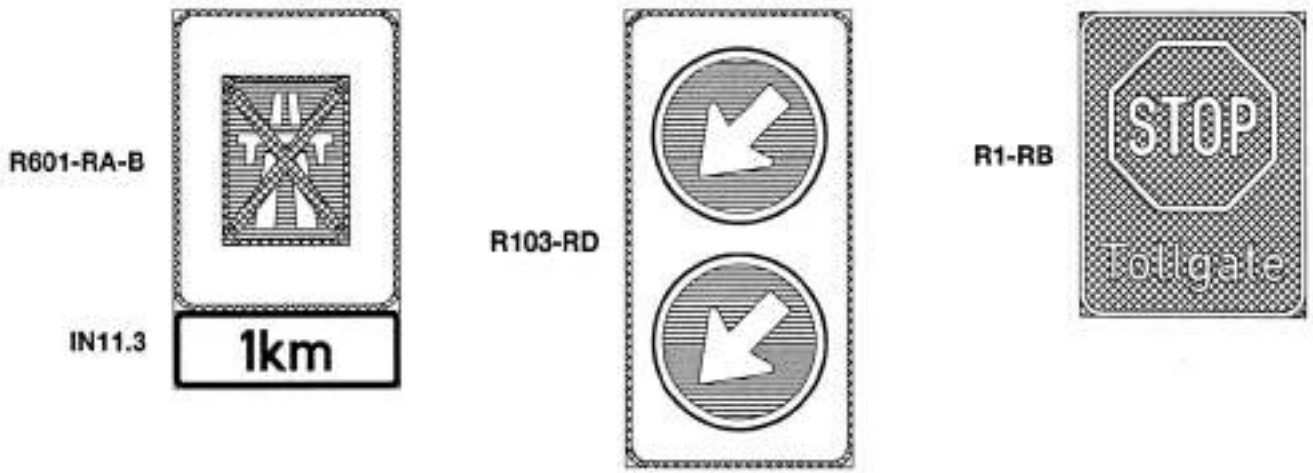
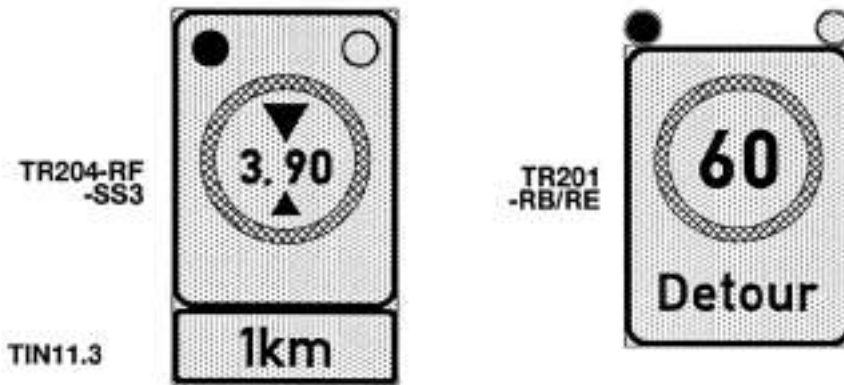


Fig 2.18

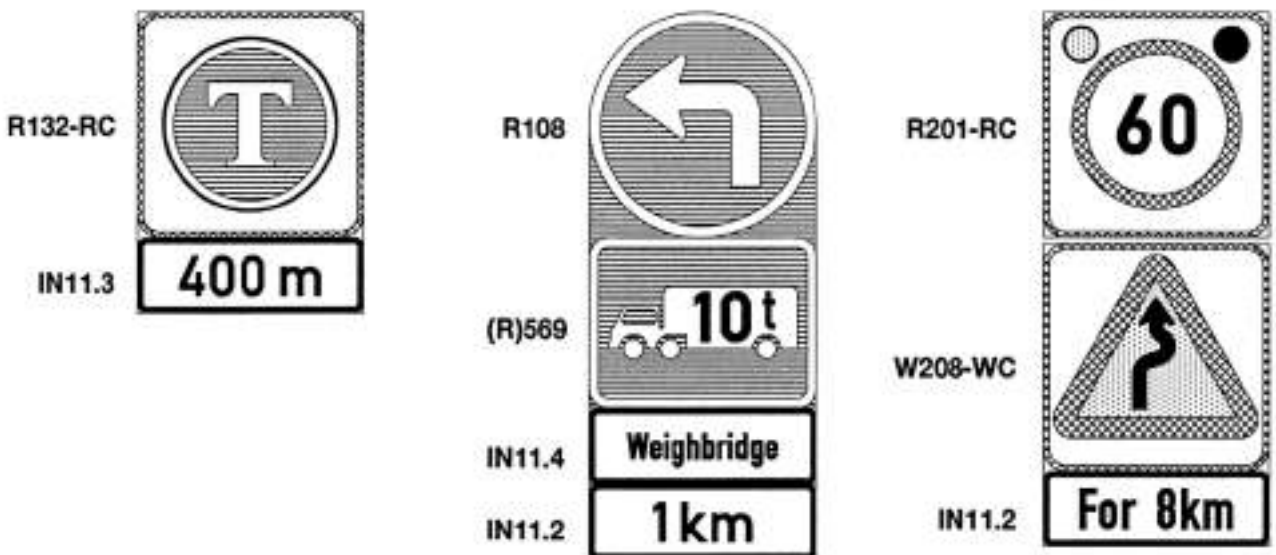
High Visibility Regulatory Signs Sizes



Detail 2.19.1 Regulatory Sign on High Visibility Backgrounds



Detail 2.19.2 With Flashing Yellow Signals



Detail 2.19.3 Multiple Combinations including Information signs

Fig 2.19 Typical Examples of Regulatory Sign Combinations

2.8.4 Regulatory Signs in Guidance Signs

- 1 It may occasionally be beneficial to incorporate a regulatory sign into a direction sign. This technique is most relevant to higher speed roadways and to junctions with complex road geometry. The technique may also be effectively used to indicate prohibitions, for instance, on goods vehicle movements in urban areas.
- 2 If it is required to indicate a regulatory message in this manner the standard regulatory sign, either PERMANENT or TEMPORARY, as appropriate, should be incorporated into a map-type or overhead direction sign. The use of regulatory signs with stack type direction signs is not generally recommended although such use is appropriate to the signing of interchanges or junctions with TOLL routes to indicate the mandatory requirement to PAY TOLL if entering the TOLL route at a junction so signed.
- 3 When used with ground mounted map-type direction signs the regulatory message may apply to either the exit direction or the straight on direction for high speed exits, or at at-grade junctions to left, right or straight-on directions. The need to increase the size of the direction sign to incorporate the regulatory sign will depend on the amount of destination information given on the sign. If there are several destination names the regulatory sign will probably fit without a significant increase in sign size. The regulatory sign should preferably be located over the appropriate leg of the arrow on the sign.
- 4 Regulatory sign messages incorporated into overhead direction signs will normally relate to one or more of the lanes of a multi-lane roadway. Overhead signs may use upward or downward pointing arrows. When upward pointing arrows are used the regulatory sign(s) should be incorporated into the appropriate arrow shaft(s) in a similar manner to ground-mounted signs. When used with downward pointing arrows a regulatory sign should be located over the appropriate arrow.
- 5 Regulatory sign messages may also be incorporated into diagrammatic guidance signs. These are covered in detail in Chapter 4.
- 6 If a PROHIBITION sign is to be placed on a green or blue retroreflective background it may be necessary that a narrow white retroreflective border be placed outside the prohibition sign red border (see Volume 4).
- 7 The background of a PERMANENT diagrammatic sign shall be manufactured in white retroreflective material with a red retroreflective border. If it is required to place a PERMANENT COMMAND sign on such a background a thin black semi-matt border should be placed outside the normal white border of the COMMAND sign (see Volume 4).
- 8 The background colour of a TEMPORARY diagrammatic sign shall be manufactured in yellow retroreflective material with a black semi-matt border. If it is required to place a TEMPORARY COMMAND sign on such a background the COMMAND sign black semi-matt border should be increased in width (see Volume 4).
- 9 When a regulatory sign is incorporated into a direction sign which includes a distance **it is an advance indication of a regulation to be applied. The**

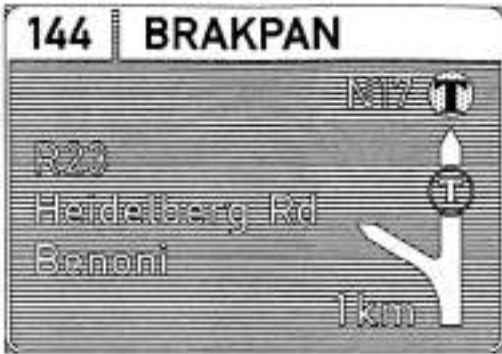
regulatory message does not come into force at such a sign. The regulatory message shall be repeated using conventional ground mounted, or high visibility regulatory signs at the point from which the regulation applies.

2.8.5 Variable Message Regulatory Signs

- 1 Development of the use of VARIABLE MESSAGE signs is in its early stages in South Africa. Such signs are expensive and can only be justified in extreme situations.
- 2 VARIABLE MESSAGE regulatory signs are particularly appropriate to traffic control on roadways carrying very high traffic volumes and on sections of roadway subject to widely varying and dramatic changes in weather. Such conditions may require regular variation in speed limit or other regulatory messages. Such signs may become justified if a history of collisions exists in spite of other engineering measures taken to reduce these.
- 3 Three types of variable message sign manufacture could be appropriate to regulatory sign messages:
 - (a) rotating vane signs;
 - (b) roller blind signs;
 - (c) light emitting signs (either illuminated bulbs or fibre optics).

The first two types are limited in the choice of message and would normally require external or internal illumination. The light emitting sign type is capable of reproducing a wider range of sign face layouts but cannot maintain full sign colour code without considerable expense both in capital outlay and running costs.

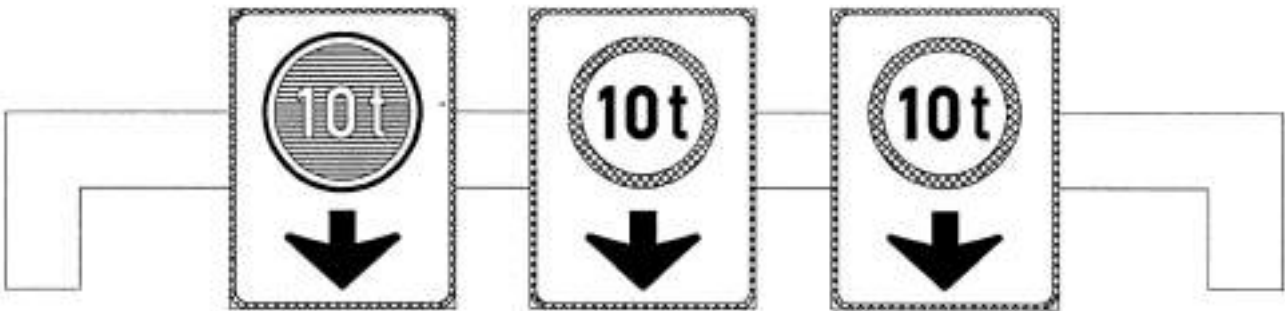
- 4 If only two or three alternative messages are required the use of rotating vane or roller blind types should be considered. If this type is used the signs shall conform fully to the PERMANENT and TEMPORARY regulatory sign colour codes.
- 5 If a light emitting sign system is specified due to the large range of messages required the design of the system should be related to the likelihood that enforcement will be carried out. If enforcement is not going to be carried out the required message should be given in the simplest possible form which will normally comprise a white legend on a matt-black or semi-matt black background. This will in effect be an information or advisory message.
- 6 If it is intended to enforce light emitting variable message signs then it is recommended that variable message regulatory signs be manufactured so that the white and red borders of COMMAND and PROHIBITION signs, including a slash if required by the prohibition sign, and the white symbols, be illuminated and that the background be non-illuminated and semi-matt black in colour. This colour code should apply to both PERMANENT and TEMPORARY uses of VARIABLE MESSAGE regulatory signs. Any CONTROL regulatory sign message shall be provided ONLY in the standard RED and WHITE colour code.
- 7 Refer to Chapter 9 and Volume 2 for more detail on variable message signing.



Mandatory payment of TOLL ahead - last free exit to ALTERNATIVE ROUTE.

No goods vehicles permitted to exit in 1 km.

Detail 2.20.1 Regulatory Signs in Direction Signs

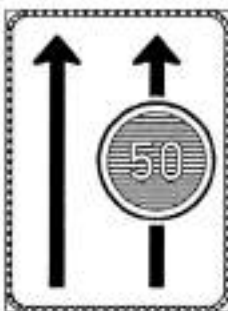


ONLY Vehicles exceeding 10 tonnes GVM or GCM MUST use lane indicated.

Vehicles exceeding 10 tonnes GVM or GCM prohibited.

NOTE: Sign types illustrated at different scales.

Detail 2.20.2 Overhead Lane Use Signs Incorporating Regulatory Signs



Minimum Speed in lane.



No goods vehicles in right hand lane.



No Construction Vehicles in "fast" lane.

Detail 2.20.3 Regulatory Signs in Diagrammatic Signs

Fig 2.20 Typical Examples of Regulatory Signs in Guidance Signs

DE-RESTRICTION

COLOURS:
PERMANENT&TEMPORARY
Red

(R)600

For dimensions
ref. Vol. 4
pages



2.6.1
2.6.2
2.6.3

2.9.1 De-Restriction

- 1 De-restriction signs (R)600 indicate the termination of the applicability of one or more regulations. As such they have the legal significance that drivers are **no longer required to comply with the regulation or regulations covered by the regulatory sign displayed beneath the red cancellation cross.**
- 2 De-restriction signs (R)600 should normally only be used to de-restrict the application of signs in the CONDITIONAL COMPREHENSIVE group.
- 3 In isolated instances it may be of advantage to indicate the termination of some other regulation in the MANDATORY COMMAND or MANDATORY PROHIBITION groups. However, it is recommended that the change in regulation be given by the placing of a sign indicating the new level of regulation (see Sub-section 2.1.1 paragraph 2.1.1.7). **De-restriction signs (R)600 shall not be used to indicate a change in SPEED LIMIT or MINIMUM SPEED. A change in speed limit shall be indicated by display of the relevant R101 or R201 sign.**
- 4 Examples of MANDATORY COMMAND or PROHIBITION signs that may be used as a DE-RESTRICTION sign by the superimposing of a red cross as shown in

Fig.2.21. When ordering signs the addition of the suffix -600 is required.

- 5 Signs R401-600 and R402-600 should be used to indicate the end of comprehensive regulations applicable to DUAL CARRIAGEWAY FREEWAYS and SINGLE CARRIAGEWAY FREEWAYS respectively. The signs should be located on the left and right hand sides of dual carriageway roadways and the left side of single carriageway roadways.
- 6 A de-restriction sign should be the same size as the corresponding regulatory sign.
- 7 A TEMPORARY version of sign R401-600, numbered TR401-600, may be used to indicate the temporary end of a DUAL CARRIAGEWAY FREEWAY when, during roadworks, traffic is diverted onto one carriageway to operate under two-way traffic flow. Sign TR401-600 may be mounted in conjunction with a TEMPORARY SINGLE CARRIAGEWAY FREEWAY sign TR402, or separately some 60 m to 100 m in advance of sign TR402, in such circumstances. TEMPORARY sign TR402-600 may be displayed when returning to normal dual carriageway freeway cross-section. In this situation a PERMANENT sign R401 may be displayed.



R401-600/TR401-600
End of Dual
Carriageway
Freeway



R402-600/TR402-600
End of Single
Carriageway
Freeway



R403-600
End of
Woonerf



R132-600 End of Pay Toll



R133-600 End of Headlamps On



R202-600 End Mass Limit

Fig 2.21

Examples of De-Restriction Signs

2.10.1 General

- 1 Whilst the objective of the SADC Road Traffic Sign System is to achieve the highest possible degree of harmonization of the system throughout the region it is likely that there will be a number of details which will remain unique to individual member countries.
- 2 For the purposes of identification any such signs are considered as NATIONAL VARIANTS specific to one or more of the SADC member countries. Variants can occur in one of three ways, namely:
 - (a) as an ADDITIONAL variant using a modified or different symbol for a sign function used in most member countries; or
 - (b) as a UNIQUE variant where the sign is used in only one country; or
 - (c) as an ADDITIONAL variant to accommodate the language of a SADC member country which does not have English as an official language (at the time of publishing such variations apply to the use of Portuguese in Angola and Mozambique, although every effort has been made to minimise this need by the use of symbolic messages).
- 3 All signs are listed once in colour in the Contents section and are provided with text describing their meaning and function. Any additional National variants involving a modified or different symbol will be identified pictorially in the National Variants section. Any National Variants which are unique to one country will appear in the National Variants section complete with appropriate text.
- 4 All signs are allocated numbers. An additional variant is allocated a three letter suffix identifying the country to which it belongs. In the case of a unique variant such a sign will be allocated a unique number which includes the appropriate National three letter suffix. If the sign becomes more widely used the use of the suffix will be discontinued. The letter codes allocated for each member country are as follows:

(a) Angola	Ang;
(b) Botswana	Bot;
(c) Democratic Republic of Congo	DRC;
(d) Lesotho	Les;
(e) Malawi	Mal;
(f) Mauritius	Mau;
(g) Mozambique	Moz;
(h) Namibia	Nam;
(i) Seychelles	Sey;
(j) South Africa	RSA;
(k) Swaziland	Swa;
(l) Tanzania	Tan;
(m) Zambia	Zam;
(n) Zimbabwe	Zim.

- 5 When National Variants occur they will be covered in Subsections of this Section, bearing the name of the country. All National Variants are identified in the Contents by a black dot thus - ● The purpose in identifying variants is to assist education on road traffic signs within the region for travellers beyond National borders, and to assist sign manufacturers.

2.10.2 Angola

- 1 Angola requires a number of National Variants to cater for the fact that traffic travels on the right side of the

roadway. The following signs are additional national Variants to cater for this situation:

- (a) YIELD AT TRAFFIC CIRCLE control regulatory sign R2.2-Ang;
- (b) YIELD TO ONCOMING TRAFFIC control regulatory sign R6-Ang;
- (c) TAXI command regulatory sign R118-Ang;
- (d) ROUNDABOUT command regulatory sign R137-Ang;
- (e) NO U-TURN prohibition regulatory sign R213-Ang;
- (f) NO OVERTAKING prohibition regulatory signs R214-Ang and R215-Ang;
- (g) NO TAXI prohibition regulatory sign R224-Ang;
- (h) TAXI RESERVATION reservation regulatory signs R309-Ang and R309-P-Ang;
- (i) MINIBUS STOP "stop" reservation regulatory sign R326-Ang;
- (j) TAXI exclusive secondary message regulatory sign (R)564-Ang.

The signs are illustrated in Figure 2.22 with references to the relevant subsections where the applicable text can be found.

- 2 The signs listed in paragraph 2.10.2.1 may be manufactured by mirroring the standard symbols specified for the signs for use in other SADC member countries.

2.10.3 Angola and Mozambique

- 1 Since English is not an official language in Angola and Mozambique National Variants are required of a number of signs to cover the use of Portuguese equivalents to a limited number of English words or abbreviations used on the following signs:
 - (a) SIGA ("GO") temporary control regulatory signs R1.5B-Ang and R1.5B-Moz;
 - (b) VEICULO ANORMAL (abnormal vehicle) command regulatory signs R127-Ang and R127-Moz;
 - (c) AUTOCARROS DE TURISMO (tour bus) command regulatory signs R129-Ang and R129-Moz;
 - (d) NAO A VEICULOS DE DIMENSOES ANORMAIS (no abnormal vehicles) prohibition regulatory signs R233-Ang and R233-Moz;
 - (e) NAO A AUTOCARROS DE TURISMO (no tour buses) prohibition regulatory signs R235-Ang and R235-Moz;
 - (f) VEICULO ANORMAL (abnormal vehicle) reservation regulatory signs R317-Ang, R317-P-Ang, R317-Moz, and R317-P-Moz;
 - (g) AUTOCARROS DE TURISMO (tour bus) reservation regulatory signs R319-Ang, R319-P-Ang, R319-Moz, and R319-P-Moz;
 - (h) SEMANA (for "week" abbreviation of "weekdays") and SABADO (for "Saturday") exclusive secondary message signs (R)503-Ang, (R)503-Moz, (R)504-Ang, (R)504-Moz, (R)505-Ang, and (R)505-Moz;
 - (i) ATE (for "up to") exclusive secondary message signs (R) 533-Ang and (R)533-Moz;
 - (j) E SO ACESSO LOCAL (for "and Local Access Only") exclusive secondary message signs (R)534-Ang and (R)534-Moz;

**R2.2-Ang**

Refer: page 2.2.8

**R6-Ang**

Refer: page 2.2.12

**R118-Ang**

Refer: page 2.3.10

**R137-Ang**

Refer: page 2.3.17

**R213-Ang**

Refer: page 2.4.9

**R214-Ang**

Refer: page 2.4.10

**R215-Ang**

Refer: page 2.4.10

**R224-Ang**

Refer: page 2.4.17

**R309-Ang/R309-P-Ang**

Refer: page 2.5.9

**R326-Ang**

Refer: page 2.5.14

**(R)564-Ang**

Refer: page 2.7.8

Fig 2.22

Regulatory Sign National Variants - ANGOLA (Mirror image signs or symbols)

(k) DURANTE (for "For km") exclusive secondary message signs (R) 535-Ang and (R)535-Moz;

(l) VEICULO ANORMAL (abnormal vehicle) exclusive secondary message signs (R)572-Ang and (R)572-Moz;

(m) AUTOCARROS DE TURISMO (tour bus) exclusive secondary message signs (R)574-Ang and (R)574-Moz.

The signs are illustrated in Figure 2.23 with references to the relevant subsections where the applicable text can be found.



R1.5B-Ang
R1.5B-Moz
Refer: page 2.2.3



R127-Ang
R127-Moz
Refer: page 2.3.12



R129-Ang
R129-Moz
Refer: page 2.3.13



R233-Ang
R233-Moz
Refer: page 2.4.19



R235-Ang
R235-Moz
Refer: page 2.4.20



R317-Ang/R317-P-Ang
R317-Moz/R317-P-Moz
Refer: page 2.5.11



R319-Ang/R319-P-Ang
R319-Moz/R319-P-Moz
Refer: page 2.5.12



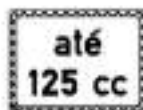
(R)503-Ang
(R)503-Moz
Refer: page 2.7.4



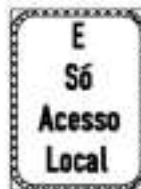
(R)504-Ang
(R)504-Moz
Refer: page 2.7.4



(R)505-Ang
(R)505-Moz
Refer: page 2.7.4



(R)533-Ang
(R)533-Moz
Refer: page 2.7.6



(R)534-Ang
(R)534-Moz
Refer: page 2.7.6



(R)535-Ang
(R)535-Moz
Refer: page 2.7.6



(R)572-Ang
(R)572-Moz
Refer: page 2.7.10



(R)574-Ang
(R)574-Moz
Refer: page 2.7.11

Fig 2.23

Regulatory Sign Language Variants –
Portuguese - ANGOLA and MOZAMBIQUE

2.10.5 "Police" Signs - All Countries

1 Sections 2.5 and 2.7 show signs R322/R322-P and (R)577 respectively with a generic "Police Vehicle" reservation symbol. This sign may be used in any country in the SADC region. Alternatively individual

countries may utilize a specific symbol representative of the police in their country. Examples of such signs are included in Figure 2.24 for the benefit of education on road traffic signs and to assist manufacturers.

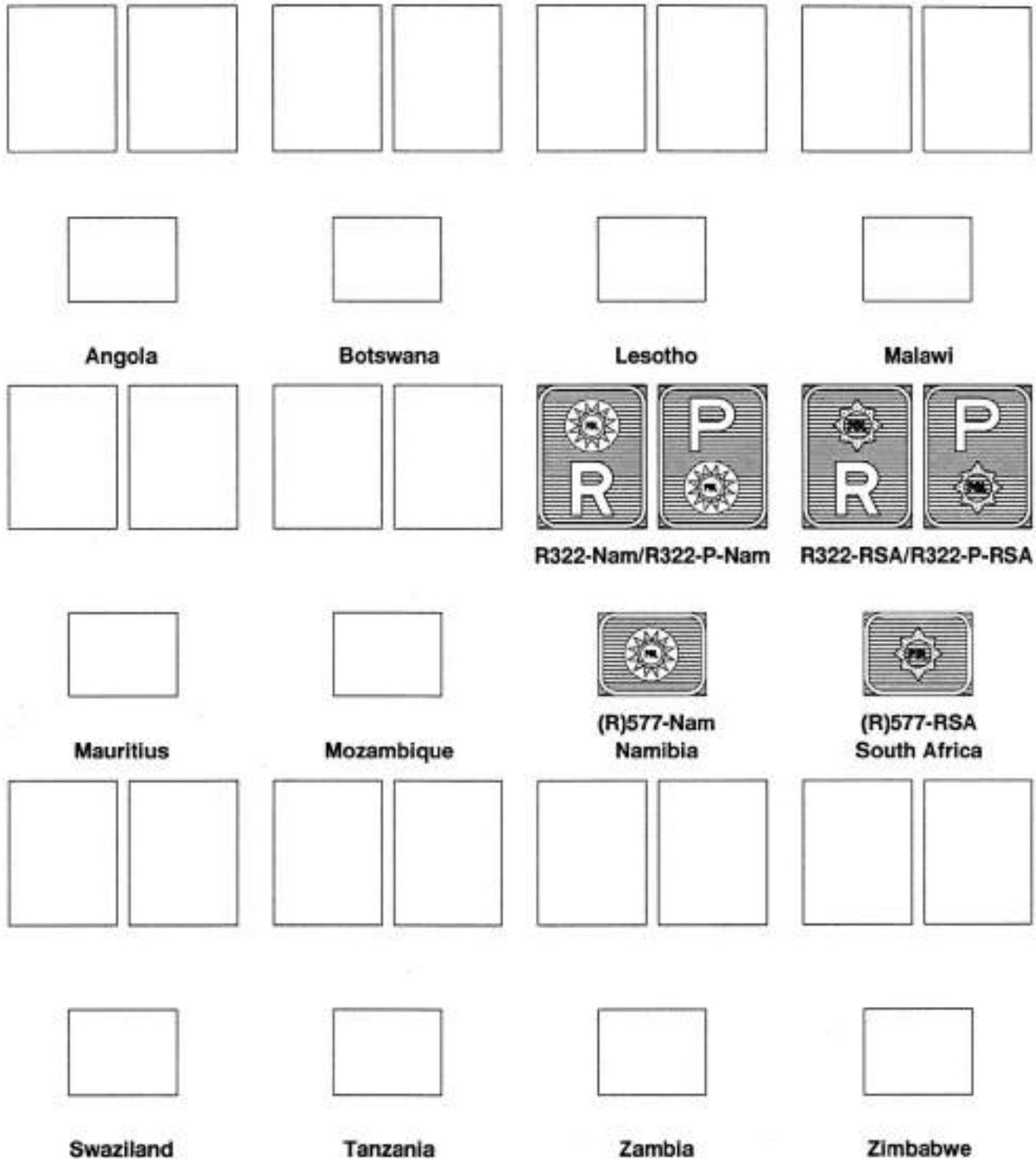


Fig 2.24 Individual National Variants of "Police" Signs in Reservation and Exclusive Secondary Message Classes



WARNING SIGNS

SECTIONS

- 3.0 Contents
- 3.1 Introduction
- 3.2 Road Layout Signs
- 3.3 Direction of Movement Signs
- 3.4 Symbolic Signs
- 3.5 Hazard Marker Signs
- 3.6 Warning Sign Combinations
- 3.7 National Variants

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CHAPTER

3

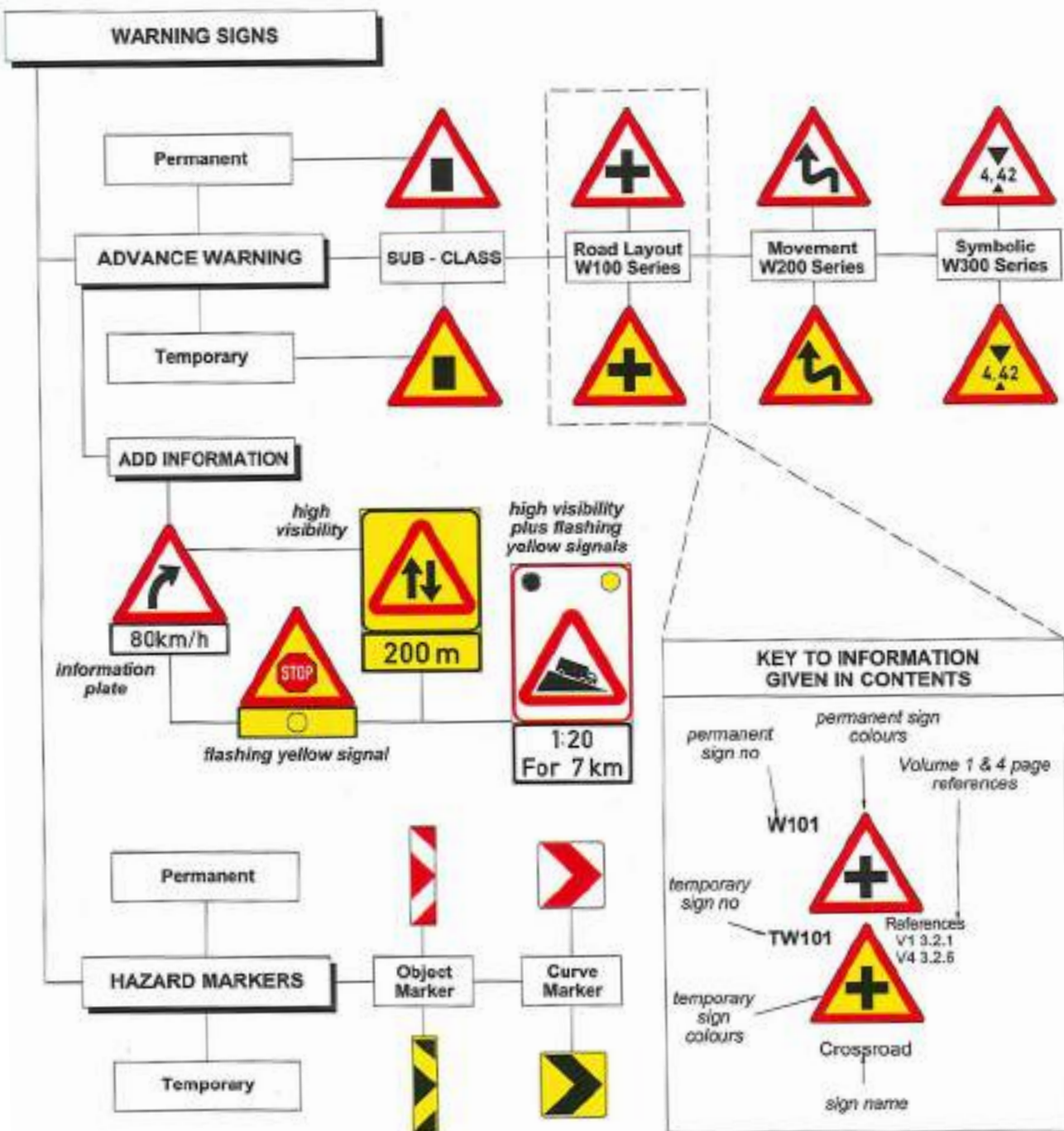
CHAPTER 3: WARNING SIGNS

3.0 CONTENTS

This contents listing illustrates each officially approved sign in the warning sign class with the sign number and name. A page reference is given within this chapter where details of the use of each sign can be found and a cross reference is given to Volume 4 where the symbol and other data are detailed.

The front page of the Contents gives an overview of the warning sign class and its subdivisions. Most signs are available in permanent and temporary forms. If a sign is not recommended in one or other form a blank space has been left and only one form is illustrated. For further details on the use of warning signs see Section 3.1: Introduction.

WARNING SIGN CLASSIFICATION AND COLOUR CODE



Section 3.2: ROAD LAYOUT - Advance Warning Signs

<p>W101</p>  <p>TW101 <small>References V1 3.2.1 V4 3.2.1</small></p>  <p>Crossroad</p>	<p>W102</p>  <p>TW102 <small>References V1 3.2.1 V4 3.2.2</small></p>  <p>Crossroad on Priority Road</p>	<p>W103</p>  <p>TW103 <small>References V1 3.2.1 V4 3.2.3</small></p>  <p>Priority Crossroad on Non-Priority Road</p>	<p>W104</p>  <p>TW104 <small>References V1 3.2.1 V4 3.2.4</small></p>  <p>T-Junction</p>	<p>W105</p>  <p>TW105 <small>References V1 3.2.1 V4 3.2.5</small></p>  <p>Skew T-Junction (Right)</p>
<p>W106</p>  <p>TW106 <small>References V1 3.2.1 V4 3.2.6</small></p>  <p>Skew T-Junction (Left)</p>	<p>W107</p>  <p>TW107 <small>References V1 3.2.2 V4 3.2.7</small></p>  <p>Side Road Junction (Left)</p>	<p>W108</p>  <p>TW108 <small>References V1 3.2.2 V4 3.2.8</small></p>  <p>Side Road Junction (Right)</p>	<p>W109</p>  <p>TW109 <small>References V1 3.2.2 V4 3.2.9</small></p>  <p>Staggered Junctions (Right - Left)</p>	<p>W110</p>  <p>TW110 <small>References V1 3.2.2 V4 3.2.10</small></p>  <p>Staggered Junctions (Left - Right)</p>
<p>W111</p>  <p>TW111 <small>References V1 3.2.3 V4 3.2.11</small></p>  <p>Sharp Junction (Half Left)</p>	<p>W112</p>  <p>TW112 <small>References V1 3.2.3 V4 3.2.12</small></p>  <p>Sharp Junction (Left)</p>	<p>W113</p>  <p>TW113 <small>References V1 3.2.3 V4 3.2.13</small></p>  <p>Sharp Junction (Half Right)</p>	<p>W114</p>  <p>TW114 <small>References V1 3.2.3 V4 3.2.14</small></p>  <p>Sharp Junction (Right)</p>	<p>W115</p>  <p>TW115 <small>References V1 3.2.3 V4 3.2.15</small></p>  <p>Y - Junction</p>
<p>W116</p>  <p>TW116 <small>References V1 3.2.4 V4 3.2.16</small></p>  <p>End of Dual Roadway (To Right)</p>	<p>W117</p>  <p>TW117 <small>References V1 3.2.4 V4 3.2.17</small></p>  <p>End of Dual Roadway (Straight on)</p>	<p>W118</p>  <p>TW118 <small>References V1 3.2.4 V4 3.2.18</small></p>  <p>Beginning of Dual Roadway (Straight on)</p>	<p>W119</p>  <p>TW119 <small>References V1 3.2.4 V4 3.2.19</small></p>  <p>Beginning of Dual Roadway (To Left)</p>	










































Section 3.3: DIRECTION OF MOVEMENT – Advance Warning Signs

● = See Section 3.7 for variations appropriate to individual countries.


<p>W201 ●</p>  <p>References: V1 3.3.1, V4 3.3.1</p> <p>TW201</p>  <p>Traffic Circle</p>	<p>W202</p>  <p>References: V1 3.3.1, V4 3.3.2</p> <p>TW202</p>  <p>Gentle Curve (Right)</p>	<p>W203</p>  <p>References: V1 3.3.1, V4 3.3.3</p> <p>TW203</p>  <p>Gentle Curve (Left)</p>	<p>W204</p>  <p>References: V1 3.3.2, V4 3.3.4</p> <p>TW204</p>  <p>Sharp Curve (Right)</p>	<p>W205</p>  <p>References: V1 3.3.2, V4 3.3.5</p> <p>TW205</p>  <p>Sharp Curve (Left)</p>
<p>W206</p>  <p>References: V1 3.3.2, V4 3.3.6</p> <p>TW206</p>  <p>Hairpin Bend (Right)</p>	<p>W207</p>  <p>References: V1 3.3.2, V4 3.3.7</p> <p>TW207</p>  <p>Hairpin Bend (Left)</p>	<p>W208</p>  <p>References: V1 3.3.3, V4 3.3.8</p> <p>TW208</p>  <p>Winding Road (Right - Left)</p>	<p>W209</p>  <p>References: V1 3.3.3, V4 3.3.9</p> <p>TW209</p>  <p>Winding Road (Left - Right)</p>	<p>W210</p>  <p>References: V1 3.3.4, V4 3.3.10</p> <p>TW210</p>  <p>Combined Curves (Right - Left)</p>
<p>W211</p>  <p>References: V1 3.3.4, V4 3.3.11</p> <p>TW211</p>  <p>Combined Curves (Left - Right)</p>	<p>W212 ●</p>  <p>References: V1 3.3.4, V4 3.3.12</p> <p>TW212</p>  <p>Two - Way Traffic</p>	<p>W213 ●</p>  <p>References: V1 3.3.5, V4 3.3.13</p> <p>TW213</p>  <p>Two - Way Traffic Crossroad</p>	<p>W214</p>  <p>References: V1 3.3.5, V4 3.3.14</p> <p>TW214</p>  <p>Right Lane Ends</p>	<p>W215</p>  <p>References: V1 3.3.5, V4 3.3.15</p> <p>TW215</p>  <p>Left Lane Ends</p>
<p>W216</p>  <p>References: V1 3.3.6, V4 3.3.16</p> <p>TW216</p>  <p>Concealed Driveway (From Right)</p>	<p>W217</p>  <p>References: V1 3.3.6, V4 3.3.17</p> <p>TW217</p>  <p>Concealed Driveway (From Left)</p>	<p>W218</p>  <p>References: V1 3.3.6, V4 3.3.18</p> <p>TW218</p>  <p>Concealed Driveway (From Both Sides)</p>		

Section 3.4: SYMBOLIC – Advance Warning Signs














● = See Section 3.7 for variations appropriate to individual countries.

<p>W301</p>  <p>References V1 3.4.1 V4 3.4.1</p> <p>TW301</p>  <p>Traffic Signals Ahead</p>	<p>W302</p>  <p>References V1 3.4.1 V4 3.4.2</p> <p>TW302</p>  <p>Traffic Control "Stop" Ahead</p>	<p>W303</p>  <p>References V1 3.4.2 V4 3.4.3</p> <p>TW303</p>  <p>Traffic Control "Yield" Ahead</p>	<p>TW304</p>  <p>References V1 3.4.2 V4 3.4.4</p> <p>Traffic Control Ahead</p>	<p>TW305</p>  <p>References V1 3.4.3 V4 3.4.5</p> <p>Scholar Patrol Ahead</p>
<p>W306 ●</p>  <p>References V1 3.4.3 V4 3.4.6</p> <p>TW306</p>  <p>Pedestrian Crossing</p>	<p>W307 ●</p>  <p>References V1 3.4.4 V4 3.4.7</p> <p>TW307</p>  <p>Pedestrians</p>	<p>W308 ●</p>  <p>References V1 3.4.4 V4 3.4.8</p> <p>Children</p>	<p>W309 ●</p>  <p>References V1 3.4.5 V4 3.4.9</p> <p>TW309</p>  <p>Cyclists</p>	<p>W310 ●</p>  <p>References V1 3.4.5 V4 3.4.10</p> <p>TW310</p>  <p>Farm Animals (Cattle)</p>
<p>W311 ●</p>  <p>References V1 3.4.5 V4 3.4.11</p> <p>TW311</p>  <p>Farm Animals (Horses)</p>	<p>W312 ●</p>  <p>References V1 3.4.5 V4 3.4.12</p> <p>TW312</p>  <p>Farm Animals (Sheep)</p>	<p>W313 ●</p>  <p>References V1 3.4.6 V4 3.4.13</p> <p>Wild Animals Ahead</p>	<p>W314</p>  <p>References V1 3.4.6 V4 3.4.14</p> <p>Gate</p>	<p>W315</p>  <p>References V1 3.4.7 V4 3.4.15</p> <p>Motor Gate (Right)</p>
<p>W316</p>  <p>References V1 3.4.7 V4 3.4.16</p> <p>Motor Gate (Left)</p>	<p>W317</p>  <p>References V1 3.4.7 V4 3.4.17</p> <p>Motor Gate</p>	<p>W318 ●</p>  <p>References V1 3.4.7 V4 3.4.18</p> <p>TW318</p>  <p>Railway Crossing</p>	<p>W319 ●</p>  <p>References V1 3.4.8 V4 3.4.19</p> <p>Tunnel</p>	<p>W320</p>  <p>References V1 3.4.8 V4 3.4.20</p> <p>TW320</p>  <p>Height Restricted</p>
<p>W321</p>  <p>References V1 3.4.9 V4 3.4.21</p> <p>TW321</p>  <p>Length Restricted</p>	<p>W322</p>  <p>References V1 3.4.10 V4 3.4.22</p> <p>TW322</p>  <p>Steep Descent</p>	<p>W323</p>  <p>References V1 3.4.10 V4 3.4.23</p> <p>TW323</p>  <p>Steep Ascent</p>	<p>W324</p>  <p>References V1 3.4.11 V4 3.4.24</p> <p>TW324</p>  <p>Slow Moving Heavy Vehicle</p>	<p>W325</p>  <p>References V1 3.4.11 V4 3.4.25</p> <p>TW325</p>  <p>Gravel Road Begins</p>

● = See Section 3.7 for variations appropriate to individual countries.




























<p>W326</p>  <p>TW326 <small>References V1 3.4.12 V4 3.4.26</small></p>  <p>Narrow Bridge</p>	<p>W327</p>  <p>TW327 <small>References V1 3.4.12 V4 3.4.27</small></p>  <p>One Vehicle Width Structure</p>	<p>W328</p>  <p>TW328 <small>References V1 3.4.13 V4 3.4.28</small></p>  <p>Road Narrows Both Sides</p>	<p>W329</p>  <p>TW329 <small>References V1 3.4.13 V4 3.4.29</small></p>  <p>Road Narrows From Right Side Only</p>	<p>W330</p>  <p>TW330 <small>References V1 3.4.13 V4 3.4.30</small></p>  <p>Road Narrows From Left Side Only</p>
<p>W331</p>  <p>TW331 <small>References V1 3.4.14 V4 3.4.31</small></p>  <p>Uneven Roadway</p>	<p>W332</p>  <p>TW332 <small>References V1 3.4.14 V4 3.4.32</small></p>  <p>Speed Humps</p>	<p>W333</p>  <p>TW333 <small>References V1 3.4.15 V4 3.4.33</small></p>  <p>Slippery Road</p>	<p>W334</p>  <p>TW334 <small>References V1 3.4.15 V4 3.4.34</small></p>  <p>Falling Rocks (From Right)</p>	<p>W335</p>  <p>TW335 <small>References V1 3.4.15 V4 3.4.35</small></p>  <p>Falling Rocks (From Left)</p>
<p>TW336 <small>References V1 3.4.16 V4 3.4.36</small></p>  <p>Roadworks</p>	<p>TW337 <small>References V1 3.4.17 V4 3.4.37</small></p>  <p>Grader Working</p>	<p>TW338 <small>References V1 3.4.17 V4 3.4.38</small></p>  <p>Loose Stones</p>	<p>W339</p>  <p>TW339 <small>References V1 3.4.18 V4 3.4.39</small></p>  <p>General Warning</p>	<p>TW340 <small>References V1 3.4.19 V4 3.4.40</small></p>  <p>Surface Step (Right)</p>
<p>TW341 <small>References V1 3.4.19 V4 3.4.41</small></p>  <p>Surface Step (Left)</p>	<p>TW342 <small>References V1 3.4.19 V4 3.4.42</small></p>  <p>Soft Shoulder</p>	<p>TW343 <small>References V1 3.4.20 V4 3.4.43</small></p>  <p>"Stop/Go" Control Ahead</p>	<p>TW344 <small>References V1 3.4.20 V4 3.4.44</small></p>  <p>Construction Vehicles Crossing (From Left)</p>	<p>TW345 <small>References V1 3.4.20 V4 3.4.45</small></p>  <p>Construction Vehicles Crossing (From Right)</p>
<p>W346</p>  <p>TW346 <small>References V1 3.4.21 V4 3.4.46</small></p>  <p>Emergency Flashing Light</p>	<p>TW347 <small>References V1 3.4.21 V4 3.4.47</small></p>  <p>Temporary Police Flashing Light</p>	<p>W348 ●</p>  <p><small>References V1 3.4.22 V4 3.4.48</small></p> <p>Jetty Edge or River Bank</p>	<p>W349</p>  <p>TW349 <small>References V1 3.4.22 V4 3.4.49</small></p>  <p>Crosswinds</p>	<p>W350</p>  <p>TW350 <small>References V1 3.4.23 V4 3.4.50</small></p>  <p>Drift</p>

● = See Section 3.7 for variations appropriate to individual countries.

<p>W351</p>  <p>References V1 3.4.23 V4 3.4.51</p> <p>Low Flying Aircraft</p>	<p>W352 ●</p>  <p>References V1 3.4.24 V4 3.4.52</p> <p>Agricultural Vehicles</p>	<p>TW353</p>  <p>References V1 3.4.24 V4 3.4.53</p> <p>Accident</p>	<p>W354</p>  <p>References V1 3.4.25 V4 3.4.54</p> <p>Reduced Visibility</p>	<p>W355</p>  <p>References V1 3.4.25 V4 3.4.55</p> <p>Congestion</p>
<p>W356 ●</p>  <p>References V1 3.4.26 V4 3.4.56</p> <p>Horse and Rider</p>	<p>W357 ●</p>  <p>References V1 3.4.6 V4 3.4.57</p> <p>Elephant</p>	<p>W358 ●</p>  <p>References V1 3.4.6 V4 3.4.58</p> <p>Warthog</p>	<p>W359 ●</p>  <p>References V1 3.4.6 V4 3.4.59</p> <p>Hippo</p>	<p>W360</p>  <p>References V1 3.4.28 V4 3.4.60</p> <p>Width Restriction</p>
<p>W361</p>  <p>References V1 3.4.27 V4 3.4.61</p> <p>Electrical Shock</p>	<p>W362 ●</p>  <p>References V1 3.4.27 V4 3.4.62</p> <p>Tram</p>	<p>W363</p>  <p>References V1 3.4.11 V4 3.4.63</p> <p>Gravel Road Ends</p>		

Section 3.5: HAZARD MARKER SIGNS

● = See Section 3.7 for variations appropriate to individual countries.

<p>W401</p>  <p>TW401</p>  <p>References V1 3.5.1 V4 3.5.1</p> <p>Danger Plate/ Delineator Plate</p>	<p>W402</p>  <p>TW402</p>  <p>References V1 3.5.1 V4 3.5.1</p> <p>Danger Plate/ Delineator Plate</p>	<p>W403</p>  <p>References V1 3.5.2 V4 3.5.2</p> <p>Railway Crossing</p>	<p>W404</p>  <p>References V1 3.5.2 V4 3.5.2</p> <p>Railway Crossing (more than one line)</p>
<p>W405</p>  <p>TW405</p>  <p>References V1 3.5.3 V4 3.5.3</p> <p>Sharp Curve Chevron (Single)</p>	<p>W406</p>  <p>TW406</p>  <p>References V1 3.5.3 V4 3.5.3</p> <p>Sharp Curve Chevron (Single)</p>	<p>W407</p>  <p>References V1 3.5.3 V4 3.5.4</p> <p>TW407</p>  <p>Sharp Curve Chevron (Triple)</p>	<p>W408</p>  <p>References V1 3.5.3 V4 3.5.4</p> <p>TW408</p>  <p>Sharp Curve Chevron (Triple)</p>
<p>W409</p>  <p>TW409</p>  <p>References V1 3.5.5 V4 3.5.5</p> <p>T-Junction Chevron</p>	<p>W410</p>  <p>TW410</p>  <p>References V1 3.5.6 V4 3.5.6</p> <p>Dead End / Road Closed Chevron</p>		
<p>W411</p>  <p>TW411</p>  <p>References V1 3.5.7 V4 3.5.7</p> <p>Boom/ Barricade</p>	<p>W412</p>  <p>References V1 3.5.8 V4 3.5.8</p> <p>Traffic Signals Out of Order</p>		
<p>W413</p>  <p>TW413</p>  <p>References V1 3.5.8 V4 3.5.8</p> <p>Gore Plate</p>	<p>W414</p>  <p>TW414</p>  <p>References V1 3.5.8 V4 3.5.9</p> <p>Gore Chevron</p>	<p>W415</p>  <p>TW415</p>  <p>References V1 3.5.9 V4 3.5.11</p> <p>Overhead Danger Plate</p>	

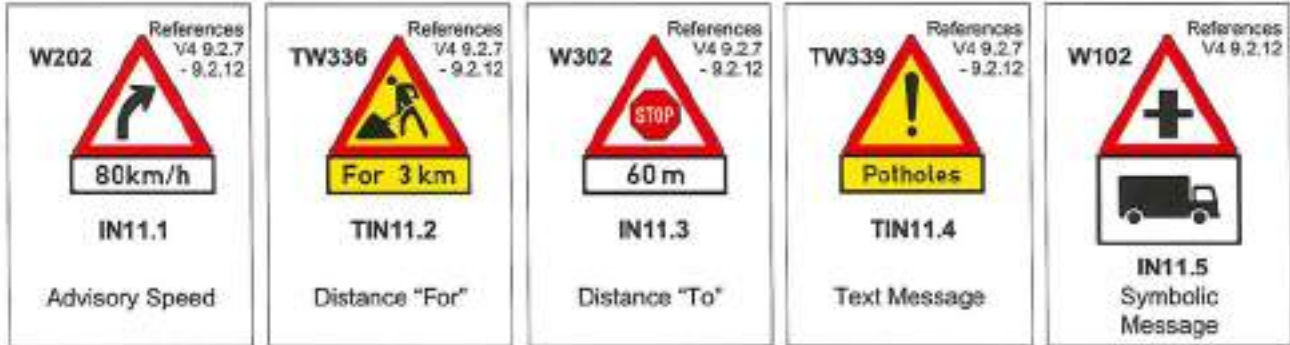
Section 3.6: COMBINATION – Advance Warning Signs

The effectiveness of advance warning signs may be enhanced by using them in combination with other road traffic signs such as:

- (b) HIGH VISIBILITY background signs;
- (c) Flashing yellow signals SS3; or combinations of these; both permanent and temporary examples are shown.

(a) SUPPLEMENTARY PLATE information sign in11;

Advance Warning Signs with Supplementary Plates



Advance Warning Signs in High Visibility Backgrounds



with Flashing Yellow Signals - SS3



Multiple Combinations



CHAPTER 3: WARNING SIGNS

3.1 INTRODUCTION

3.1.1 General

- 1 Warning signs are used to alert drivers to hazardous or potentially hazardous conditions, on or adjacent to the roadway. Warning signs indicate a need for extra caution by road users, and may require a reduction in speed or other manoeuvre, in the interest of their safety and that of other drivers, pedestrians and animals.
- 2 Adequate warning signs can greatly assist road safety however, their use should be restricted to a minimum consistent with safety. The use of warning signs under normal conditions where a driver can be expected to see and appreciate the potential hazard should be avoided. This brings road traffic signs in general into disrepute and detracts from their effectiveness.
- 3 The sign applicable to a specific condition shall be selected in accordance with the criteria set out in this section. The warning signs listed cover most conditions likely to be met. If other warning signs are needed, the signs shall conform to the standard shape and colour for warning signs (see Subsection 3.1.12).
- 4 In principle any PERMANENT warning sign symbol may be used in the appropriate colours for a TEMPORARY warning sign. In order to specify a temporary warning sign, the number of the permanent sign should be quoted, preceded by the prefix letter T:
e.g.
permanent CROSSROAD warning sign- W101
temporary CROSSROAD warning sign- TW101.
- 5 Due to the function of certain warning signs their application should always be of a temporary nature. For this reason these signs will always be numbered with a T prefix letter:

e.g.	roadworks	TW336
	grader working -	TW337

3.1.2 Types of Warning Signs

- 1 Warning signs may be PERMANENT or TEMPORARY and they may be located in advance of the hazard or potential hazard, or at the hazard.
- 2 Where conditions require greater visual impact or emphasis warning signs may be displayed within a HIGH VISIBILITY background (see Section 3.6).
- 3 In circumstances where varying conditions of topography and/or weather occur, it may be necessary to provide a VARIABLE MESSAGE warning sign which is capable of displaying a number of warning messages appropriate to the possible hazards which may occur (see Section 3.6).

3.1.3 Classification of Warning Signs

- 1 Warning signs are classified and numbered into the following groups:
 - (a) ROAD LAYOUT group numbered in the W100 series.
 - (b) DIRECTION OF MOVEMENT group numbered in the W200 series.
 - (c) SYMBOLIC group numbered in the W300 series. This series covers the following categories with representative pictograms :
 - (i) traffic control;
 - (ii) pedestrians, cyclists and animals;
 - (iii) roadway constraints and surface condition;
 - (iv) roadworks (or other temporary installations);
 - (v) miscellaneous.
 - (d) HAZARD MARKER group numbered in the W400 series.
- 2 Warning signs in groups (a), (b) and (c) are principally used in advance of the hazard or potential hazard site.
- 3 Warning signs in group (d) are used to indicate the actual location of a hazard or potential hazard.

3.1.4 Shape

- 1 All warning signs used in advance of a hazard or potential hazard have an equilateral triangular shape and are mounted with the apex uppermost.
- 2 According to the need for greater conspicuity any triangular warning sign may be displayed in a rectangular HIGH VISIBILITY background.
- 3 Hazard marker signs have a rectangular shape. Exceptions are the RAILWAY CROSSING and the TRAFFIC SIGNAL OUT OF ORDER hazard marker signs which have a cross shape.

3.1.5 Colour

- 1 All PERMANENT advance warning signs shall consist of a black symbol on a white background with a red triangular border. When displayed on a HIGH VISIBILITY background, the HIGH VISIBILITY background shall be white with a red border.
- 2 All TEMPORARY advance warning signs shall consist of a black symbol on a yellow background with a red triangular border. When displayed on a HIGH VISIBILITY background, the HIGH VISIBILITY background shall be yellow with a black border.
- 3 Hazard marker signs have unique colours according to their application as detailed in Section 3.5.

3.1.6 Retroreflectivity and Illumination

- 1 All PERMANENT warning signs may be fully retroreflective with the exception of any black areas which shall be of a semi-matt finish. Where a warning sign has no night-time significance semi-matt finishes may be specified, however, the use of retroreflective materials for red and white areas is recommended to cover the possible extension of the hazard related activity into pre-dawn or dusk periods of the day. For further guidance see Subsections on specific warning signs.
- 2 All TEMPORARY warning signs shall be fully retroreflective with the exception of any black areas which shall be of a semi-matt finish, unless the maintenance unit using the signs is going to operate during daylight hours only, in which case the signs may be semi-matt.

- 3 External or internal illumination may be specified as an alternative to the use of retroreflective materials.

3.1.7 Location

- 1 In general the lateral and vertical location of warning signs shall comply with the provisions of Chapter 1.
- 2 Hazard marker warning signs should be located as close as possible to the hazard being marked.
- 3 Unless indicated otherwise in the descriptive sections on the individual signs, advance warning signs should be displayed at the distance from the hazard given in Table 3.1. The location of advance warning signs may, alternatively be determined by using Figure 3.1 (see page 3.1.4).
- 4 In the event that inadequate sight distance exists to the warning sign when located in accordance with Table 3.1 or Figure 3.1, the warning sign shall be moved further from the hazard until the clear visibility distance given in Table 3.2 is realised.
- 5 When a warning sign is located 300m or more from the hazard the use of a distance supplementary plate is recommended.
- 6 Normally only one warning sign should be attached to one support pole, although it may be an advantage to combine a regulatory and a warning sign so that their messages supplement each other. If it is necessary to convey two or more different warning messages for one location, separate signs may be located V m apart (where V is the operating speed in km/h). The closest sign to the hazard should conform to the requirements of Table 3.1 or Figure 3.1.
- 7 Throughout this Chapter when reference is made to operating speed this should be used if it is known. If this speed is not known the design speed may be used.

TABLE 3.1 ADVANCE WARNING SIGN LOCATION AND SIZE **TABLE 3.1**

Operating speed (km/h)	Location distance from hazard (m)(2)	Recommended size (mm)
120	330 (400)	1500
100	240 (320)	1500
80	160 (218)	1200
60	120 (160)	900

NOTES:

(1) Hazard marker warning signs are located at the hazard - see Section 3.5 for sizes.

(2) If advance warning signs are provided on gravel roads the distances in brackets are recommended.

TABLE 3.2 VISIBILITY DISTANCE TO WARNING SIGN **TABLE 3.2**

Operating speed (km/h)	Clear visibility distance (m)
120	120
100	100
80	80
60	60

3.1.8 Supplementary Plates

- 1 Supplementary plates may be used with warning signs to qualify the principal message by stating:
 - (a) an advisory speed;
 - (b) a distance;
 - (c) a text message.
- 2 Wherever possible a supplementary plate should be dimensioned to match the length of the bottom side of the warning sign which it supplements.
- 3 Supplementary plates used with fully retroreflective warning signs shall be fully retroreflective (see Subsection 3.1.6).
- 4 Distance supplementary plates are recommended for consideration in conjunction with warning signs, particularly when sight distances to hazards are restricted. The use of distance supplementary plates is particularly recommended with TEMPORARY warning signs to give drivers information on the length of a roadworks site :

e.g. "For 8 km"

and to give a countdown indication into a major deviation of alignment with sequential signs :

e.g. "600 m", "400 m", "200m".
- 5 For examples of supplementary plates used with warning signs see Section 3.6 and for further information on colour coding, etc., see Chapter 5, Subsection 5.2.6.

3.1.9 Size

- 1 Full dimensional details of all warning signs are given in Volume 4 of the Manual.
- 2 The size of a warning sign should be suited to the conditions for which it is required. Traffic volumes, speeds, road conditions, background lighting and other factors will all influence the choice of the appropriate size. Generally when conditions require greater visual impact or emphasis larger size signs should be used (see Table 3.1 on page 3.1.2).
- 3 Standard sizes covered in Volume 4 are:
 - (a) triangular signs- nominal side length :
 - 900 mm
 - 1200 mm
 - 1500 mm
 - 1800 mm

- (b) triangular signs displayed on a HIGH VISIBILITY background - nominal side length:
 - 900 mm
 - 1235mm
 - 1834 mm.

3.1.10 Portable Warning Signs

- 1 Portable warning signs are commonly used in temporary situations. Such signs should be in TEMPORARY sign colours.
- 2 The reverse side of a portable warning sign which is used or displayed in such a manner that it may be a source of danger to traffic travelling in the opposite direction should be marked with 150 mm wide yellow retroreflective stripes on a black semi-matt background.

3.1.11 Applications

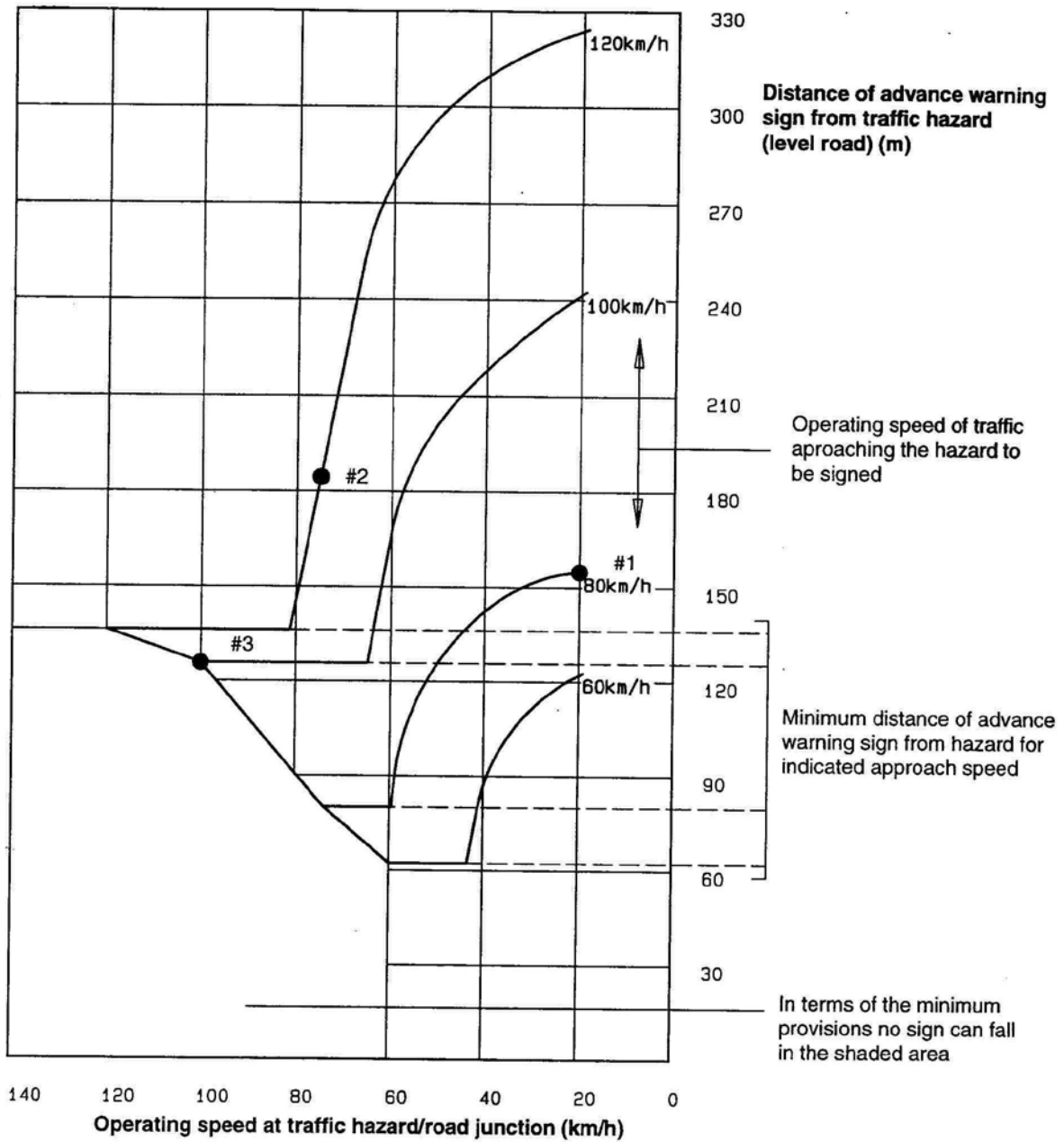
- 1 Various types of sign may be mounted or displayed in combination with each other. Examples of such combinations involving warning signs are covered in Section 3.6.
- 2 Examples of specific applications of warning signs will be covered in relevant chapters of Volume 2 of the Manual.

3.1.12 Other Warning Signs

- 1 When no specific warning symbol or legend is available, the appropriate warning message should be indicated within the sign in black semi-matt letters of the DIN 1451 letter type of the style and size permitted by the space available and related to the operating speed of the road, but not less than 70 mm high (see Volume 4, Chapter 3, Section 3.1).
- 2 The need to manufacture any such sign shall be reported by the authority to:

The Secretary
Route Numbering and Road Traffic Signs Sub-Committee
c/o Department of Transport
Private Bag X193
PRETORIA
0001.

This will enable a co-ordinated approach to be adopted for the design of an appropriate symbol for general use.



Typical Examples

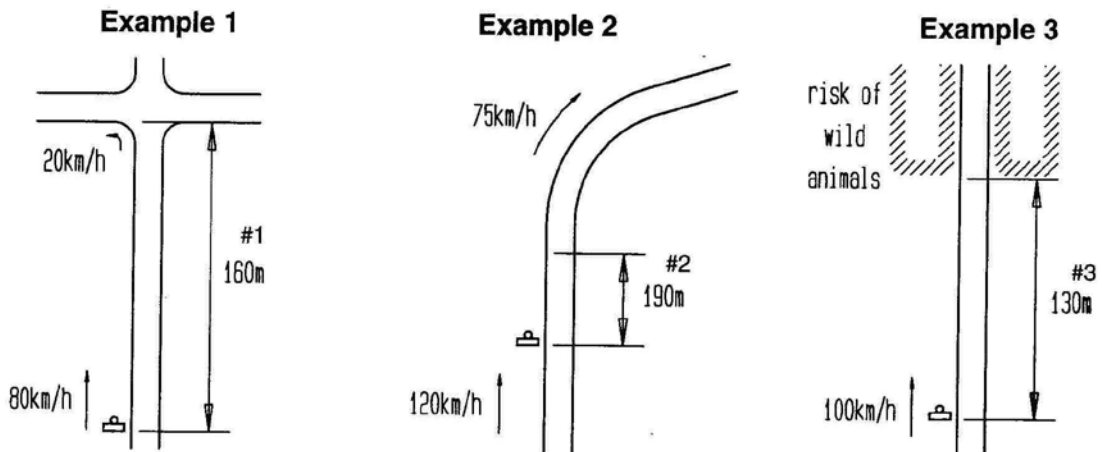


Fig 3.1

Location of Advance Warning Signs

CROSSROAD

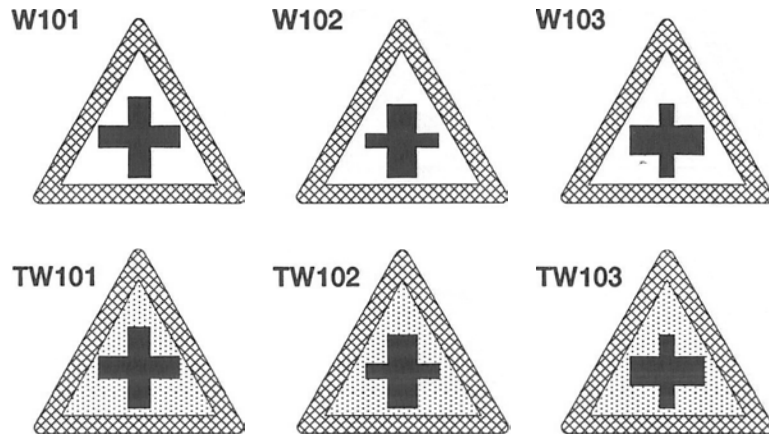
COLOURS:

PERMANENT

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective



For dimensions

ref. Vol. 4

pages

3.2.1

3.2.2

3.2.3

3.2.1 Crossroad

- 1 The CROSSROAD warning signs W101, W102 or W103 are **to warn road users that a crossroad is ahead**. The signs should be displayed in advance on all rural crossroad approaches that have no advance direction signs. They may also be used in urban areas if the junction has a history of accidents or speeding or when sight distance is limited.
- 2 Sign W101 should be used when it is not necessary to indicate the priority on the approaches, ie. at a four-way STOP controlled junction.
- 3 Sign W102 may be used in advance on crossroad approaches that have no advance direction signs, to indicate that these approaches have priority.
- 4 Sign W103 may be used in advance on approaches that have no advance direction signs, to warn that the approaches are controlled by STOP signs R1, or YIELD signs R2. If such approaches have a history of accidents then TRAFFIC CONTROL "STOP" AHEAD sign W302 or TRAFFIC CONTROL "YIELD" AHEAD sign W303 may be used as an alternative (see Subsections 3.4.2 and 3.4.3).
- 5 If the crossroad has a history of accidents an appropriate warning sign may be used in addition to advance direction signs and/or traffic signal control.

T- AND SKEW T-JUNCTIONS

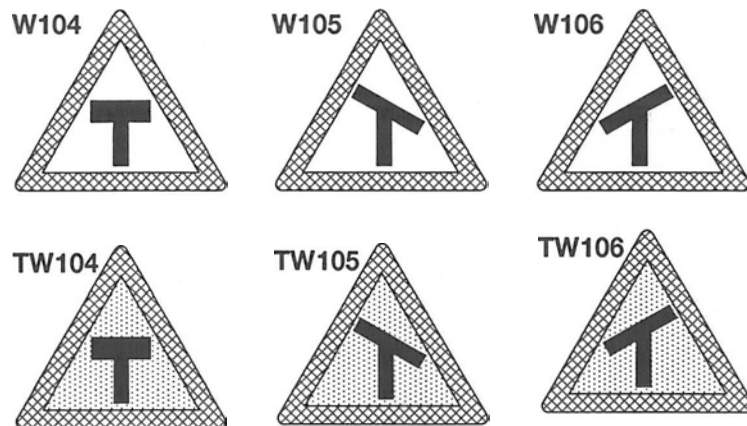
COLOURS:

PERMANENT

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective



For dimensions

ref. Vol. 4

pages

3.2.4

3.2.5

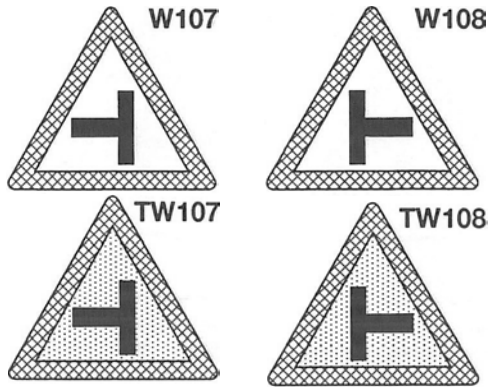
3.2.6

3.2.2 T- and Skew T-junctions

- 1 The T-JUNCTION and SKEW T-JUNCTION warning signs W104, W105 and W106 are **to warn road users that a T-junction is ahead where traffic is required to make a sharp or acute turn either to the left or to the right**. These signs should be displayed in advance on approaches that have no advance direction signs.
- 2 These signs may also be displayed in advance on an approach to a junction controlled by a STOP sign R1, or a YIELD sign R2. If the junction has a history of accidents then TRAFFIC CONTROL "STOP" AHEAD sign W302, or TRAFFIC CONTROL "YIELD" AHEAD sign W303 may be used as an alternative (see Sub-sections 3.4.2 and 3.4.3).
- 3 Sign W105 should be used in advance on an approach that has no advance direction signs when that approach intersects the crossing road at an angle of 15 degrees or more to the right of perpendicular.
- 4 Sign W106 should be used in advance on an approach that has no advance direction signs when that approach intersects the crossing road at an angle of 15 degrees or more to the left of perpendicular.
- 5 If the junction has a history of accidents an appropriate warning sign may be used in addition to advance direction signs and/or traffic signal control.

SIDE ROAD JUNCTIONS

For dimensions
Ref. Vol.4
pages
3.2.7
3.2.8



COLOURS:
PERMANENT
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective
TEMPORARY
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.2.3 Side-road Junctions

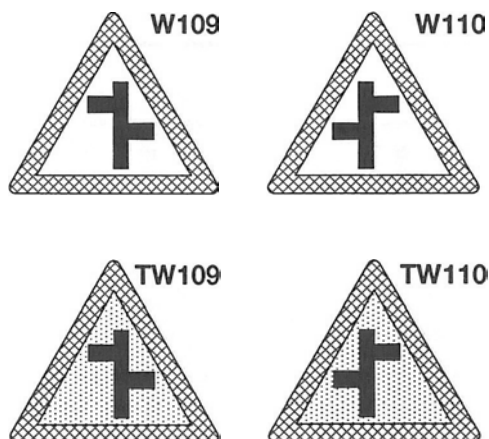
- 1 The SIDE-ROAD JUNCTION warning signs W107 and W108 are **to warn road users that joining or leaving traffic from the left or right, or a restricted sight distance to left or right may make the junction ahead dangerous or potentially dangerous.** They should be displayed in advance on approaches that have no advance direction signs.
- 2 The signs should not normally be located on the approach to a side-road junction that has:
 - (a) advance direction signs;
 - (b) traffic signal control.

If the side-road junction has a history of accidents an appropriate warning sign may be used in addition to advance direction signs and/or traffic signal control.

- 3 Temporary warning signs TW107 and TW108 may be used under the same circumstances as permanent SIDE-ROAD JUNCTION warning signs when temporary side-road junctions are created as a result of roadworks.

STAGGERED JUNCTIONS

For dimensions
ref. Vol. 4
pages
3.2.9
3.2.10



COLOURS:
PERMANENT
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective
TEMPORARY
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.2.4 Staggered Junctions

- 1 The STAGGERED JUNCTIONS warning signs W109 and W110 are **to warn road users that joining or leaving traffic or a restricted sight distance may make closely situated side-road junctions ahead dangerous or potentially dangerous.**
- 2 Closely situated side-road junctions are considered to be staggered junctions when the distance between them is equal to or less than a distance given by:

D less than or equal to $3,0 \sqrt{V}$ metres.

V is the operating speed or design speed on the through route when the distance between the side-

road junctions exceeds this value then separate SIDEROAD JUNCTION signs W107 or W108 should be used.

- 3 If the junctions have a history of accidents an appropriate warning sign may be used in addition to advance direction signs, channelizing islands and/or traffic signals.
- 4 The signs should be located in advance of the first junction of the staggered junctions in accordance with Table 3.1 or Figure 3.1.

SHARP JUNCTIONS

COLOURS:

PERMANENT

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

For dimensions

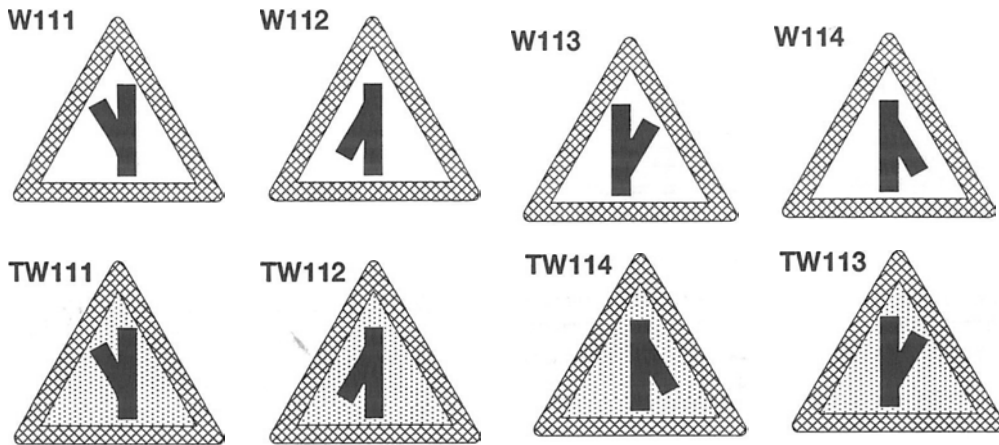
ref. Vol. 4
 pages

3.2. 11

3.2. 12

3.2. 13

3.2. 14



3.2.5 Sharp Junctions

- 1 The SHARP JUNCTION warning signs W111, W112, W113 and W114 are **to warn road users that joining or leaving traffic or a restricted sight distance may make the sharp junction ahead dangerous or potentially dangerous.**
- 2 The appropriate SHARP JUNCTION warning sign will normally be located on a road accorded priority at the junction. If the approach is controlled by a STOP sign R1 or a YIELD sign R2, the appropriate SHARP JUNCTION sign may be used in addition to the control signs.

- 3 SHARP JUNCTION warning sign W112 is commonly used to warn drivers of a freeway on-ramp junction with the main freeway carriageway. The sign should be located to be visible to drivers on the freeway. In this application the sign applies to the junction of one-way traffic flows. SHARP JUNCTION warning signs are equally applicable to sharp junctions on two-way roads.
- 4 Temporary warning signs TW111, TW112, TW113 and TW114 may be used under the same circumstances as permanent SHARP JUNCTION warning signs when temporary sharp junctions occur at roadworks.

COLOURS:

PERMANENT

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

TEMPORARY

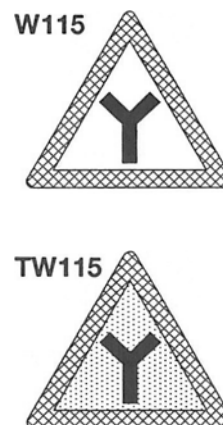
Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

Y-JUNCTION

For dimensions

ref. Vol. 4
 page

3.2. 15



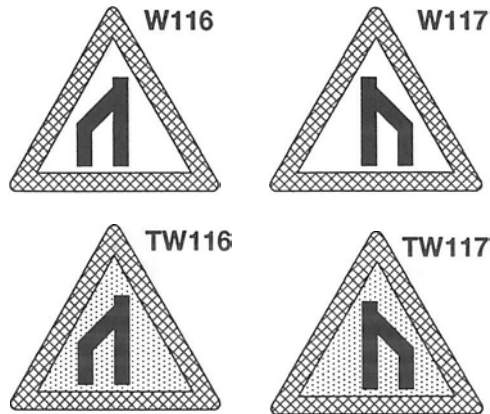
3.2.6 Y-junction

- 1 The Y-JUNCTION warning sign W115 is **to warn road users that a Y-junction is ahead on the road that forms the stem of the Y.** The sign may be displayed in advance on approaches that have no advance direction signs.
- 2 The sign may also be displayed in advance on an approach to a Y-junction which has no advance direction sign but is controlled by a STOP sign R1, or a YIELD sign R2.

- 3 If the Y-junction has a history of accidents an appropriate warning sign may be used in addition to advance direction signs and/or traffic signals.
- 4 When it is considered necessary to display warning signs on the other approaches to a Y-junction and the angle of junction is such that the appearance is no longer that of a Y-JUNCTION, SKEW T-JUNCTION warning signs W105 or W106 may be used.

END OF DUAL ROADWAY

For dimensions
Ref. Vol.4
pages
3.2.16
3.2.17



COLOURS:
PERMANENT
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective
TEMPORARY
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.2.7 End of Dual Roadway

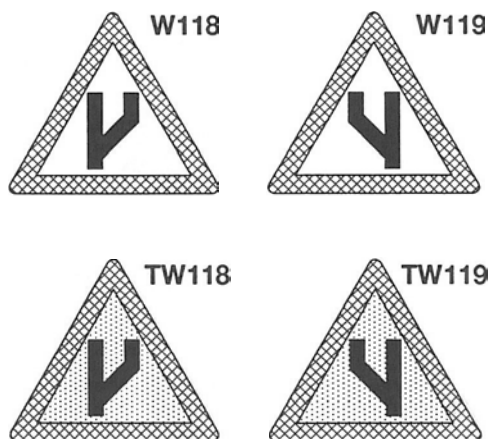
- 1 The END OF DUAL ROADWAY warning signs W116 and W117 are **to warn road users that there is a junction of two one-way roadways ahead and that a roadway with two-way traffic is about to be entered.**
- 2 Sign W116 should be used in advance of a situation where the roadway being signed deviates to the right of sign W117 and in advance of a situation where the roadway being signed continues on, the principal alignment of the approach roadway whether this be straight or curved. It is recommended that the signs be displayed on both sides of the roadway when the median island permits.
- 3 The signs should be displayed in conjunction with TWO-WAY TRAFFIC warning sign W212. Sign W212 should be located at a distance from the point where two-way traffic commences, in accordance with Table 3.1 or Figure 3.1.

Signs W116 or W117 should then be located in advance of sign W212 by a distance equal to "V" metres (where V is the operating speed in km/h). These locational distances should be adjusted if warranted by the geometric layout (see paragraph 3.1.7.6 on page 3.1.5 and Volume 2).

- 4 Temporary warning signs TW116 and TW117 may be particularly appropriate during roadworks when a temporary crossing is taken through a median island. Temporary diagrammatic signs should be considered for their greater visibility if traffic volumes or speeds are high (see Sections 3.6 and 4.10).
- 5 Signs W116, W117 or TW116 and TW117 should not be used to indicate a road narrows situation on an undivided roadway (see Subsections 3.3.9, 3.4.23 and 3.4.24).

BEGINNING OF DUAL ROADWAY

For dimensions
ref. Vol. 4
pages
3.2.18
3.2.19



COLOURS:
PERMANENT
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective
TEMPORARY
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.2.8 Beginning of Dual Roadway

- 1 The BEGINNING OF DUAL ROADWAY warning signs W118 and W119 are **to warn road users that a two-way roadway is ending and that a one-way roadway is about to be entered at which point a physical median island will commence.**
- 2 Sign W118 should be used in advance of a situation where the median island commences when the roadway being signed follows, and sign W119 in advance of a situation where the median island commences when the roadway being signed deviates to the left, from the principal alignment of the approach road whether this be straight or curved.

- 3 The signs should be displayed in accordance with the provision of Table 3.1 or Figure 3.1. These distances should be adjusted if the geometric layout of the BEGINNING OF DUAL ROADWAY being signed requires special treatment.
- 4 Temporary warning signs TW118 and TW119 may be particularly appropriate during roadworks when a temporary crossing is taken through a median island to return traffic to the normal dual roadway width at the end of a construction site. Temporary diagrammatic signs should be considered for their greater visibility if traffic volumes or speeds are high (see Sections 3.6 and 4.10).

TRAFFIC CIRCLE

COLOURS:

PERMANENT

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

For dimensions
 ref. Vol. 4
 page
 3.3.1



3.3.1 Traffic Circle

- 1 The TRAFFIC CIRCLE warning sign W201 is **to warn approaching road users that the direction of movement at the junction ahead is clockwise round the mini circle or roundabout.**
- 2 The sign should be displayed in advance of a mini circle or roundabout at a distance in accordance with the design speed of the road and the safe speed at which the mini circle or roundabout can be negotiated. Figure 3.1 should be used to determine the appropriate distance.
- 3 The sign should normally not be displayed on an approach to a roundabout where a MAP-TYPE TRAFFIC

CIRCLE JUNCTION sign GD8 is displayed.

- 4 The sign shall not be displayed on any approach to a mini circle or roundabout which is not operating under the normal priority control system applicable to mini circles and roundabouts. (MAP-TYPE direction signs should be used especially if RIGHT OF WAY signs IN7 have been displayed on the principal approaches to the junction.)
- 5 Temporary warning sign TW201 may be used under the same circumstances as a permanent TRAFFIC CIRCLE warning sign when a temporary traffic circle is created to deal with traffic on a roadworks detour.

GENTLE CURVE

COLOURS:

PERMANENT

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

For dimensions
 ref. Vol. 4
 page
 3.3.2
 3.3.3



3.3.2 Gentle Curve

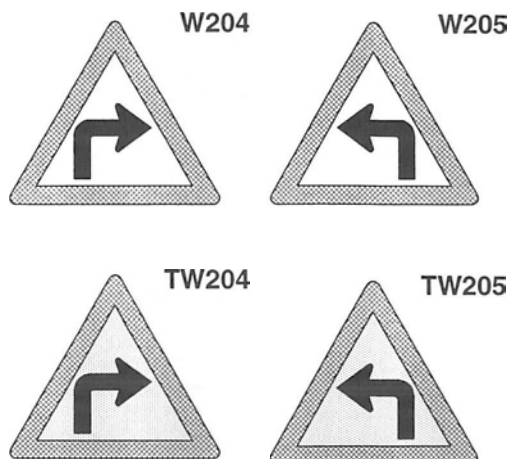
- 1 The GENTLE CURVE warning signs W202 and W203 are **to warn road users of a gentle curve ahead to right or to left.**
- 2 These signs should be displayed in advance of an obscured curve that can only be negotiated comfortably by reducing speed by one tenth to one third of the operating speed of traffic travelling on the preceding straight. The comfortable safe speed should be determined by actual trial runs. Figure 3.1 should be used

to determine the advance distance for location of the sign. The advisory safe speed may be indicated by displaying a supplementary information plate below the sign on the same post (see Section 3.6).

- 3 Temporary warning signs TW202 and TW203 may be used under the same circumstances as permanent GENTLE CURVE warning signs when gentle curves exist within detours created at roadworks sites.

SHARP CURVE

For dimensions
ref. Vol. 4
pages
3.3.4
3.3.5



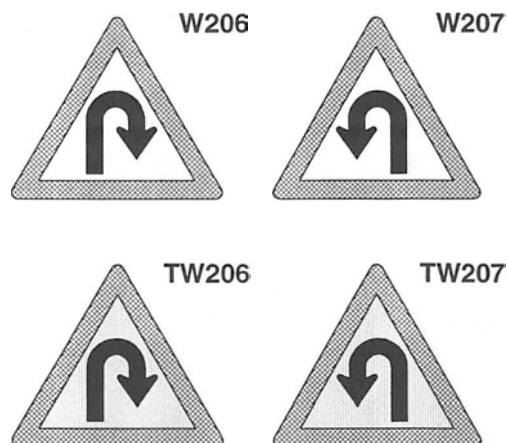
COLOURS:
PERMANENT
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective
TEMPORARY
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.3.3 Sharp Curve

- 1 The SHARP CURVE warning signs W204 and W205 are **to warn road users of a sharp curve ahead to the right or to the left.**
- 2 These signs should be displayed in advance of an obscured curve that can only be negotiated comfortably by reducing speed by more than one third of the operating speed of traffic travelling on the preceding straight. The comfortable safe speed should be determined by actual trial runs.
- 3 The signs should be located in advance of the beginning of the curve at a distance dependent on the average entering speed for the preceding straight and the safe speed determined from the trial runs. Figure 3.1 should be used to determine the advance distance. The advisory safe speed may be indicated by displaying a supplementary information plate below the sign on the same post (see Section 3.6).
- 4 Temporary warning signs TW204 and TW205 may be used under the same circumstances as permanent SHARP CURVE warning signs when sharp curves exist within detours created at roadworks sites.

HAIRPIN BEND

For dimensions
ref. Vol. 4
pages
3.3.6
3.3.7



COLOURS:
PERMANENT
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective
TEMPORARY
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.3.4 Hairpin Bend

- 1 The HAIRPIN BEND warning signs W206 and W207 are **to warn road users of a sharp bend ahead which results in an almost complete change of direction to the right or to the left.**
- 2 These signs should be displayed in advance of an obscured sharp bend that can only be negotiated by reducing speed by more than half of the operating speed of traffic travelling on the preceding straight.
- 3 The signs should be located in advance of the beginning of the curve at a distance dependent on the average entering speed for the preceding straight and the safe speed determined from the trial runs. Figure 3.1 should be used to determine the advance distance. The advisory safe speed may be indicated by displaying a supplementary information plate below the sign on the same post (see Section 3.6).
- 4 Temporary warning signs TW206 and TW207 may be used under the same circumstances as permanent HAIRPIN BEND warning signs when hairpin bends exist within detours at roadworks sites.

COLOURS:

PERMANENT

Triangle: Red retroreflective

Symbol: Black semi-matt

Background: White retroreflective

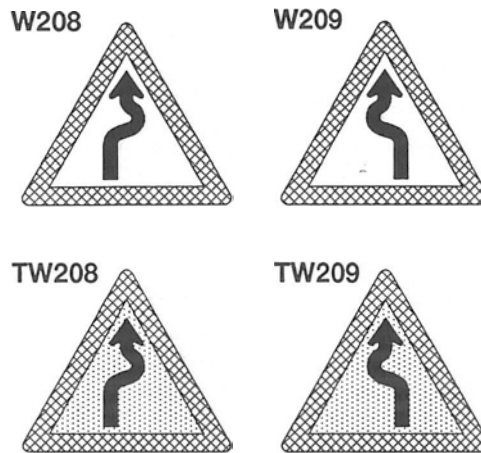
TEMPORARY

Triangle: Red retroreflective

Symbol: Black semi-matt

Background: Yellow retroreflective

WINDING ROAD

For dimensions
ref. Vol.4
pages

3.3.8

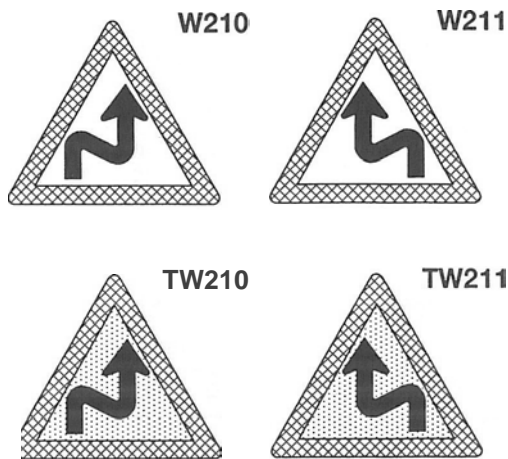
3.3.9

3.3.5 Winding Road

- 1 The WINDING ROAD warning signs W208 and W209 are **to warn road users of a series of curves in the road ahead**. These signs should be displayed in advance of a section of road in which a number of reverse curves exist such that the safe comfortable speed is considerably below that for the remainder of the road.
- 2 Signs W208 and W209 should be displayed where:
 - (a) speed should be reduced by one tenth to one third of the operating speed of traffic travelling on the preceding straight; or
 - (b) the length of the straight between curves is less than 120m; or
 - (c) the nature of the reverse curves is not obvious to approaching traffic and therefore constitutes a hazard.
- 3 The sign symbol should be chosen so that it correctly represents the direction of curvature of the first curve in the series; W208 when the first curve is to the right and W209 when the first curve is to the left.
- 4 The length of a section of road consisting of several succeeding reverse curves should be displayed to the nearest kilometre on a supplementary plate below sign W208 or W209 and on the same post. If the section is longer than 10 km the sign should be repeated every 10 km with the reduced distances displayed (see Section 3.6).
- 5 Individual curves where speed should be reduced by more than one third of the operating speed of traffic travelling on the preceding winding road section should be indicated by the appropriate SHARP CURVE warning signs W204 or W205, or HAIRPIN BEND warning signs W206 or W207.
- 6 Signs W208 and W209 should be located in advance of the beginning of the winding section of road at a distance dependent on the average entering speed for the preceding straight. Figure 3.1 should be used to determine the advance distance.
- 7 Temporary warning signs TW208 and TW209 may be used under the same circumstances as permanent WINDING ROAD warning signs on winding detours at roadworks sites.

COMBINED CURVES

For dimensions
ref. Vol. 4
pages
3.3.10
3.3.11



COLOURS:

PERMANENT

Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

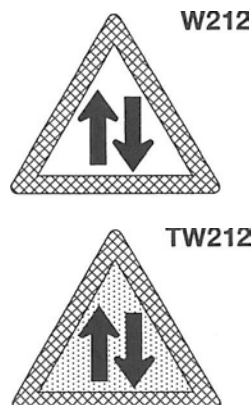
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.3.6 Combined Curves

- The COMBINED CURVES warning signs W210 and W211 are **to warn road users of a combination of two sharp curves in opposite directions.**
- Signs W210 and W211 should be displayed where:
 - speed should be reduced by more than one third of the operating speed of traffic travelling on the preceding straight; or
 - the length of the straight between curves is less than 120 m; or
 - the nature of the reverse curves is not obvious to approaching traffic and is therefore a hazard.
- Sign W210 is for a combined curve to the right and then to the left. Sign W211 is for a combined curve to the left and then to the right.
- These signs should be located in advance of the beginning of the curve at a distance dependent on the average entering speed for the preceding straight and the safe speed determined from the trial runs. Figure 3.1 should be used to determine the advance distance. The advisory safe speed may be indicated by displaying a supplementary information plate below the sign on the same post (see Section 3.6).
- Temporary warning signs TW210 and TW211 may be used under the same circumstances as permanent COMBINED CURVES warning signs when combined curves exist within detours at roadworks sites.

TWO-WAY TRAFFIC

For dimensions
ref. Vol. 4
page
3.3.12



COLOURS:

PERMANENT

Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.3.7 Two-way Traffic

- The TWO-WAY TRAFFIC warning sign W212 is **to warn road users in a one-way roadway that the roadway ahead carries traffic in both directions.**
- Sign W212 should be displayed where a one-way roadway becomes a two-way roadway either at the end of a dual roadway or beyond a junction. The sign may also be used in similar circumstances where it is, for some reason, unclear to drivers that two-way traffic exists on a roadway.
- The sign should be located at a distance in advance of the point where the actual two-way traffic is achieved at normal roadway width in accordance with the provisions of Table 3.1 or Figure 3.1.
- It is recommended that the sign be displayed on both sides of the one-way roadway when the median island permits and that it be used in conjunction with END OF DUAL ROADWAY warning signs W116 and W117 (see Subsection 3.2.7).
- Temporary warning sign TW212 may be used under the same circumstances as the permanent TWO-WAY TRAFFIC warning sign. Detours at roadworks sites commonly result in the temporary use of a one way roadway to carry two-way traffic. The use of HIGH VISIBILITY warning signs or diagrammatic signs is highly recommended in such situations (see Sections 3.6 and 4.10).

COLOURS:

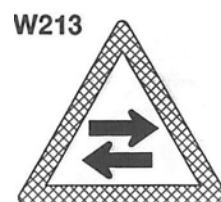
PERMANENT

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 Symbol: Black semi-matt
 Background: White retroreflective

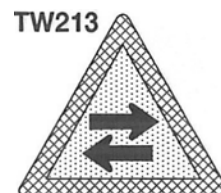
TEMPORARY

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

TWO-WAY TRAFFIC CROSSROAD



For dimensions
 ref. Vol. 4
 page
 3.3.13



3.3.8 Two-way Traffic Crossroad

- 1 The TWO-WAY TRAFFIC CROSSROAD warning sign W213 is **to warn road users moving in a one-way street network that the next crossroad carries two-way traffic.**
- 2 Sign W213 should only be displayed in urban areas at the limits of urban one-way street systems when a subsequent cross-street is operating under two-way traffic conditions. It may commonly be required to be displayed in a one-way street which intersects a two-way street and

in such circumstances should be displayed on both sides of the one-way roadway.

- 3 The sign should be located at least 60 m in advance of the crossroad and preferably up to half a city block if this exceeds 60 m.
- 4 Temporary warning sign TW213 may be used on an intersecting side road if the roadway ahead, normally a one-way roadway, is temporarily operating as a two-way roadway as part of a roadworks detour.

LANE ENDS

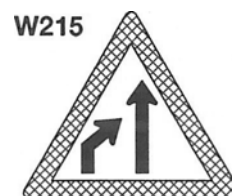
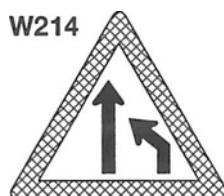
COLOURS:

PERMANENT

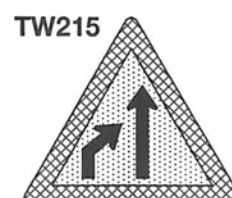
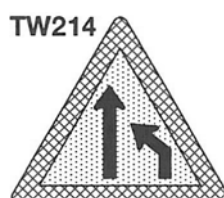
Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective



For dimensions
 ref. Vol. 4
 page
 3.3.14
 3.3.15



3.3.9 Lane Ends

- 1 The LANE ENDS warning signs W214 and W215 are **to warn road users that in the direction in which they are moving the roadway ahead is reduced in width by a full lane from the right side or from the left side.**
- 2 Signs W214 and W215 may be displayed on sections of minor Class "B", Class "C" or Class "D" roadways where a lane is ended. These signs shall NOT be displayed to indicate a reduction in width of roadway other than by a full lane width. If the roadway is reduced in width by less than a lane width and the number of lanes marked is not reduced ROAD NARROWS FROM BOTH SIDES warning sign W328 or ROAD NARROWS FROM ONE SIDE ONLY warning signs W329 or W330 should be used as appropriate.
- 3 When a lane is ended on a freeway or major Class "B" road the use of the appropriate diagrammatic signs as

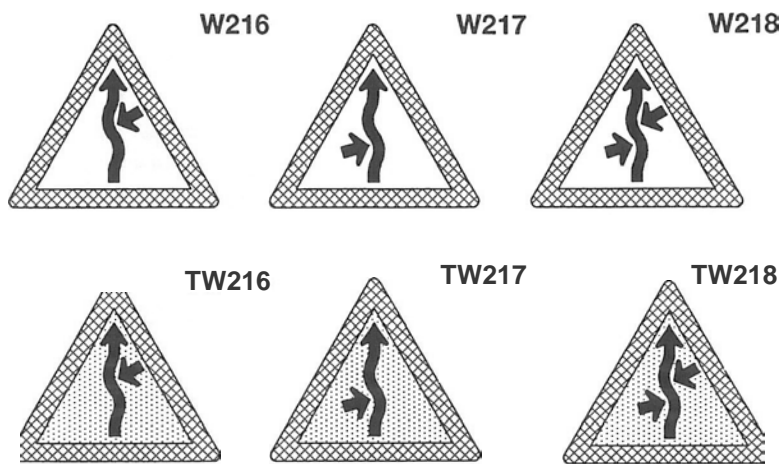
detailed in Section 4.10 is highly recommended in preference to signs W214 or W215.

- 4 Signs W214 and W215 should be located in advance of the start of the lane ends taper in accordance with the provisions of Table 3.1 or Figure 3.1.
- 5 Temporary warning signs TW214 and TW215 may be used under similar circumstances to the permanent LANE ENDS warning signs at roadworks and other temporary traffic management operations such as roadblocks or traffic surveys. The use of signs TW214 and TW215 should be limited to sites carrying light traffic and for short time periods. The use of temporary diagrammatic signs as detailed in Section 4.10 is highly recommended for any temporary traffic management operation dealing with moderate or greater traffic flows for periods greater than a few hours.

CONCEALED DRIVEWAY

For dimensions
ref. Vol. 4
pages

3.3.16
3.3.17
3.3.18



COLOURS:

PERMANENT

Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.3.10 Concealed Driveway

- 1 The CONCEALED DRIVEWAY warning signs W216, W217 and W218 are **to warn road users that, due to restricted sight distance vehicles entering the roadway ahead from driveways on the right, or the left, or both sides, may constitute a hazard and that extra caution should be exercised.**
- 2 Signs W216 and W217 should be displayed in advance of a section of a roadway to indicate that one or more concealed driveways exist to the right or left respectively. Sign W218 may be displayed if the concealed driveways occur on both sides of the roadway. It is intended that the symbolic indication of a winding road be typical only. However, if it is required to make the symbol more specific this may be achieved by modifying the main direction of movement arrow e.g. if the condition creating a concealed entrance is a crest vertical curve on an otherwise straight road a straight direction of movement arrow may be used.
- 3 The reason for concealment will normally be a combination of horizontal or vertical curvature and/or encroaching vegetation.
- 4 The signs should be located in advance of the section of roadway on which the hazard may occur in accordance with the operating speed of the road. They should be located as indicated in Table 3.1 or in Figure 3.1.
- 5 From experience of the particular roadway it may be of advantage to add a supplementary advisory speed or distance plate below the sign on the same post. (See Section 3.6)
- 6 Temporary warning signs TW216, TW217 and TW218 may be used under the same circumstances as permanent CONCEALED DRIVEWAY warning signs when temporary detours at roadworks sites result in conditions of reduced visibility of access points to the road-way.

TRAFFIC SIGNALS AHEAD

COLOURS:

PERMANENT

Triangle: Red retroreflective
 Symbol: Red, yellow and green retroreflective on black semi-matt

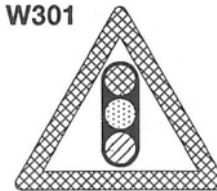
Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective
 Symbol: Red, yellow and green retroreflective on black semi-matt

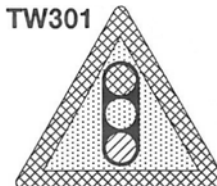
Background: Yellow retroreflective

W301



For dimensions
 ref. Vol. 4
 page
 3.4.1

TW301



3.4.1 Traffic Signals Ahead

- The TRAFFIC SIGNALS AHEAD warning sign W301 is **to warn road users of the presence of a traffic control signal ahead.**
- Sign W301 should be displayed in advance of:
 - any isolated or new traffic control signal installation;
 - any junction controlled by traffic signals where approach speeds are 70 km/h or more, or where the signal is not visible within 180 m of the junction;
 - an isolated or midblock pedestrian crossing controlled by traffic signals.
- Subject to the other requirements in (3.4.1.2) above, a TRAFFIC SIGNALS AHEAD warning sign, which has been displayed in advance of a new traffic signal installation, may be removed after a period of three months.
- These signs should be located in advance of a traffic signal in accordance with the design speed of the road. They should be located as indicated in Table 3.1 or Figure 3.1.
- Temporary warning sign TW301 should be used in advance of any temporary traffic signal.

COLOURS:

PERMANENT

Triangle: Red retroreflective
 Symbol: White & red retroreflective

Background: White retroreflective

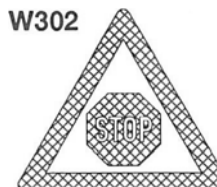
TEMPORARY

Triangle: Red retroreflective
 Symbol: White & red retroreflective

Background: Yellow retroreflective

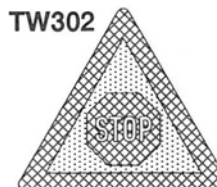
TRAFFIC CONTROL "STOP" AHEAD

W302



For dimensions
 ref. Vol. 4
 page
 3.4.2

TW302

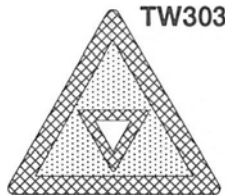
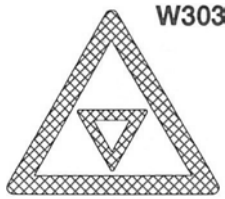


3.4.2 Traffic Control "Stop" Ahead

- The TRAFFIC CONTROL "STOP" AHEAD warning sign W302 is **to warn road users of the presence of a STOP sign R1, ahead.**
- Sign W302 may be displayed in advance of a STOP sign R1, when visibility of the STOP sign is restricted due to road alignment or encroaching vegetation. It may also be displayed for a limited period of time in advance of a new STOP sign R1, until drivers are used to the control.
- These signs should be located in accordance with the operating speed of approaching traffic and the provisions of Table 3.1 or Figure 3.1. The use of a supplementary distance information plate is recommended to advise drivers of the distance to the STOP control, particularly if the STOP sign is not visible from the W302 sign (see Section 3.6).
- Temporary warning sign TW302 may be used under the same circumstances as permanent TRAFFIC CONTROL "STOP" AHEAD warning signs when temporary STOP controls exist at roadworks sites.

TRAFFIC CONTROL "YIELD" AHEAD

For dimensions
Ref. Vol. 4
page
3.4.3



COLOURS:
PERMANENT
Triangle: Red retroreflective
Symbol: White & red retroreflective
Background: White retroreflective
TEMPORARY
Triangle: Red retroreflective
Symbol: White & red retroreflective
Background: Yellow retroreflective

3.4.3 Traffic Control "Yield" Ahead

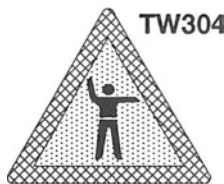
- 1 The TRAFFIC CONTROL "YIELD" AHEAD warning sign W303 is *to warn road users of the presence of a YIELD sign R2, ahead.*
- 2 Sign W303 may be displayed in advance of a YIELD sign R2, when visibility of the YIELD sign is restricted due to road alignment or encroaching vegetation. It may also be displayed for a limited period of time in advance of a new YIELD sign R2, until drivers are used to the control.
- 3 These signs should be located in accordance with the operating speed of approaching traffic and the provisions

of Table 3.1 or Figure 3.1. The use of a supplementary distance information plate is recommended to advise drivers of the distance to the YIELD control, particularly if the YIELD sign is not visible from the W303 sign (see Section 3.6).

- 4 Temporary warning sign TW303 may be used under the same circumstances as permanent TRAFFIC CONTROL "YIELD" AHEAD warning signs when temporary YIELD controls exist at roadworks sites.

TRAFFIC CONTROL AHEAD

For dimensions
ref. Vol. 4
page
3.4.4



COLOURS:
TEMPORARY
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.4.4 Traffic Control Ahead

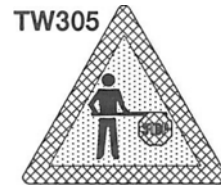
- 1 The TRAFFIC CONTROL AHEAD warning sign TW304 is *to warn road users that there is an officer ahead who is controlling traffic on a temporary basis.*
- 2 The sign should be portable and should be displayed only for the period during which the control is in operation. The reverse side of portable signs shall be marked with alternating black and yellow horizontal stripes 150 mm wide.
- 3 Sign TW304 should, where possible, be located in advance of the officer exercising traffic control in accordance with the operating speed of approaching traffic and the provisions of Table 3.1 and Figure 3.1.

- 4 In the event that it is not possible for the officer exercising traffic control to transport the temporary sign on a daily basis, consideration may be given to installing a sign permanently. In this case the sign should be manufactured in such a way that the message can be hidden during the times of day when the sign is not applicable. This can be achieved by using an extra plate, hinged on the vertical centreline of the sign, and of half sign size, which can be turned to hide the message and show a blank face. The blank sign should display a semi-matt grey colour.

SCHOLAR PATROL AHEAD

COLOURS:
 TEMPORARY
 Triangle: Red retroreflective
 Symbols: Black semi-matt & White & red retroreflective
 Background: Yellow retroreflective

For dimensions
 ref. Vol. 4
 page
 3.4.5



3.4.5 Scholar Patrol Ahead

- 1 The SCHOLAR PATROL AHEAD warning sign shall only be used as a temporary warning sign TW305 and is **to warn road users that a temporary scholar patrol is operating ahead**. The scholar patrol crossing shall be properly marked in accordance with provisions of Chapter 7.
- 2 Sign TW305 should be a temporary portable sign and should be displayed only for the period during which the scholar patrol is in operation. The reverse side of portable signs shall be marked with alternating black and yellow horizontal stripes 150 mm wide.
- 3 These signs should be displayed in the centre of a two-way roadway or on the left side of the median island of a dual roadway and should be of the size given in

Table 3.1. It is recommended that when the signs are mounted in a sunken socket in the roadway that where possible this and the sign post be of a square section to prevent the swivelling of the sign due to wind.

- 4 Pedestrian crossings subject to part-time control by scholar patrols should be preceded by permanent PEDESTRIAN CROSSING warning signs W306 and CHILDREN warning signs W308 (see Subsections 3.4.6 and 3.4.8).
- 5 These signs should where possible be located at least 60 m ahead of the crossing. In rural areas Table 3.1 is applicable.

PEDESTRIAN CROSSING



For dimensions
 ref. Vol. 4
 page
 3.4.6



COLOURS:
 PERMANENT
 Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective
 TEMPORARY
 Triangle: Red retroreflective
 Symbols: Black semi-matt
 Background: Yellow retroreflective

3.4.6 Pedestrian Crossing

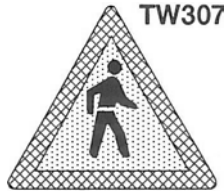
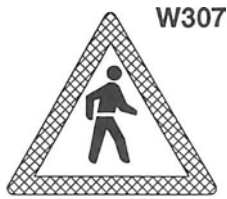
- 1 The PEDESTRIAN CROSSING warning sign W306 is **to warn road users of a marked pedestrian crossing ahead**.
- 2 Sign W306 should, where possible, be displayed not less than 90 m or more than 180 m in advance of any block-marked pedestrian crossing. In addition, if the block-marked crossing is primarily for school children a CHILDREN warning sign W308, should be placed a

suitable distance in advance of sign W306. A pedestrian crossing controlled by a traffic signal should be preceded by a TRAFFIC SIGNALS AHEAD warning sign W301, as detailed in Subsection 3.4.1

- 3 A temporary warning sign TW306 should be used if a temporary pedestrian crossing is installed as part of a roadworks detour.

PEDESTRIANS

For dimensions
ref. Vol. 4
page
3.4.7



COLOURS:

PERMANENT

Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.4.7 Pedestrians

- 1 The PEDESTRIANS warning sign W307 is **to warn road users of the possible presence of above average numbers of pedestrians ahead.**
- 2 Sign W307 is intended for use where a formal pedestrian crossing point has not been marked in accordance with the provisions of Chapter 7. It should normally be reserved for areas or sections of road where pedestrian activities are significantly higher than normal. This applies particularly in rural areas. The use of a supplementary advisory speed plate, may be considered for use with PEDESTRIANS warning signs. In addition if the section of road on which pedestrian activities are significantly higher than normal exceeds 2 km the sign should be repeated at suitable intervals, not greater than 2 km apart. When used, a supplementary plate should be mounted below the warning sign on the same post (see Section 3.6).
- 3 Since sign W307 commonly refers to sections of road the location of the sign should be sited to result in the best possible visibility of the sign consistent with the provisions of Figure 3.1.
- 4 Temporary warning sign TW307 may be used under the same circumstances as permanent PEDESTRIANS warning signs if roadworks detours cross well used pedestrian routes.

CHILDREN

For dimensions
ref. Vol. 4
page
3.4.8



COLOURS:

PERMANENT

Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

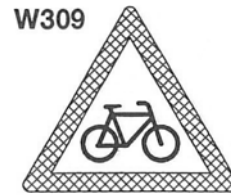
3.4.8 Children

- 1 The CHILDREN warning sign W308 is **to warn road users of the possible presence of children near schools, playgrounds, sports fields or other places ahead.**
- 2 Sign W308 should, where possible, be displayed not less than 90 m, nor more than 180 m in advance of a point or area where children may be expected. The CHILDREN warning sign may be appropriate some distance from a school particularly if a system of "safe routes" to the school has been established for children. If there is a combination of pedestrian and cyclist children present the CHILDREN warning sign W308 should be used in preference to the CYCLISTS warning sign W309 (see Subsection 3.4.9).
- 3 Since many school activities continue into late afternoon or evening the use of fully retroreflective signs is recommended.
- 4 A temporary version of sign W308 is unlikely to be used, but if required should be numbered and referred to as TW308.

CYCLES

COLOURS:
 PERMANENT
 Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective
 TEMPORARY
 Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

For dimensions
 Ref. Vol. 4
 page
 3.4.9



3.4.9 Cycles

- 1 The CYCLES warning sign W309 is **to warn road users of the possible presence of cycles ahead.**
- 2 Sign W309 should preferably be displayed on sections of roadway where a significant volume of cyclists cross or travel along the roadway and no separate provision such as a cycle lane or track has been made. When a separate cycle facility is provided the use of sign W309 should normally be reserved for places where such facilities cross roadways. If the section is longer than 2 km the sign should be repeated at suitable intervals. A supplementary distance information plate may be displayed below sign.

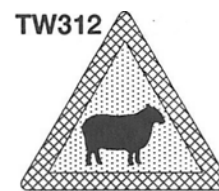
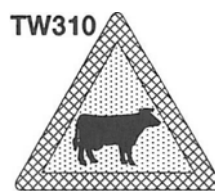
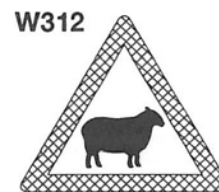
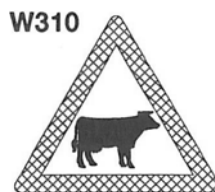
- 3 It is generally not necessary to use the sign near schools to supplement the CHILDREN warning sign W308. However, if separate cycle facilities exist near a school, sign W309 may be the more appropriate warning sign at places where such facilities cross roadways, particularly if there are few children other than cyclists present.
- 4 A temporary warning sign TW309 may be specified if a roadworks detour is located adjacent to a previously separate cycle facility.

W309 and mounted on the same post (see Section 3.6).

FARM ANIMALS

COLOURS:
 PERMANENT
 Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective
 TEMPORARY
 Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

For dimensions
 ref. Vol. 4
 pages
 3.4.10
 3.4.11
 3.4.12



3.4.10 Farm Animals

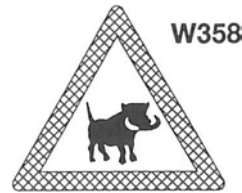
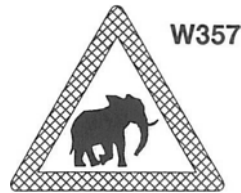
- 1 The FARM ANIMALS warning signs W310 (CATTLE), W311 (HORSES) and W312 (SHEEP) are **to warn road users of the possible presence of cattle, horses or sheep either crossing or straying in the roadway ahead.**
- 2 Signs W310, W311 or W312 should be displayed where straying live-stock may create a hazard.
- 3 The signs should be located in advance of such a section in accordance with the provisions of Figure 3.1. If the section is more than 2 km in length the distance should be displayed on a supplementary distance information plate, mounted on the same post below the main sign and the sign should be repeated at suitable intervals (see Section 3.6).

- 4 Temporary warning signs TW310 (CATTLE), TW311 (HORSES) or TW312 (SHEEP) should be displayed in advance of regular live-stock crossings of the roadway. These signs should be portable and should be placed in position by the person tending the live-stock. They should be removed when the crossing activities are complete.
- 5 The portable signs should be located on each approach to the crossing point to be used, each at a distance as given by Table 3.1 or Figure 3.1 subject to a minimum of 150m.
- 6 The reverse side of portable signs shall be marked with black and yellow 150 mm horizontal stripes.

WILD ANIMALS

For dimensions
ref. Vol. 4
pages

3.4.13
3.4.57
3.4.58
3.4.59



COLOURS:

PERMANENT

Triangle: Red retroreflective

Symbol: Black semi-matt

Background: White retroreflective



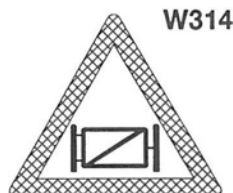
3.4.11 Wild Animals Ahead

- 1 WILD ANIMALS AHEAD warning signs W313 (GENERAL), W357 (ELEPHANT), W358 (WARTHOG), and W359 (HIPPO) are **to warn road users of the possible presence of wild animals, or specifically elephant, warthogs or hippo straying in the roadway ahead.**
- 2 Signs W313, W357, W358 and W359 should be displayed in advance of sections of roadway where straying wild animals create a hazard. If the hazard relates to wild animals in general, sign W313 should be used. If, however, a specific breed of animal is a problem the use of the appropriate sign W357, W358 or W359 is recommended.
- 3 The sign should be located in accordance with the provisions of Figure 3.1. If the section is more than 2 kilometres in length the distance should be displayed on a supplementary distance information plate, mounted on the same post below the main sign and the sign should be repeated at suitable intervals (see Section 3.6).
- 4 Temporary versions of signs W313, W357, W358 and W359 are unlikely to be used, but if required should be numbered and referred to as TW313, TW357, TW358 and TW359 respectively.

GATE

For dimensions
ref. Vol. 4
page

3.4.14



COLOURS:

PERMANENT

Triangle: Red retroreflective

Symbol: Black semi-matt

Background: White retroreflective

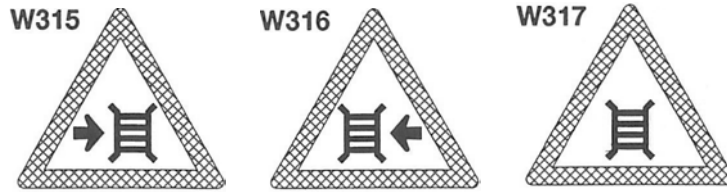
3.4.12 Gate

- 1 The GATE warning sign W314 is **to warn road users that the roadway ahead may be closed by a gate, or that a railway crossing with gates, booms or barriers is ahead.**
- 2 Sign W314 may be used in conjunction with a RAILWAY CROSSING warning sign W318 to warn drivers of a railway crossing with gates, booms or barriers (See Subsection 3.4.14).
- 3 The sign should be displayed in advance of the hazard at a distance in accordance with the design speed of the road. These signs should be located and sized as indicated in Table 3.1 or Figure 3.1.
- 4 A temporary version of sign W314 is unlikely to be used, but if required should be numbered and referred to as TW314.

MOTOR GATE

COLOURS:
 PERMANENT
 Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

For dimensions
 Ref. Vol. 4
 page
 3.4.15
 3.4.16
 3.4.17



3.4.13 Motor Gate

- 1 The MOTOR GATE warning signs W315, W316 and W317 are **to warn road users that a motor gate is ahead on the right side, the left side or in the centre of the roadway.**
- 2 Signs W315 and W316 should be displayed to warn drivers that a motor gate is situated ahead on the right side or left side of the roadway. Sign W317 should be displayed when the motor gate is situated centrally in the roadway. Where the motor gate does not extend over the full width of the roadway consideration should be given to displaying a ONE VEHICLE WIDTH STRUCTURE warning sign W327 instead of, or in

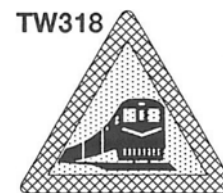
addition to signs W315, W316 or W317. Sign W327 should, when used in addition, be located a distance "V" metres in advance of the MOTOR GATE sign (where "V" is the operating speed in km/h).

- 3 These signs should be located in advance of the hazard and be of a size, and at a distance as given in Table 3.1.
- 4 Temporary versions of signs W315, W316 and W317 are unlikely to be used, but if required should be numbered and referred to as TW315, TW316 and TW317.

RAILWAY CROSSING

COLOURS:
 PERMANENT
 Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective
 TEMPORARY
 Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

For dimensions
 ref. Vol. 4
 page
 3.4.18



3.4.14 Railway Crossing

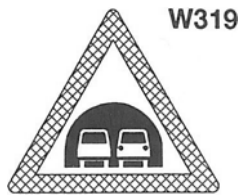
- 1 The RAILWAY CROSSING warning sign W318 is **to warn road users that a railway crossing is ahead.**
- 2 An additional sign W318 may also be displayed on the right-hand side of the roadway for extra emphasis.
- 3 The sign should be located in advance of the hazard, and be of a size, and at a distance as given in Table 3.1.
- 4 At railway crossings with gates, booms or barriers, GATE warning sign W314 should also be displayed.

Sign W314 should be located a distance "V" metres in advance of the RAILWAY CROSSING sign (where "V" is the operating speed in km/h).

- 5 Typical sign and road marking sequences for various classes of railway crossing protection are given in Volume 2, Chapter 7.
- 6 Temporary warning sign TW318 may be used to warn of a temporary railway crossing created as a result of roadworks or railway detours.

TUNNEL

For dimensions
ref. Vol. 4
page
3.4.19



COLOURS:

PERMANENT

Triangle: Red retroreflective

Symbol: Black semi-matt

Background: White retroreflective

3.4.15 Tunnel

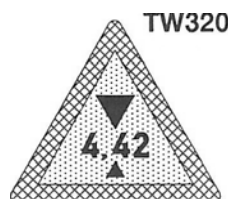
- 1 The TUNNEL warning sign W319 is **to warn road users of the presence of a tunnel on the roadway ahead** in which the use of headlights may be required.
- 2 Sign W319 should also be displayed on the approach to a tunnel that is not visible from a point at least 150 m in advance of the tunnel entrance.
- 3 The sign should be displayed in advance of the

hazard at a distance dependent on the operating speed of traffic. Figure 3.1 should be used to determine the appropriate distance.

- 4 A temporary version of sign W319 is most unlikely to be used, but if required should be numbered and referred to as TW319.

HEIGHT RESTRICTED

For dimensions
ref. Vol. 4
page
3.4.20



COLOURS:

PERMANENT

Triangle: Red retroreflective

Symbol: Black semi-matt

Background: White retroreflective

Legend: Din 1451 Style B

TEMPORARY

Triangle: Red retroreflective

Symbol: Black semi-matt

Background: Yellow retroreflective

Legend: Din 1451 Style B

3.4.16 Height Restricted

- 1 The HEIGHT RESTRICTED warning sign W320 is **to warn road users that the clearance of:**
 - (a) a height gauge located ahead of a railway crossing over which overhead electrical power cables are installed; or
 - (b) an overhead structure; is restricted.
- 2 Sign W320 should be displayed in advance of a height gauge or overhead structure when the clearance over any portion of the roadway is less than 4,7 m. The clearance height shown should be the same as shown on the HEIGHT LIMIT sign R204, which sign shall be displayed on the overhead structure flanked by two OVER HEAD

DANGER PLATES signs W415. A typical sign arrangement for a height restricted site is given in Volume 2.

- 3 Sign W320 should be located in advance of the height restriction in accordance with the design speed of the road. The sign shall be of a size as indicated in Table 3.1. Sign W320 may, however, be located some distance from the hazard and in such circumstances should be supplemented by a distance information plate, giving the distance to the hazard (see Section 3.6).
- 4 Temporary sign TW320 may be used under the same circumstances as permanent HEIGHT RESTRICTED warning signs if construction or maintenance work requires that the normal height restriction at a structure be temporarily reduced.

LENGTH RESTRICTED

COLOURS:

PERMANENT

Triangle: Red retroreflective

Symbol: Black semi-matt

Background: White retroreflective

Legend Din 1451 Style B TEMPORARY

TEMPORARY

Triangle: Red retroreflective

Symbol: Black semi-matt

Background: Yellow retroreflective

Legend Din 1451 Style B

W321



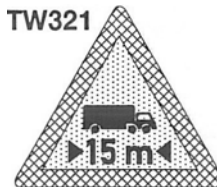
For dimensions

Ref. Vol. 4

page

3.4.21

TW321

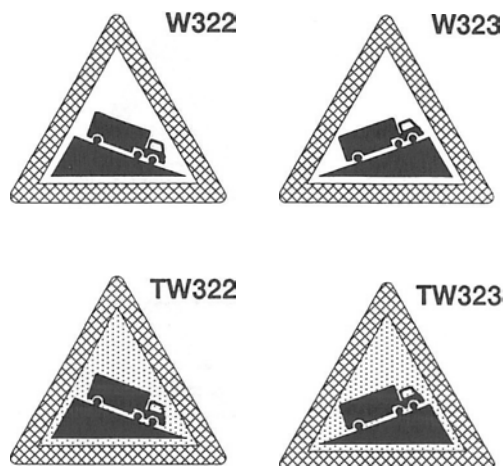


3.4.17 Length Restricted

- 1 The LENGTH RESTRICTED warning sign W321 is **to warn road users that the permissible length of vehicles is restricted** because the lateral clearances to bridge parapets, retaining walls, road traffic signs or other road furniture is limited due to road curvature or some other design feature of construction.
- 2 Sign W321 should be displayed in advance of a section of roadway where the road alignment and cross-section is such that long vehicles will overhang the inner edge of the roadway on left-hand curves and the dividing line on right curves. The restricted vehicle length shown should be the same as shown on the LENGTH LIMIT sign R205, which sign shall be displayed in advance of the restricted section of roadway.
- 3 Sign W321 shall be located in advance of the start of the length restricted section of roadway in accordance with the design speed of the road. The sign shall be of a size as indicated in Table 3.1. Sign W321 may, however, be located some distance from the hazard and in such circumstances should be supplemented by a distance information plate giving the distance to the hazard (see Section 3.6).
- 4 Temporary sign TW321 may be used under the same circumstances as permanent LENGTH RESTRICTED warning signs if a roadworks detour contains sections of roadway with the characteristics given in (3.4.17.2) above.

STEEP DESCENT AND STEEP ASCENT

For dimensions
ref. Vol. 4
pages
3.4.22
3.4.23



COLOURS:
PERMANENT
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective
TEMPORARY
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.4.18 Steep Descent and Steep Ascent

- The STEEP DESCENT warning sign W322 is **to warn road users of a steep downhill section of roadway ahead which may, particularly for heavy vehicles, constitute a hazard**; and the STEEP ASCENT warning sign W323 is **to warn road users of a steep uphill section of roadway ahead**.
- Sign W322 should be displayed in advance of a steep downhill section of roadway with a grade of 5 per cent or more and a length greater than the distances given in Table 3.3.
- The use of sign W323 for a gradient of less than 5% may be considered if the gradient continues for a distance of over 2 km. Such combinations of length and degree of grade may constitute a potential hazard to heavy vehicles.
- Sign W323 should be located in advance of the start of the downhill grade in accordance with the provisions of Table 3.1 or Figure 3.1. In the case of conditions as described in (3.4.18.3) the sign should be located further from the start of the downhill grade to allow provision of diagrammatic signs such as "ENGAGE LOW GEAR" sign GS505 (see Section 4.10). The signs should be "V" metres apart (where "V" is the operating speed of normal traffic excluding heavy vehicles in km/h).
- Sign W324 may be displayed in advance of an uphill grade where the nature of the road alignment is such that the steep uphill grade is not obvious to approaching traffic.
- Sign W324 should be located in accordance with the provisions of Table 3.1 or Figure 3.1.
- It is recommended that SUPPLEMENTARY PLATE information sign IN11 be used with signs W323 and W324 to indicate the length and/or steepness of a gradient, particularly when the gradient is regularly used by heavy vehicles. SUPPLEMENTARY PLATE sign IN112, showing the length of the gradient in the form "For 8 km", or IN11.4, showing the slope of the gradient in the form "1:12", are most appropriate. Under certain conditions both messages may be combined in one SUPPLEMENTARY PLATE sign. (See Volume 4, Chapter 9 for dimensional details).
- Temporary warning signs TW323 and TW324 may be used under the same circumstances as the permanent STEEP DESCENT and STEEP ASCENT warning signs when steep downhill or uphill sections of roadway occur on roadworks detours.

TABLE 3.3 CRITICAL LENGTHS OF STEEP DESCENT TABLE 3.3

Grade	Minimum length (m)
5% (1)(2)	1000
7%	300
8%	250
10%	150
Steeper	90

NOTES:

(1) Grades of this order need not normally be signed unless there are significant numbers of heavy vehicles using the section of roadway.

(2) The minimum length of grade given presumes a curving alignment which will add to the potential hazard of such grades for heavy vehicles.

SLOW MOVING HEAVY VEHICLE

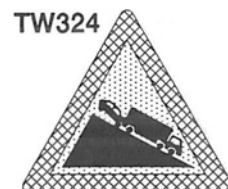
COLOURS:

PERMANENT

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective



For dimensions
 ref. Vol. 4
 page
 3.4.24

3.4.19 Slow Moving Heavy Goods Vehicles

- 1 The SLOW MOVING HEAVY GOODS VEHICLES warning sign W324 is **to warn road users that due to the steep downhill section of roadway ahead heavy vehicles may be travelling very slowly in a low gear**, which, combined with reduced visibility conditions due to road curvature or weather, constitutes a potential hazard.
- 2 Sign W324 should be displayed in advance of a downhill section of roadway on which significant numbers of slow-moving heavy vehicles are present and visibility is, or may be, limited, for whatever reason. The sign may therefore be used in addition to, or in place of, a STEEP DESCENT sign W322.
- 3 The sign should be located in advance of the downhill section in accordance with the provisions of Table 3.1 or Figure 3.1 if mounted on the same post as a W322 sign. If located separately from sign W322 then sign W324 should be located a distance "V" metres in advance of sign W322 (where "V" is the operating speed in km/h).
- 4 Temporary warning sign TW324 may be used under the same circumstances as the permanent SLOW MOVING HEAVY VEHICLES warning sign when similar conditions occur on a roadworks detour.

GRAVEL ROAD BEGINS/ENDS

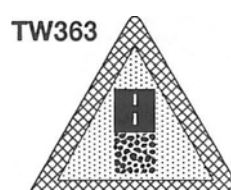
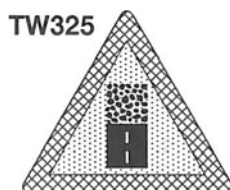
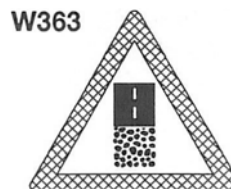
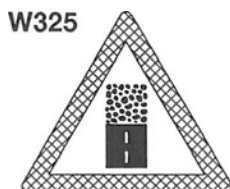
COLOURS:

PERMANENT

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective



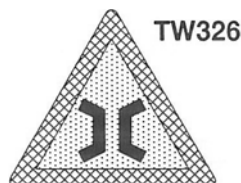
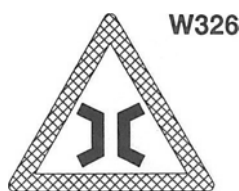
For dimensions
 ref. Vol. 4
 pages
 3.4.25
 3.4.63

3.4.20 Gravel Road Begins/Ends

- 1 The GRAVEL ROAD BEGINS/ENDS warning signs W325 and W363 **to warn road users that the road surface on which they are travelling is about to change from asphalt or concrete to gravel, or vice versa, and that the point of change in surface, and the gravel road surface, may require a reduction in speed.**
- 2 Signs W325 and W363 should be displayed in advance of the change in road surface. The point of change in surface commonly deteriorates rapidly to the extent that it may become a hazard. This condition depends on levels of maintenance but, although the condition is not constant, the use of the signs is recommended to warn drivers to exercise caution.
- 3 The signs should be located at a distance from the start or end of the gravel road as indicated by Figure 3.1.
- 4 Temporary warning sign TW325 and TW363 may be used under the same circumstances as the permanent warning signs when temporary changes in road surface occur at roadworks sites.

NARROW BRIDGE

For dimensions
ref. Vol. 4
page
3.4.26



COLOURS:

PERMANENT

Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

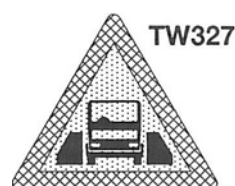
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.4.21 Narrow Bridge

- 1 The NARROW BRIDGE warning sign W326 is **to warn road users that the width of roadway is reduced over a short distance at a bridge on the roadway ahead.**
- 2 Sign W326 should be displayed in advance of bridge structures when the clear width over or under the bridge is more than 1m narrower than the clear width of the approaching roadway.
- 3 The sign should be located in advance of the narrow bridge at a distance in accordance with the provisions of Figure 3.1.
- 4 Temporary warning sign TW326 may be used under the same circumstances as a permanent NARROW BRIDGE warning sign during road construction.

ONE VEHICLE WIDTH STRUCTURE

For dimensions
ref. Vol. 4
page
3.4.27



COLOURS:

PERMANENT

Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.4.22 One Vehicle Width Structure

- 1 The ONE VEHICLE WIDTH STRUCTURE warning sign W327 is **to warn road users that the width of the structure on the roadway ahead is less than 5m and that traffic shall stop at the entrance thereto if a vehicle approaching from the opposite direction is already on the structure, or so close thereto as to constitute a danger.**
- 2 Sign W327 should not be displayed in advance of a narrow structure which is wide enough to permit two vehicles to pass. If this structure is a bridge the NARROW BRIDGE warning sign W326 should be displayed. The ROAD NARROWS FROM BOTH SIDES warning sign W328 should be used if no structure is involved. A structure in this sense could include a gate or motorgate.
- 3 The sign should be located in advance of the hazard, and be of a size, and at a distance as given in Table 3.1.
- 4 Temporary warning sign TW327 may be required at construction sites where temporary or partly constructed structures are in use to carry detour traffic.

ROAD NARROWS FROM BOTH SIDES

COLOURS:

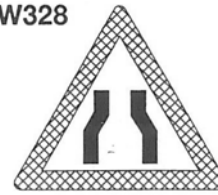
PERMANENT

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

TEMPORARY

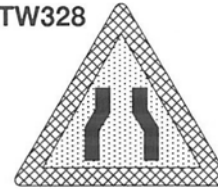
Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

W328



For dimensions
 ref. Vol. 4
 page
 3.4.28

TW328



3.4.23 Road Narrows From Both Sides

- 1 The ROAD NARROWS FROM BOTH SIDES warning sign W328 is **to warn road users that the roadway ahead narrows from both sides.**
- 2 Sign W328 should be displayed where the width of the roadway is abruptly reduced and continues at a reduced width for some distance. The sign need not be displayed on minor low volume roads with a width of more than 5 m after narrowing.
- 3 The sign should not be displayed for localised narrowing at a bridge. The NARROW BRIDGE warning sign W326 should rather be displayed in preference.
- 4 Sign W328 should be displayed in advance of all sections of roadway with a width of less than 5 m. The sign

should be located in advance of the point where the narrowing begins in accordance with the provisions of Table 3.1 or Figure 3.1.

- 5 Temporary warning sign TW328 may be used within roadworks detours.
- 6 Signs W328 and TW328 shall not be used to indicate a road narrowing by a full lane width in one, or both, directions. Such a situation, when signed with a warning sign, shall be signed using a LANE ENDS warning sign W214, W215, TW214 or TW215 as appropriate or an appropriate diagrammatic sign (see Section 4.10 and Subsection 3.3.9).

ROAD NARROWS FROM ONE SIDE ONLY

COLOURS:

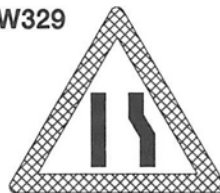
PERMANENT

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

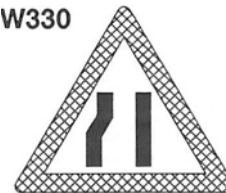
TEMPORARY

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

W329

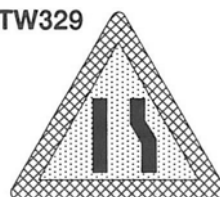


W330

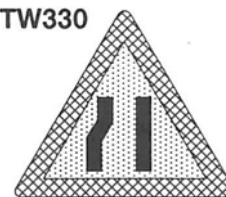


For dimensions
 ref. Vol. 4
 page
 3.4.29
 3.4.30

TW329



TW330



3.4.24 Road Narrows From One Side Only

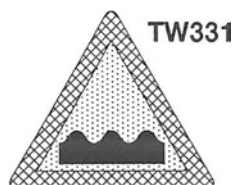
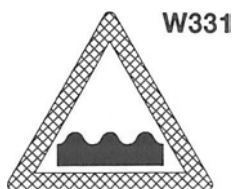
- 1 The ROAD NARROWS FROM ONE SIDE ONLY warning signs W329 and W330 are **to warn road users that the roadway ahead narrows from the right or left side only.**
- 2 Signs W329 and W330 should be displayed where the width of the roadway is abruptly reduced from the right side or the left side respectively, and continues at a reduced width for some distance. The sign need not be displayed on minor low volume roads with a width of more than 5m after narrowing.
- 3 The signs should be displayed in advance of all sections of roadway with a width of less than 5 m. The signs should be located in advance of the point where the

narrowing begins in accordance with the provisions of Table 3.1 or Figure 3.1.

- 4 Temporary warning signs 1W329 and 1W330 may be used within roadworks detours.
- 5 Signs W329, W330, 1W329 and 1W330 shall not be used to indicate a road narrowing by a full lane width. Such a situation, when signed with a warning sign, shall be signed using a LANE ENDS warning sign W214, W215, TW214 or 1W215 as appropriate or an appropriate diagrammatic sign (see Section 4.10 and Subsection 3.3.9).

UNEVEN ROADWAY

For dimensions
ref. Vol. 4
page
3.4.31



COLOURS:
PERMANENT
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective
TEMPORARY
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.4.25 Uneven Roadway

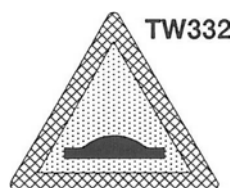
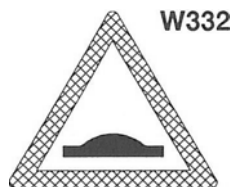
- 1 The UNEVEN ROADWAY warning sign W331 is **to warn road users that there is a depression or ridge in the roadway or that the road surface is generally uneven or potholed.**
- 2 Sign W331 should be displayed in advance of a section of uneven or potholed roadway which is hazardous and requires a reduction in speed. Such a condition would normally indicate the start of the failure of the road. This sign should not be used to warn motorists of speed humps in the roadway.
- 3 This sign should be located in advance of the hazard at a distance dependent on the operating speed of

approaching traffic. Figure 3.1 should be used to determine the appropriate distance.

- 4 A temporary warning sign TW331 should be used for an uneven or potholed roadway. The sign may be supplemented by an advisory speed plate, or a distance plate and/or repeated at suitable intervals (see Section 3.6).
- 5 GENERAL WARNING sign TW339 with a supplementary information plate with the text "Potholes" may be used as a short term alternative (see Subsection 3.4.32 on page 3.4.33).

SPEED HUMPS

For dimensions
ref. Vol. 4
page
3.4.32



COLOURS:
PERMANENT
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective
TEMPORARY
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.4.26 Speed Humps

- 1 The SPEED HUMPS warning sign W332 is **to warn road users of speed humps on the roadway ahead** which require a reduction in speed.
- 2 Sign W332 should be displayed when speed control humps have been installed to reduce traffic speed in various environments. UNEVEN ROADWAY warning sign W331 should not be used to warn traffic of speed humps.
- 3 The sign should be located in advance of the hazard at a distance dependent on the operating speed or, in the case of a speed hump immediately after a turn, the

average speed at which the hazard can be negotiated safely. If a number of speed humps are installed the sign should preferably be located within 30m of the first hump which should be placed within 50 m of the start of a section of roadway so that drivers encounter the hump at low speed. The sign should preferably be supplemented by an appropriate information plate indicating a "distance for", a recommended speed or some general message.

- 4 Temporary warning sign TW332 may be used when speed humps are used to reduce speeds at roadworks sites.

SLIPPERY ROAD

COLOURS:

PERMANENT

Triangle: Red retroreflective

Symbols: Black semi-matt

Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective

Symbols: Black semi-matt

Background: Yellow retroreflective

W333



For dimensions
Ref. Vol. 4
pages
3.4.33

TW333



3.4.27 Slippery Road

- 1 The SLIPPERY ROAD warning sign W333 is **to warn road users of abnormally slippery conditions on the roadway ahead** for which a considerable reduction in speed is necessary.
- 2 Sign W333 should be located in advance of the beginning of the section of slippery roadway in accordance with the provisions of Table 3.1 or Figure 3.1. The signs should be repeated at intervals of about 2 km, where necessary. Alternatively a supplementary distance information plate may be displayed on the same post below sign W333.
- 3 When a section of slippery road is unlikely to be re-paired for some time the use of a permanent sign is in order. In such instances, if the slippery condition is occasional and occurs during wet weather, sign W333 may be supplemented by a plate with the text "When wet".
- 4 Temporary warning sign TW333 should be used if slippery road conditions occur at roadworks sites or if the affected roadway is due to be repaired within a short time period.

COLOURS:

PERMANENT

Triangle: Red retroreflective

Symbol: Black semi-matt

Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective

Symbols: Black semi-matt

Background: Yellow retroreflective

FALLING ROCKS

W334

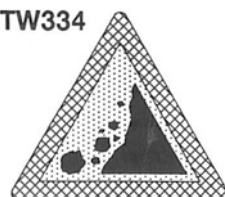


W335

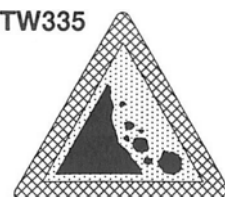


For dimensions
Ref. Vol. 4
pages
3.4.34
3.4.35

TW334



TW335



3.4.28 Falling Rocks

- 1 The FALLING ROCKS warning signs W334 and W335 are **to warn road users of the possibility of falling rocks or stones on the right or left of the roadway ahead.**
- 2 Signs W334 and W335 should be displayed in advance of sections of roadway in loose rock cuttings where broken rock may be lying on the road surface. If the section is long the sign should be repeated at regular intervals of 2km or the affected distance may be displayed on a SUPPLEMENTARY PLATE sign IN11.2, mounted below signs W334 or W335 and on the same post for some time until the rock slope is deemed to have stabilized.
- 3 Temporary warning sign TW334 and TW335 may be required at roadworks sites where freshly opened cuttings are relatively common. The signs may be retained.

ROADWORKS

For dimensions
ref. Vol. 4
page
3.4.36

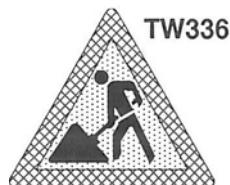
COLOURS:

TEMPORARY

Triangle: Red retroreflective

Symbol: Black semi-matt

Background: Yellow retroreflective



3.4.29 Roadworks

- 1 The ROADWORKS temporary warning sign TW336 is **to warn road users that temporary road construction, maintenance or related work is in progress ahead.**
- 2 It should be noted that the function of this sign has been broadened from that of "Road Workmen" to the more general application of ROADWORKS.
- 3 Sign TW336 should be displayed in advance of a roadworks site, however insignificant. For increased visual impact, sign TW336 may be repeated on the approach to the roadworks. In the case of a dual carriageway roadway the signs may also be repeated on the right-hand side of the roadway if the median width permits. When used at minor works in a portable form the reverse side of the sign should be marked with alternating black and yellow horizontal stripes 150mm wide. These may be retroreflective for improved visibility.
- 4 Sign TW336 may be used on a HIGH VISIBILITY background as an advance warning sign at major roadworks sites (see Section 3.6).
- 5 Supplementary distance information plates are recommended to indicate :
 - (a) the distance to the hazard;
 - (b) the extent of the work site or detour.
- 6 TW336 signs should be located in accordance with the provisions of Figure 3.1.
- 7 The use of sign TW336 should be standardised. Typical layouts for signing at roadworks are given in Volume 2. Special care should be taken to differentiate between when work is in progress and when no work is going on but roadway conditions are restricted in some way. Correct procedure in this regard will improve public perception of roadworks signing practices.
- 8 A permanent version of the ROADWORKS warning sign should not be used.

GRADER WORKING

COLOURS:

TEMPORARY

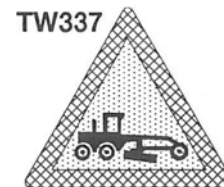
Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

For dimensions

ref. Vo14

page

3.4.37



3.4.30 Grader Working

- 1 The GRADER WORKING temporary warning sign TW337 is **to warn road users that a grader is working on the roadway ahead either on its own or as part of a general road construction or maintenance site and that it may constitute a hazard.**
- 2 Sign TW337 should be displayed as a portable sign in advance of the section of road being worked by the grader. When a grader is working on its own on a road maintenance programme the sign TW337 should be supplemented with a distance information plate giving the length of the section being worked. The reverse side of the portable sign and distance plate should be marked

with alternating black and yellow horizontal stripes 150 mm wide. These may be retroreflective for improved visibility under poor light conditions (see Section 3.6).

- 3 The grader operator shall move the sign forward with his work so that the distance given is approximately correct for his maximum working distance ahead of the sign.
- 4 The SUPPLEMENTARY PLATE sign IN112 should be fastened to the same portable stand as the GRADER WORKING temporary warning sign.
- 5 A permanent version of the GRADER WORKING sign should not be used.

COLOURS:

TEMPORARY

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

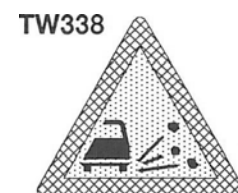
For dimensions

ref. Vo14

page

3.4.38

LOOSE STONES



3.4.31 Loose Stones

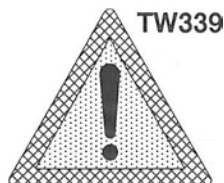
- 1 The LOOSE STONES temporary warning sign TW338 is **to warn road users of the presence of loose stones or gravel on the roadway ahead.**
- 2 Sign TW338 should be displayed in advance of a freshly resealed roadway to minimise the risk of loose chippings or gravel being thrown up which could cause damage to vehicles. The sign should be portable when used during resealing work. The reverse side of the portable sign shall be marked with alternating

black and yellow horizontal stripes 150 mm wide. These may be retroreflective for improved visibility under poor light conditions. A SUPPLEMENTARY PLATE sign IN11.2 may be used to indicate the length of the resealing section. The plate should be mounted on the portable sign frame.

- 3 A permanent version of sign TW338 is unlikely to be used but if required it should be numbered and referred to as W338.

GENERAL WARNING

For dimensions
ref. Vol. 4
page
3.4.39



COLOURS:

PERMANENT

Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

3.4.32 General Warning

- 1 The GENERAL WARNING sign W339 is **to warn road users that there is a hazard of a general or random nature in the roadway ahead**. The most common use of this sign should be in its temporary form as TW339. In keeping with the non-specific nature of the "General Warning" symbol temporary use of the sign is appropriate when unplanned events occur which create a normally short-term hazard.
- 2 Sign TW339 should be displayed in advance of a section of roadway where a random temporary hazard such as fallen trees, subsidence, burst water mains etc., has occurred.
- 3 The sign will commonly be portable so that it can be quickly erected, moved or removed as the nature of the hazard requires.
- 4 The reverse side of a portable sign TW339 shall be marked with alternating black and yellow non-retroreflective horizontal stripes 150 mm wide.
- 5 The sign should be located in advance of the hazard at a distance in accordance with Table 3.1.
- 6 A SUPPLEMENTARY PLATE sign IN11.4 with a relevant text message such as "Ice", "Snow", "Potholes" or "Accident", etc., should be displayed immediately below the sign wherever possible. Maintenance and incident response units should carry a number of the most frequently used messages so that road users will receive a message appropriate to the circumstances prevailing.
- 7 Sign TW339 may be used at roadworks sites or detours if it is considered more appropriate than a ROADWORKS temporary warning sign, TW336.
- 8 At night the sign may be used in conjunction with a yellow flashing light as provided for in Section 3.6.
- 9 The use of permanent GENERAL WARNING sign W339 should be strictly limited, and, when used, it shall be used with a SUPPLEMENTARY PLATE sign IN11 appropriate to the circumstances.

SURFACE STEP

COLOURS:

TEMPORARY

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

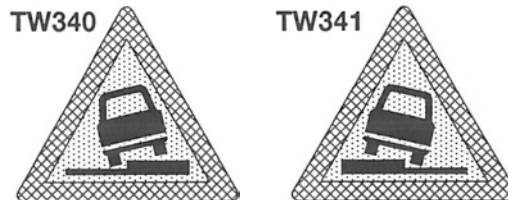
For dimensions

ref. Vol. 4

page

3.4.40

3.4.41



3.4.33 Surface Step

- 1 The SURFACE STEP temporary warning signs IW340 and IW341 are **to warn road users of a hazardous change in the level of the roadway.**
- 2 Signs IW340 and IW341 should be displayed in advance of a section of roadway where there is a significant difference in level along the length of the roadway, usually as a result of resurfacing operations. The step will commonly coincide with the position occupied by a line marking prior to resurfacing. These signs should be located in advance of the section of roadway at a distance in accordance with the provisions of Figure 3.1.
- 3 Sign IW340 should be used when the right-hand side road surface is higher than the left, and sign IW341 when the left-hand side road surface is higher than the right.
- 4 Resurfacing operations tend to occur over some distance. SUPPLEMENTARY PLATE sign IN11.2 are therefore recommended or signs may be repeated at intervals.
- 5 Signs TW340 and TW341 should not be used for temporary steps across the width of the road surface. The UNEVEN ROADWAY temporary warning sign TW331 should be used to warn of such a potential hazard in accordance with the provisions of Subsection 3.4.25.
- 6 Permanent versions of the SURFACE STEP warning signs are unlikely to be used, but if required they should be numbered and referred to as W340 and W341.

SOFT SHOULDER

COLOURS:

TEMPORARY

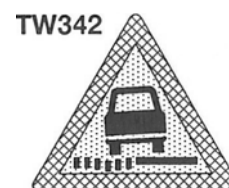
Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

For dimensions

ref. Vol. 4

page

3.4.42



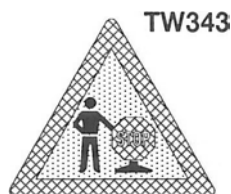
3.4.34 Soft Shoulder

- 1 The SOFT SHOULDER temporary warning sign TW342 is **to warn road users that the material of the shoulder is softer than would be reasonably expected and constitutes a hazard to anyone wishing to pull off the roadway.**
- 2 Sign TW342 should be displayed in advance of sections of roadway where a soft shoulder is present due to incomplete road construction or unexpectedly high surface water or water table conditions. The sign should be located in advance of the section of roadway
- at a distance in accordance with the operating speed of approach traffic and the provisions of the graph in Figure 3.1.
- 3 The use of a SUPPLEMENTARY PLATE sign IN11.2 with sign TW342 is recommended if the condition exists for some distance.
- 4 A permanent version of the SOFT SHOULDER warning sign is most unlikely to be used, but if required it should be numbered and referred to as W342.

"STOP/GO" CONTROL AHEAD

For dimensions
ref. Vol. 4
page
3.4.43

COLOURS:
TEMPORARY
Triangle: Red retroreflective
Symbol: Black semi-matt
red & white
retroreflective
Background: Yellow retroreflective



3.4.35 "Stop-Go" Control Ahead

- 1 The "STOP/GO" CONTROL AHEAD temporary warning sign 1W343 is **to warn road users that traffic ahead is being temporarily controlled by a portable "STOP/GO" sign R1.5A/R1.5B.**
- 2 Sign 1W343 should be used at roadworks in advance of a section of roadway which is subject to control by portable STOP sign R1.5A and GO sign R1.5B. The sign will commonly be used with temporary ROAD NARROWS warning signs TW328, TW329 or TW330. Sign TW343 signs should be located and sized as indicated in Table 3.1 or Figure 3.1.
- 3 When the "STOP/GO" control is not operating, and

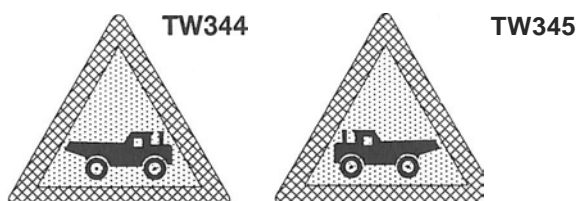
sufficient roadway width for two-way traffic is available, and operation has to continue into the hours of darkness or outside work time, sign TW343 should be removed or suitably covered, e.g. at night time. When one way operation is required outside normal working hours, a 24-hour system of traffic control must be implemented such as temporary traffic signal control. Details of typical sign sequences and operational characteristics of "STOP/GO" control sites are given in Volume 2.

- 4 A permanent version of the "STOP/GO" CONTROL AHEAD warning sign should not be used.

CONSTRUCTION VEHICLES CROSSING

For dimensions
ref. Vol. 4
page
3.4.44
3.4.45

COLOURS:
TEMPORARY
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective



3.4.36 Construction Vehicles Crossing

- 1 The CONSTRUCTION VEHICLES CROSSING temporary warning signs TW344 and TW345 are **to warn road users that temporarily construction vehicles may enter or cross the roadway ahead, from the left and/or right side as appropriate, and that unless care is exercised this may constitute a hazard.**
- 2 Signs TW344 or TW345 should be displayed on the approach to a junction or access where construction vehicles, particularly heavy vehicles, regularly enter or cross the traffic stream. Sign TW344 should be used when construction vehicles represent a particular hazard when entering from the left, or the left and right. If the hazard is related mainly to construction vehicles entering from the right, sign TW345 should be used.
- 3 The signs should be located in advance of the junction in accordance with the design speed of the road according to Table 3.1 or Figure 3.1. If the junction is already signed with another junction warning sign or an advance direction sign care should be taken with the location of signs TW344 or TW345.
- 4 These signs are particularly relevant for use at roadworks or construction sites to warn of the movements of construction vehicles. Signs should be correctly oriented to indicate the actual direction from which construction vehicles are most likely to enter or leave the roadway and should be covered or moved when not applicable.

COLOURS:

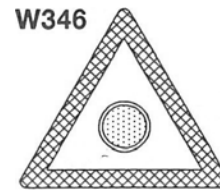
PERMANENT

Triangle: Red retroreflective
 Symbol: Yellow flashing light
 Background: White retroreflective

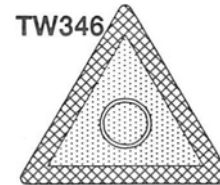
TEMPORARY

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

EMERGENCY FLASHING LIGHT



For dimensions
 ref. Vol. 4
 page
 3.4.46



3.4.37 Emergency Flashing Light

- 1 The EMERGENCY FLASHING LIGHT warning sign W346 is **to warn road users of a hazard ahead by means of an intermittent flashing yellow circular light within the warning sign.**
- 2 Sign W346 may be installed permanently to draw attention to road conditions which are unusually hazardous. The use of sign W346 should be to supplement some other warning sign to give it added emphasis.
- 3 The yellow flashing light should have a periodicity of approximately 1 second. The minimum disc shall be 150 mm in diameter (see Subsection 6.2.9.). The sign shall be of such a design as to render the disc clearly visible to oncoming traffic and to prevent as far as

possible the light from being seen from a direction to which its indication does not apply.

- 4 The sign should be located in advance of the hazard at a distance in accordance with Table 3.1 provided that if another warning sign is used the two signs should be spaced apart a distance "V" metres (where "V" is the operating speed in km/h).
- 5 Temporary warning sign TW346 may be used under the same circumstances. Sign TW346 may be used as a temporary portable sign the reverse side of which should be marked with black and yellow horizontal stripes.

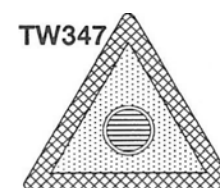
TEMPORARY POLICE FLASHING LIGHT

COLOURS:

TEMPORARY

Triangle: Red retroreflective
 Symbol: Blue flashing light
 Background: Yellow retroreflective

For dimensions
 ref. Vol. 4
 page
 3.4.47



3.4.38 Temporary Police Flashing Light

- 1 The TEMPORARY POLICE FLASHING LIGHT warning sign TW347 is **to warn road users of the presence of a temporary STOP sign R1, ahead, which is being attended by police, or to warn of a collision site or similar occurrence attended by police.**
- 2 Sign TW347 is a temporary portable warning sign incorporating an intermittent flashing blue circular light which should only be displayed in advance of temporary "STOP" or other sites attended by police.
- 3 The blue flashing light should have a periodicity of approximately 1 second. The minimum disc diameter shall be 150 mm (see Subsection 6.2.9.). The sign shall be of such a design as to render the disc clearly visible to oncoming traffic and to prevent as far as possible

the light from being seen from a direction to which its indication does not apply.

- 4 The sign should be located at a safe distance consistent with the likely approach speed of traffic. When used with another temporary warning sign the TEMPORARY POLICE FLASHING LIGHT should precede such sign by a distance of at least 60m in rural areas or 25 m in urban areas.
- 5 The reverse side of a portable sign TW347 shall be marked with 150 mm black and yellow horizontal stripes.
- 6 A permanent version of the TEMPORARY POLICE FLASHING LIGHT warning sign should not be used.

JETTY EDGE OR RIVER BANK

For dimensions
ref. Vol4
page
3.4.48



COLOURS:
PERMANENT
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

3.4.39 Jetty Edge or River-bank

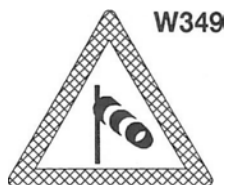
- 1 The JETTY EDGE or RIVER-BANK warning sign W348 is **to warn road users that the roadway ahead leads to a jetty edge or a river bank.**
- 2 Sign W348 should be displayed in advance of the hazard at a distance in accordance with the design

speed of the road. These signs should be located and sized as indicated in Table 3.1 or Figure 3.1.

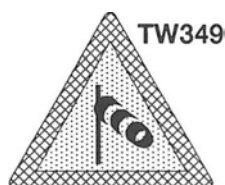
- 3 A temporary version of sign W348 is unlikely to be used, but if required should be numbered and referred to as TW348.

CROSSWINDS

For dimensions
ref. Vol4
page
3.4.49



COLOURS:
PERMANENT
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective
TEMPORARY
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective



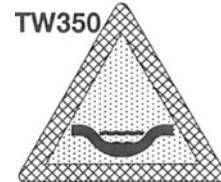
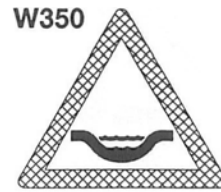
3.4.40 Crosswinds

- 1 The CROSSWINDS warning sign W349 is **to warn road users of the possibility of severe crosswinds ahead which are capable of deflecting a vehicle from its path.**
- 2 Sign W349 should be displayed in advance of sections of roadway where severe crosswinds are known to occur even if occasionally. The use of a permanent windsock at the actual site of the hazard is recommended. Such sites tend to occur in mountainous and undulating areas.

- 3 The sign should be displayed in advance of the section where crosswinds are likely to be dangerous in accordance with the provisions of Table 3.1 or Figure 3.1. The use of a SUPPLEMENTARY PLATE sign IN11.2 indicating the distance for which severe crosswinds may be expected is recommended. (See Section 3.6.)
- 4 Temporary warning sign TW349 may be used under the same circumstances as a permanent CROSSWINDS sign if a roadworks detour is located in an area subject to severe crosswinds.

DRIFT

For dimensions
ref. Vol. 4
page
3.4.50



COLOURS:

PERMANENT

Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White
retroreflective

TEMPORARY

Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow
retroreflective

3.4.41 Drift

- 1 The DRIFT warning sign W350 is **to warn road users of a drift ahead which may contain water, rock outcrops or loose material or which may represent a hazardous depression in the roadway when dry.**
- 2 Sign W350 should be displayed in advance of the hazard at a distance dependent on the operating speed

of traffic. Figure 3.1 should be used to determine the appropriate distance.

- 3 Temporary warning sign TW350 may be used under the same circumstances as permanent DRIFT warning signs when a temporary drift forms part of a roadworks detour.

LOW FLYING AIRCRAFT

For dimensions
ref. Vol. 4
page
3.4.51



COLOURS:

PERMANENT

Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

3.4.42 Low Flying Aircraft

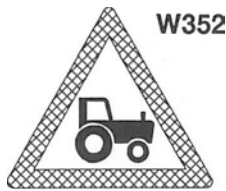
- 1 The LOW FLYING AIRCRAFT warning sign W351 is **to warn road users of the likelihood of the presence of low flying aircraft in the vicinity of the roadway.**
- 2 Sign W351 may be displayed in the environs of airports to warn drivers of the possibility of sudden aircraft noise. This

noise could result in a startled reaction on the part of a driver.

- 3 A temporary version of sign W351 is unlikely to be used, but if required should be numbered and referred to as TW351.

AGRICULTURAL VEHICLES

For dimensions
ref. Vol4
page
3.4.52



COLOURS:
PERMANENT
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

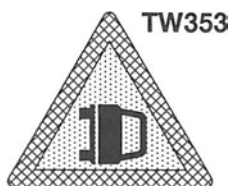
3.4.43 Agricultural Vehicles

- 1 The AGRICULTURAL VEHICLES warning sign W352 is to warn road users that tractors or other slow moving agricultural vehicles regularly cross or enter the roadway ahead.
- 2 Sign W352 may be displayed on the approach to a junction or farm access where tractors or other agricultural vehicles regularly cross or enter the traffic stream.
- 3 Care should be exercised in the use of sign W352. It should only be used when the hazard relates specifically to the use of public roads by agricultural vehicles and in particular slow-moving agricultural vehicles. If the potential hazard is more general an appropriate road layout warning sign (see Section 3.2) or direction of movement warning sign (see Section 3.3) should be used. For instance if the real problem is limited sight distance to the point of access a CONCEALED DRIVEWAY warning sign W216, W217 or W218 may be more appropriate.
- 4 When used the sign should be located in advance of the junction or farm access in accordance with the design speed of the road at a distance as indicated in Table 3.1 or Figure 3.1.
- 5 A temporary version of sign W352 is unlikely to be used, but if required should be numbered and referred to as TW352.

ACCIDENT

For dimensions
ref. Vol4
page
3.4.53

COLOURS:
TEMPORARY
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective



3.4.44 Accident

- 1 The ACCIDENT temporary warning sign TW353 is to warn road users that there is an accident ahead.
- 2 Sign TW353 should be displayed in advance of an accident site which is being attended by police and/or emergency services and which represents a temporary hazard to approaching road users. If such a sign is carried by a member of the public it may be displayed immediately an accident occurs or is discovered, prior to the arrival of emergency services.
- 3 The sign should be portable so that it can be quickly erected, moved or removed as circumstances require.
- 4 The reverse side of sign TW353 shall be marked with alternating black and yellow horizontal stripes 150 mm wide. These may be retroreflective to improve visibility under poor light conditions.
- 5 The sign should be positioned in advance of the accident site at a distance in accordance with Table 3.1. Particular attention should be paid to road vertical and horizontal curvature and sight distance to the sign. If necessary the sign should be placed further from the site than recommended in Table 3.1.
- 6 At a major accident site the sign may be mounted in conjunction with a flashing yellow warning light or a SUPPLEMENTARY PLATE sign IN11. In circumstances where traffic queues are likely to form someone, if necessary a member of the public, should be given the task of moving the sign to keep it safely in advance of end of the queue of traffic.

REDUCED VISIBILITY

COLOURS:

PERMANENT

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

W354



For dimensions
 ref. Vol. 4
 page
 3.4.50

TW354



3.4.45 Reduced Visibility

- 1 The REDUCED VISIBILITY warning sign W354 is **to warn drivers of the possibility of reduced visibility ahead due to mist, fog or smoke which constitutes a hazard and may require a considerable reduction in speed.**
- 2 Sign W354 should be displayed in advance of sections of roadway on which reduced visibility is a common or regular occurrence. Since the condition is likely to exist for some distance it is recommended that sign W354 be used with a SUPPLEMENTARY PLATE sign IN11.2.
- 3 For practical purposes although the condition is not likely to be a permanent one, if it occurs with significant

frequency, the use of a permanent sign is recommended. As a guide a significant frequency could be considered to be the occurrence of the condition for an hour or more on 30 or more days per year. In addition the intensity of the mist or fog should be such that a reduction in speed to 75% or less of the normal operating speed is required for safety.

- 4 Temporary warning sign TW354 should be part of the standard equipment carried by any maintenance unit responsible for attending or controlling any roadside fires likely to produce sufficient smoke to reduce visibility. The ability to display sign TW354 in any advance warning variable message system is recommended.

COLOURS:

PERMANENT

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

TEMPORARY

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

CONGESTION

W355



For dimensions
 ref. Vol. 4
 page
 3.4.55

TW355



3.4.46 Congestion

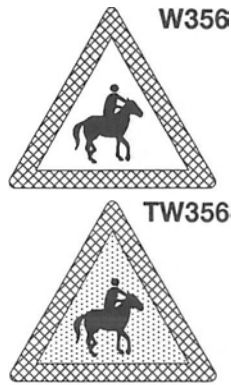
- 1 The CONGESTION warning sign W355 is **to warn drivers of the possibility of slow-moving or stationary traffic in the roadway ahead which may constitute a hazard due to limited visibility due to road alignment or traffic density and which may require the driver to significantly reduce speed or to stop.**
- 2 Sign W355 should be displayed in advance of sections of roadway on which congestion occurs in an unpredictable or random manner. Use of the sign is only recommended on roadways such as normally high speed arterials or freeways

subject to such low levels of service that traffic flow becomes so unstable as to cause significant reductions in speed or traffic to come to a stop. Under such circumstances the recommended application of the sign is by means of the temporary form and as a variable message sign.

- 3 If a permanent version of the sign is used it is recommended that the sign be displayed in advanced of sections of normally free-flowing roadway with a SUPPLEMENTARY PLATE sign IN 11.2 indicating the distance for which such congestion is likely to occur.

HORSE AND RIDER

For dimensions
ref. Vo14
page
3.4.56



COLOURS:
PERMANENT
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective

TEMPORARY
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective

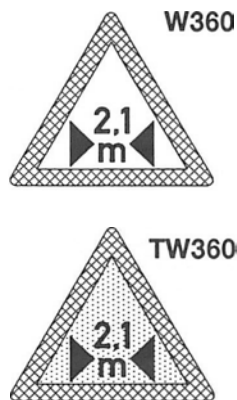
3.4.47 Horse and Rider

- The HORSE AND RIDER warning sign W356 is **to warn drivers of the possible presence of horses in the roadway ahead which are being ridden or led by their riders and which may constitute a hazard if disturbed by passing traffic.**
- Sign W356 should be displayed in advance of road crossing points regularly used by horses and riders in accordance with the provisions of Figure 3.1. If horses and riders are likely to travel along the length of a road, within the road reserve, for 500 metres or more, it is recommended that sign W356 be used with SUPPLEMENTARY PLATE sign IN11.2. If necessary such a sign combination should be repeated at suitable intervals.
- Temporary warning sign TW356 should be displayed when the activities of horses and riders are part of infrequent sporting or social functions. Such a sign, or signs, should be portable and should be placed in position by the person organizing the function. The signs shall be removed when the horse and rider activities have ended.
- The reverse side of portable signs shall be marked with black and yellow 150 mm horizontal stripes.

NOTE: Advance warning signs W357 to W359 are additional variations of WILD ANIMALS sign W313 (see Subsection 3.4.11).

WIDTH RESTRICTED

For dimensions
ref. Vol 4
page
3.4.60



COLOURS:
PERMANENT
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: White retroreflective
Legend Din 1451 Style b

TEMPORARY
Triangle: Red retroreflective
Symbol: Black semi-matt
Background: Yellow retroreflective
Legend Din 1451 Style B

3.4.48 Width Restricted

- The WIDTH RESTRICTED warning sign W360 is **to warn drivers that the width of the roadway or part of the roadway ahead is restricted and that a prohibition on vehicles with a width equal to or greater than that indicated in metres by means of a number on such sign may exist.**
- Sign W360 should be displayed in advance of:
 - any specific narrow structure which cannot accommodate a single vehicle with a width equal to or greater than that displayed on the sign if the structure concerned carries one-way traffic;
 - any specific narrow structure which cannot accommodate two vehicles travelling in opposite directions at the same time, each of which vehicles having a width equal to or greater than that displayed on the sign if the structure concerned carries two-way traffic;
 - any section of narrow roadway which cannot accommodate two-way movement of vehicles having a width equal to or greater than that displayed on the sign.
- The width restriction shown on sign W360 should be the same as shown on the following WIDTH LIMIT sign R238.
- Sign W360 should be located some distance from the restriction in the roadway so that restricted vehicles have the opportunity to follow an alternative route. A typical sign arrangement for a width restricted site is given in Volume 2. (See Volume 2, Chapters 3 and 11).
- Temporary sign TW360 may be used under the same circumstances as permanent WIDTH RESTRICTED warning signs if construction or maintenance work requires that the normal roadway width be temporarily reduced to such an extent that a restriction needs to be applied.

ELECTRICAL SHOCK

COLOURS:

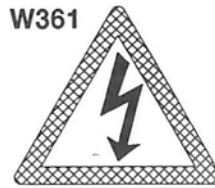
PERMANENT

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

TEMPORARY

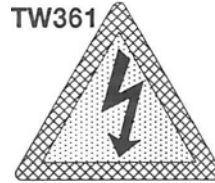
Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: Yellow retroreflective

W361



For dimensions
 ref. Vol. 4
 page
 3.4.61

TW361



3.4.49 Electrical Shock

- 1 The ELECTRICAL SHOCK warning sign W361 is **to warn road users that there are potentially live over-head electrical power cables ahead and that these may constitute a hazard if contacted by a vehicle or any vertical projection carried by the road user.**
- 2 Sign W361 should be displayed in advance of any overhead electrical power cables, which cross over a roadway of whatever class, when the clearance between such power cables and the ground is less than 6 metres. Sign W361 will commonly be used in conjunction with HEIGHT RESTRICTED warning sign W320 or RAILWAY CROSSING warning sign W318, in association with height gauges and/or railway crossings.
- 3 When positioned primarily for vehicular traffic, sign W361 should be located according to Figure 3.1 and sized

according to Table 3.1. The risk of electrical shock is particularly high for road users carrying long fishing rods in a vertical position. Such road users may be pedestrians or they may be in the back of a vehicle. If sign W361 is positioned primarily for pedestrians it may be of minimum size and should be located 5 metres to 10 metres in advance of the pedestrian crossing point under the power cables. A SUPPLEMENTARY PLATE sign IN11.4 with a message such as "BEWARE Antennae and Fishing Rods" is recommended for use with sign W361 in such situations (see Volume 2, Chapter 7 for detailed signing applications).

- 4 Temporary sign TW361 may be used under the same circumstances as permanent ELECTRICAL SHOCK warning signs during construction or maintenance work under overhead electrical power cables.

COLOURS:

PERMANENT

Triangle: Red retroreflective
 Symbol: Black semi-matt
 Background: White retroreflective

W362



For dimensions
 ref. Vol. 4
 page
 3.4.62

TRAM

3.4.50 Tram

- 1 The TRAM warning sign W362 is **to warn road users that light rail vehicles, or trams, are operating in the roadway ahead.**
- 2 Sign W362 should be displayed in advance of the commencement of tram operations in accordance with Figure 3.1 and with sign sizes according to Table 3.1. Sign W362 may also be displayed on roads intersect-

ting roadways in which trams are operating. In each case the sign is to warn road users of the limited manoeuvrability of the rail-based public transport vehicles.

- 3 A temporary version of sign W362 is unlikely to be used, but if required should be numbered and referred to as TW362.

DANGER PLATES/DELINEATOR PLATES

COLOURS:

PERMANENT

Red retroreflective
on white retroreflective

TEMPORARY

Black semi-matt
on yellow retroreflective

W401



W402



For dimensions
ref. Vol. 4
page
3.5.1

TW401



TW402



3.5.1 Danger Plates/Delineator Plates

- The DANGER PLATE warning signs W401 and W402 and the DELINEATOR PLATE temporary warning signs TW401 and TW402 are **to warn road users of an obstruction or temporary obstruction, in the roadway, or alteration or temporary alteration, in the roadway alignment to the right or left side of the roadway.**
- Signs W401 and W402 should be displayed at all hazardous obstructions that occur within the shoulder or verge of a roadway such as bridge abutments, culvert head-walls or posts without guardrail protection. Sign W401 should be used on the left side of the roadway so that traffic passes to the right of the plate. Sign W402 should be used on the right side of the roadway so that traffic passes to the left of the plate.

TABLE 3.4

DELINEATOR SPACING

TABLE 3.4

Temporary condition	Delineator spacing (m)
Taper 1 in 10 ⁽¹⁾	3
Taper 1 in 20 ⁽¹⁾	5
Taper 1 in 30 ⁽¹⁾	7
Taper 1 in 40 ⁽¹⁾	10
Median crossover on curve ⁽²⁾	5 to 10
Crossover on straight	10
End taper 1 in 5 ⁽³⁾	5
End taper 1 in 10 ⁽³⁾	7
Short straight	10
Long rural straight	200 max.
High speed roadway	50 max.

NOTES:

(1) Tapers refer to those on the approach to a roadworks site or change in direction.

(2) Spacings given are for outer curves. Inner curve spacing may be increased to 10 to 20 m.

(3) End taper refers to a taper used to widen a roadway back to its normal width at the end of a roadworks site.

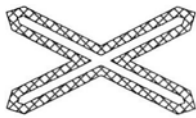
(4) Spacings greater than 50 m should be avoided on freeways.

- 3 Open ditches, high embankments and ill-defined curves, particularly where roadside space is limited in urban areas may be demarcated using a number of DANGER PLATE hazard markers (see Subsection 3.5.3 on page 3.5.4).
- 4 Signs TW401 and TW402 should be displayed at all obstructions at roadworks sites which are potentially hazardous. Sign TW401 should be used on the left side of the roadway so that traffic passes to the right of the plate. Sign TW402 should be used on the right side of the roadway so that traffic passes to the left of the plate. In addition delineator plates should be used to indicate temporary road alignments which occur at roadworks sites. **200 litre, or similar drums shall not be used for this purpose.**
- 5 DANGER PLATES and DELINEATOR PLATES should have a minimum size of 600 mm height and 150 mm width. The ratio of height to width should be maintained at 4 to 1 up to a maximum size of 1 200 mm x 300 mm, which size should be used to indicate bridge abutments and columns at freeway underpasses.
- 6 Tapers, median crossovers and other temporary alignments at roadworks sites should be demarcated using sequences of DELINEATOR PLATES spaced according to Table 3.4.
- 7 Details of applications of DELINEATOR PLATES are covered in Volume 2, Chapter 13.
- 8 Variants of signs W401/W402 and TW401/TW402, numbered W413 and TW413 respectively, may be used to identify traffic island gore areas (see Subsection 3.5.8).
- 9 Variants of signs W401 and TW401, numbered W415 and TW415 respectively, are applicable for use to mark low level overhead structures (see subsection 3.5.9).

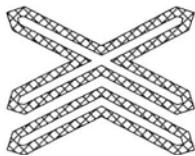
RAILWAY CROSSING

For dimensions
ref. Vol. 4
page
3.5.2

W403



W404



COLOURS:
PERMANENT
Outer cross: Red retroreflective
Inner cross: White retroreflective

3.5.2 Railway Crossing

- 1 The RAILWAY CROSSING warning signs W403 and W404 are **to warn road users of railway lines at a railway crossing**, where traffic should reduce speed and exercise extreme caution, or stop if required to do so.
- 2 Sign W403 shall be displayed on each approach to a single railway crossing. Sign W404 shall be displayed on each approach to a two or more lines railway crossing. A SUPPLEMENTARY PLATE sign IN11.4 with a text message such as "51ines" may be displayed with sign W404.
- 3 A STOP sign R1, YIELD sign R2, or dual red flashing disc signals (see Chapter 6) may be displayed at railway crossings on the same post above sign W403 or sign W404.
- 4 Typical sign and road marking sequences of various classes of railway crossing protection are detailed in Volume 2, Chapter 7.
- 5 Temporary versions of the RAILWAY CROSSING warning signs should not be used.

SHARP CURVE CHEVRON

COLOURS:

PERMANENT

Arrow(s): Red retroreflective

Background: White retroreflective

TEMPORARY

Arrow(s): Black semi-matt

Background: Yellow retroreflective

For dimensions
ref. Vo/ 4
page
3.5.3
3.5.4

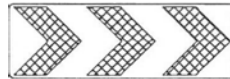
W405



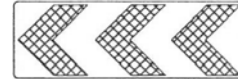
W406



W407



W408



TW405



TW406



TW407



TW408



3.5.3 Sharp Curve Chevron

- 1 The SHARP CURVE CHEVRON warning signs W405, W406, W407, and W408 are **to warn road users that the roadway ahead is diverted to the right or left through a sudden change of direction around a sharp curve.**
- 2 The SHARP CURVE CHEVRON warning signs W405 and W406 shall be displayed in multiples of three or more signs when it is required to mark sharp longitudinal curves which may constitute a hazard. A minimum of three separate signs, spaced in accordance with the provisions of Table 3.5, are necessary to give road users the required impression of sharp curvature. Subject to the prevailing speed limit this applies particularly to curves with a radius in the range of 60 metres to 600 metres. Signs W405 and W406 may be used on curves of greater than 600 metres radius when the radius of the curve is significantly less than is common on adjacent sections of roadway.
- 3 When a sharp curve, or bend, has a radius of less than 60 metres the hazard may be marked by a composite modular warning sign W407 or W408 positioned so that the three included chevrons point in the direction of curvature. SHARP CURVE CHEVRON warning signs W407 and W408 shall be manufactured as one piece signs as specified in Volume 4. This application is generally only appropriate in urban residential areas or on minor class "D" or "E" rural roads.

TABLE3.5 SHARP CURVE CHEVRON SPACING TABLE3.5

Curve radius (m)	Single module spacing (m)
45	5-8
60	8-15
150	15-25
300	25
600	25

- 4 When SHARP CURVE CHEVRON signs W405 and W406 are used to define the curvature of a road it is recommended that the first sign to be positioned should be located as close as possible to the line of sight of a driver on the tangent approach to the curve. All other W405 or W406 signs required for the curve should then be spaced forwards and backwards around the curve at "S" metre spacings as given in Table 3.5. Subject to the minimum requirement that three signs shall be visible at all times (**allowing for both horizontal and vertical curvature**) it is recommended that sufficient W405 or W406 signs be provided to define the full length of the curve. The first sign in such a sequence of signs should ideally be positioned at or close to the beginning tangent point to the curve. In cases of particular hazard it may be advantageous to provide up to three signs in advance of the tangent point on the straight, on a flat taper, to enhance the warning effect.
- 5 When a guardrail is provided on a sharp curve W405 and W406 signs may be placed above and behind the guardrail in addition to GUARDRAIL DELINEATOR D1 (see Chapter 7) to enhance the visibility of the guardrail and improve delineation of the sharp curve (see Figure 1.23).
- 6 In terms of the recommendations given in paragraphs 3.5.3.2 to 3.5.3.5 the use of W405 or W406 signs singly or in pairs is not recommended. A variant of W405/W406 (and TW405/TW406) signs combined, numbered W414 (and TW414) may be used to identify traffic island gore areas (see Subsection 3.5.8).
- 7 SHARP CURVE CHEVRON signs may be combined for use at T-junctions. This variation is covered in Subsection 3.5.4 and is termed a T-JUNCTION CHEVRON sign W409. Recommendations on the sizes of SHARP CURVE CHEVRON signs and T-JUNCTION CHEVRON signs are given in Table 3.6.
- 8 A sequence of SHARP CURVE CHEVRON signs should be mounted at a constant height above shoulder level. Details of sign position and mounting height are given in Chapter 1.
- 9 Temporary warning signs TW405, TW406, TW407 and TW408 may be used under the same circumstances as permanent SHARP CURVE CHEVRON signs at roadworks and other temporary sites. However, they should be used to supplement DELINEATOR PLATE signs TW401 and/or TW402 where necessary and not to replace these signs at temporary changes of direction. Signs TW407 and TW408 are recommended when a directional message is required at barricades used at temporary roadway or lane closures behind and above DELINEATOR PLATES. When used in this manner they may be combined with any of a wide range of temporary warning or regulatory signs used at the site. Care must be exercised in choosing the correct chevron signs for use at barricades. Refer also to Subsection 3.5.4, temporary T-JUNCTION CHEVRON sign TW409, Subsection 3.5.5, temporary ROAD CLOSED CHEVRON sign TW410 and Subsection 3.5.6, TEMPORARY BARRICADE sign TW411.
- 10 The provision of permanent and temporary SHARP CURVE CHEVRON signs is a significant change in practice. It is not intended that all existing signs be replaced with immediate effect. Details of timing for the completion of this exercise are given in Chapter 1 together with requirements for other road traffic signs on a class by class basis where appropriate. Each authority should prepare a phasing-in programme based on the following guidelines -
- (a) all new permanent installations shall use red and white coloured chevrons;
 - (b) a mixture of red and white, and black and yellow chevrons must not be permitted to occur at any specific site;
 - (c) if it is necessary to replace one or more black and yellow chevrons at a site, due to damage or other maintenance requirements, ALL chevrons in the set shall be replaced by red and white chevrons, subject to the availability of used black and yellow chevrons from other sites which may be re-installed up to the limit of their useful life, OR until the replacement deadline referred to in Chapter 1.
- 11 Detailed examples of the use of various chevron signs are given in Volume 2.

T-JUNCTION CHEVRON

PERMANENT

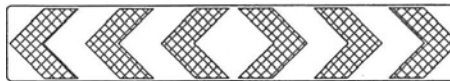
Arrows: Red retroreflective
Background: White retroreflective

TEMPORARY

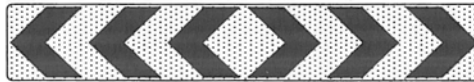
Arrows: Back semi-matt
Background: Yellow retroreflective

For dimensions
Ref. Vol.4
Page
3.5.5

W409



TW409



3.5.4 junction Chevron

- 1 The T-JUNCTION CHEVRON warning sign W409 is **to warn road users that the roadway does not continue beyond the junction and that a turn must be made to the right or left.**
- 2 The recommended minimum module sizes for SHARP CURVE CHEVRON and T-JUNCTION CHEVRON signs in relation to various categories of road are given in Table 3.6. Certain sizes given in brackets allow alternate dimensions to permit more economical use of standard material sizes.
- 3 A T-JUNCTION CHEVRON warning sign shall comprise a minimum of three right modules and three left modules. Sign W409 may be displayed on its own at a T-junction or in combination with a STACK-TYPE DIRECTION sign GD2. When used with a DIRECTION sign, the length of sign W409 shall be made the same as the DIRECTION sign. The number of right and left modules shall be increased as necessary so that there is always an equal number of each. Any extra length required to match the DIRECTION sign shall be located in the centre of the sign and be provided in the background colour. (For examples see Figure 4.45). When used with a DIRECTION sign, sign W409 shall be mounted immediately below the DIRECTION sign.
- 4 Temporary warning sign TW409 may be used under the same circumstances as a permanent T-JUNCTION CHEVRON warning sign when a temporary T-junction is created at a roadworks or building construction site or by a temporary closure of the road ahead at a crossroad. Care should be exercised in choosing the correct chevron sign for use at a temporary road closure at a crossroad if the intersecting crossroad is a one-way road. Refer also to Subsection 3.5.3, temporary SHARP CURVE CHEVRON signs TW405 to TW408, Subsection 3.5.5, temporary ROAD CLOSED CHEVRON sign TW410 and Subsection 3.5.6, TEMPORARY BARRICADE sign TW411.
- 5 Detailed examples of the use of various chevron signs are given in Volume 2.

TABLE 3.6 RECOMMENDED CHEVRON MODULE SIZES TABLE 3.6

Typical use	Minimum number of modules	Minimum module size (mm)			
		Urban	Rural	Freeway	Special applications
Sudden change of direction	3	450(400)	600	750(800)	900
T-junction	6	450(400)	450(400)	600	750(800)

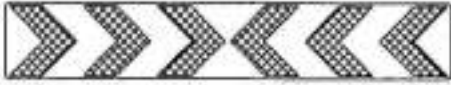
NOTES:

- (1) If particular emphasis is required due to a history of accidents at the site larger modular dimensions may be used than indicated above.
- (2) The minimum number of modules used at a T-junction shall consist of three modules pointing left and three modules pointing right.

DEAD END/ROAD CLOSED CHEVRON

For dimensions
ref. Vo/4
page
3.5.6

W410



TW410



COLOURS:

PERMANENT

Arrows: Red retroreflective
Background: White retroreflective

TEMPORARY

Arrows: Black semi-matt
Background: Yellow retroreflective

3.5.5 Dead End/ Road Closed Chevron

- 1 The DEAD-END CHEVRON warning sign W410 and the ROAD CLOSED CHEVRON temporary warning sign TW410 are **to warn road users that the roadway does not continue beyond the sign** and that traffic must return in the direction from which it has come, or proceed as directed by accompanying traffic signs.
 - 2 Sign W410 may be displayed at the physical limit of a cul-de-sac roadway to indicate to drivers that they must reduce speed and prepare to turn around. The sign is particularly recommended for use in cul-de-sac which have been created by road closure and/or where the view ahead is open and unobstructed at the end of the cul-de-sac.
 - 3 The sign may be displayed in addition to CUL-DE-SAC information signs, IN4 to IN6.
 - 4 The ROAD CLOSED CHEVRON temporary warning sign TW410, may be used to indicate the full, temporary closure of a roadway due to roadworks, building construction or maintenance operations
- which effectively makes the roadway concerned a temporary *cul-de-sac*. Sign TW410 should not be used at a partial roadway closure such as a lane or lanes closure. In such situations the use of TEMPORARY BARRICADE sign, TW411 is recommended normally in conjunction with KEEP LEFT or KEEP RIGHT temporary regulatory sign, TR103 and TR104.
- 5 Care should be exercised when choosing a chevron sign for a temporary road closure barricade. Sign TW410 should only be used for full road closures when traffic can only return in the direction from which it has arrived at the barricade. When traffic is diverted at a barricade to the right and/or left SHARP CURVE CHEVRON or T-JUNCTION CHEVRON signs will be more appropriate (see Subsections 3.5.3, 3.5.4 and 3.5.6).
 - 6 Detailed examples of the use of various chevron signs are given in Volume 2.

BOOM BARRICADE / TEMPORARY BARRICADE

COLOURS:

PERMANENT

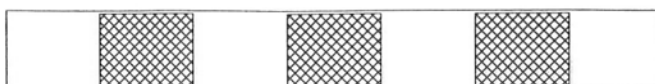
Symbols: Red retroreflective
Background: White retroreflective

TEMPORARY

Symbols: Red retroreflective
Background: Yellow retroreflective

For dimensions
ref. Vol.4
page
3.5.7

W411



TW411



3.5.6 Boom Barricade / Temporary Barricade

- 1 The BOOM BARRICADE warning sign W411 is **to warn road users that the roadway or access is closed to traffic whilst the sign is in a horizontal position**, and the TEMPORARY BARRICADE warning sign TW411 is **to warn road users that a portion of a roadway is temporarily closed to traffic**.
- 2 Sign W411 may be displayed in conjunction with a STOP sign R1, or a STOP sign R1 with flashing red disc signals, at a railway crossing to improve the visual impact of the crossing when it is closed to road users. Sign W411 may also be displayed at the entrance or access point to parking areas or other similar sites where it is desired to visually bar entry until payment has been made or access clearance been given.
- 3 Sign TW411 may be displayed behind DELINEATOR PLATE signs, TW401 or TW402 either on its own or with a temporary KEEP LEFT regulatory sign TR103, or a temporary KEEP RIGHT regulatory sign TR104 as appropriate to the direction of movement of traffic. Several TW411 signs spaced at regular intervals may be used in this manner to improve the visual impact of the signing of a temporary lane closure or partial road closure, or of a temporary crossover through a median island on a section of dual roadway.
- 4 The use of TEMPORARY BARRICADE signs in conjunction with temporary PROCEED LEFT ONLY regulatory sign TR105, or temporary PROCEED RIGHT ONLY regulatory sign TR106 is not recommended. TR105 and/or TR106 signs should rather be used with temporary ROAD CLOSED CHEVRON warning sign TW410 if a road has been completely closed temporarily to through traffic, or with one or more temporary SHARP CURVE CHEVRON warning signs TW407 and TW408 if traffic may still turn left or right in front of the road closure.
- 5 On tapers or crossovers it is recommended that TW411 signs be located at approximately 50 m intervals for higher speed conditions and at 20 m to 30 m intervals for lower speed conditions.
- 6 Details of typical roadworks applications involving TEMPORARY BARRICADE warning signs are given in Volume 2.

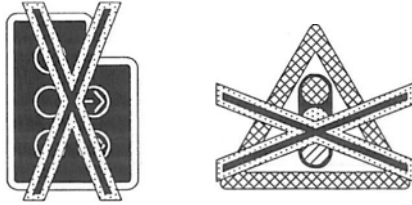
TRAFFIC SIGNALS OUT OF ORDER

For dimensions
ref. Vol. 4
page
3.5.8

COLOURS:

TEMPORARY
Outer cross: Yellow retroreflective
Inner cross: Black semi-matt

TW412



3.5.7 Traffic Signals Out Of Order

- 1 The TRAFFIC SIGNALS OUT OF ORDER temporary warning sign TW412 is **to warn road users that the traffic signals ahead, or to which the sign is attached, are out of order**, and that the junction concerned is under the control of a traffic officer or is temporarily operating under all-way STOP control.
- 2 If a TRAFFIC SIGNALS AHEAD sign W301 is located in advance of the traffic signal, a temporary warning- sign TW412 may be placed over the W301 sign for the period that the traffic signal is out of order.
- 3 Temporary warning sign TW412 may be placed in front of one or all traffic signal faces facing each direction of approach for the period that the traffic signal is out of order in addition to the use with sign W301.
- 4 Sign TW412 must be removed immediately the traffic signal is re-commissioned.

GORE MARKERS

For dimensions
ref. Vol. 4
page
3.5.9
3.5.10



W413



W414



TW413



TW414

COLOURS:

PERMANENT
Red retroreflective
on white retroreflective
TEMPORARY
Black semi-matt
on yellow retroreflective

3.5.8 Gore Plate / Gore Chevron

- 1 The GORE MARKER signs GORE PLATE warning sign W413 and GORE CHEVRON warning sign W414 are **to warn road users of a physical separation in the road ahead which they must pass either to the left or right of and that such an area, known as the "gore area", of a junction may contain road signs and/or kerbing which may constitute a hazard.**
- 2 Signs W413 and W414 are recommended for use in all gore areas where roadways for traffic travelling in the same direction diverge from each other and road users have to choose one path or the other to proceed. Such gore areas are common at all freeway off ramp exits from the main carriageway and at secondary splits in off ramps on fully directional ramps (common at systems interchanges). Gore areas are also common at high standard at-grade channelised road junctions in both rural and urban areas.
- 3 GORE PLATE sign W413 is recommended for use on small channelised traffic islands which result in a split in traffic flow for streams of traffic travelling in the same direction.
- 4 GORE CHEVRON sign W414 is recommended for use on larger channelising traffic islands and in gore areas on freeway off ramps. A variant of sign W414 is included in GORE EXIT sign GA4, which is specified for the initial gore area where an off ramp leaves the main carriageway.

COLOURS:

PERMANENT

Red retroreflective
on White retroreflective

TEMPORARY

Black semi-matt
on Yellow retroreflective

OVERHEAD DANGER PLATES

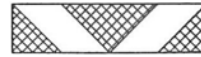
For dimensions

ref. Vo/4

page

3.5.11

W415



TW415



3.5.9 Overhead Danger Plates

- 1 The OVERHEAD DANGER PLATES warning sign W415 is **to warn road users of an obstruction above the roadway**.
- 2 Sign W415 shall be displayed on any height gauge or overhead structure when the clearance over any portion of the roadway is less than 4,7 m, in such a manner that two signs W415 shall be mounted, one on each side of HEIGHT LIMIT sign R204. It is recommended that signs W415 and R204 also be used when any portion of such an overhead structure has a clearance of less than 5,2 m (see Subsections 3.4.16 and 2.4.4 and Volume 2, Chapter 3 and Chapter 7).
- 3 Temporary warning sign 1W415 may be used under the same circumstances as permanent OVERHEAD DANGER PLATE sign W415 at roadworks and other temporary sites when some form of temporary obstructions exists above the roadway.
- 4 The minimum size of OVERHEAD DANGER PLATE warning signs W415 and 1W415 should be 600 mm in length and 150 mm in height. Recommended sizes in relation to approach speed and the size of HEIGHT LIMIT sign R204 are given in Table 3.7.

Operating speed(km/h)	RECOMMENDED SIZES FOR OVERHEAD HAZARD MARKING		
	R204/TR204	W415/TW415	
	Diameter(mm)	Length(mm)	Height(mm)
60	600	600	150
80	900	800	200
100	1200	1200	300
120	1200	1200	300

3.6 COMBINATIONS

3.6.1 General

- 1 It is generally recognised that in exceptional circumstances warning signs of both the triangular advance warning type and the hazard marker type may need to be supplemented to improve their impact on drivers.
- 2 Warning signs may be supplemented in one or more of the following ways:
 - (a) by combination with a supplementary plate which conveys additional information about the potentially hazardous site being signed;
 - (b) by display on a HIGH VISIBILITY background;
 - (c) by combination with a flashing light.
- 3 Situations may exist where a number of hazards can occur on a section of roadway for which different warning messages may be required. These different messages may be provided by using VARIABLE MESSAGE signs.

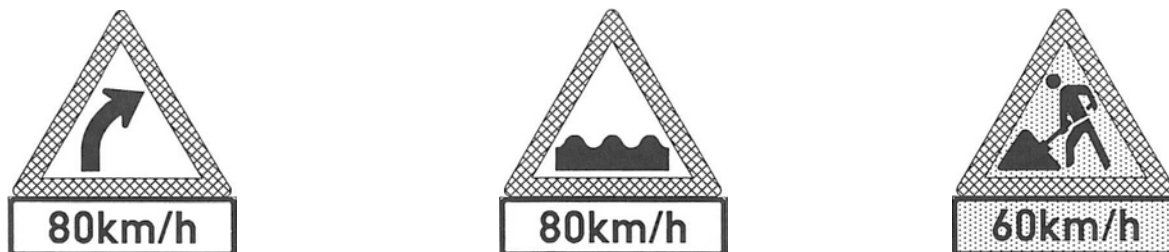
ing sign with one of several types of supplementary plate in order to provide additional information to drivers to improve their perception of the hazard. A range of typical examples of such signs, both PERMANENT and TEMPORARY are given below in the following groups:

- (a) advisory speed (IN11.1);
 - (b) distance for which the hazard occurs (IN11.2);
 - (c) distance to the hazard and distance countdown to the hazard (IN11.3);
 - (d) text messages (IN11.4);
 - (e) symbolic messages (IN11.5).
- 3 Supplementary plates should be mounted immediately below the warning sign in such a way that the gap between the sign and the plate is in the range 0 mm to 150 mm.
 - 4 Supplementary plates may also be used with warning signs mounted on HIGH VISIBILITY backgrounds. Examples of these are given in Subsection 3.6.3.
 - 5 Dimensional details of supplementary plates and details of all symbols available for use on supplementary plates, including their reference numbers are given in Volume 4, Chapter 9.

3.6.2 Supplementary Plate Examples

- 1 Details relating to type, size, colour and material of SUPPLEMENTARY PLATE signs IN11 are given in Chapter 5, Subsection 5.2.6.
- 2 Many of the Subsections dealing with specific warning signs have noted the possibility of combining the warn-

Examples: Advisory Speed - IN11.1

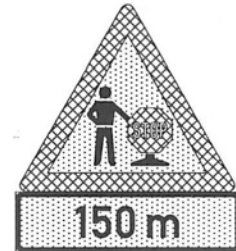
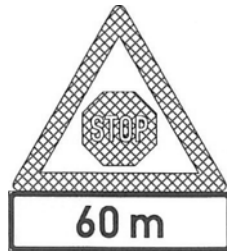


Examples: Distance "For" - IN11.2



Fig. 3.2 Typical Examples of Warning /Information Sign Combinations - 1

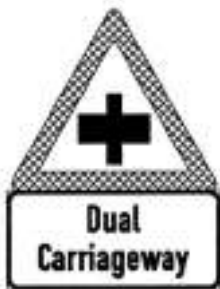
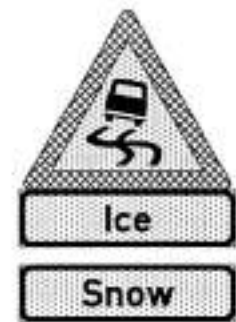
Examples: Distance “To” – IN11.3



Examples: Distance Countdown



Examples: Text Message – IN11.4



Examples: Symbolic Message – IN11.5

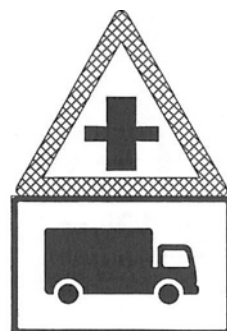
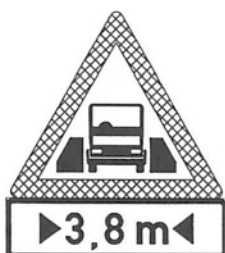


Fig. 3.3 Typical Examples of Warning /Information Sign Combinations - 2

3.6.3 High Visibility Background

- 1 Any PERMANENT or TEMPORARY warning sign may be incorporated into a HIGH VISIBILITY background to improve the target value of the sign. Two warning messages may be combined into one such sign to indicate that the hazard may comprise two factors, which either have their effect separately or in combination. Such a sign shall not contain one PERMANENT and one TEMPORARY warning sign.
 - 2 The background and border for a PERMANENT HIGH VISIBILITY warning sign shall be in white retroreflective and red retroreflective material respectively.
 - 3 The background and border for a TEMPORARY HIGH VISIBILITY warning sign shall be in yellow retroreflective and black semi-matt material respectively.
 - 4 When ordering HIGH VISIBILITY warning signs the normal warning sign number(s) should be quoted, followed by the letters WA, WB, WC, 1/0.(D), WE or WF (see Volume 4, Figures 2.19 and 3.2 to 3.5). Examples of HIGH VISIBILITY warning signs and their numbers are given below.
 - 5 The warning sign symbol and background are the same whether used as a stand-alone triangular warning sign or whether incorporated into a HIGH VISIBILITY background. However, the triangular red border is modified when used on the HIGH VISIBILITY background. Dimensional differences are illustrated below and detailed in Volume 4.
 - 6 HIGH VISIBILITY background warning signs should commonly be located at a greater distance from the hazard than indicated by Table 3.1. There may be advantages in repeating the sign on an approach to a hazard.
- This technique is recommended particularly for temporary alterations in alignment of the roadway at roadworks sites. In such cases the addition of supplementary distance information plates is recommended. The recommended locations of such signs from the hazard are 400 m and 200 m. If space is limited this may be reduced to 300 m and 100 m. Details of sign sequences for both PERMANENT and TEMPO- RARY installations are given in Volume 2.
- 7 In some circumstances it is an advantage to combine a regulatory and a warning message on the same supports so that drivers may understand better why a certain regulation is being imposed (such as a reduced speed limit- SPEED LIMIT sign R201). This technique is once again particularly recommended for use at temporary roadworks sites. In such cases the regulatory and warning messages should be provided in separate HIGH VISIBILITY backgrounds mounted on common supports with the regulatory message above the warning message, and with a 150 mm - 200 mm gap between signs.
 - 8 The use of SUPPLEMENTARY PLATE signs IN11 with warning sign HIGH VISIBILITY backgrounds is common. The border and message for a PERMANENT IN11 sign shall be black on a white background and for a TEMPORARY TIN11 sign they shall be black on a yellow background. Under some circumstances it may be necessary to give two supplementary information messages. When this is the case the messages should be given either by using a WB background and one IN11 sign, or by means of two IN11 signs, one below the other. Examples are illustrated below.

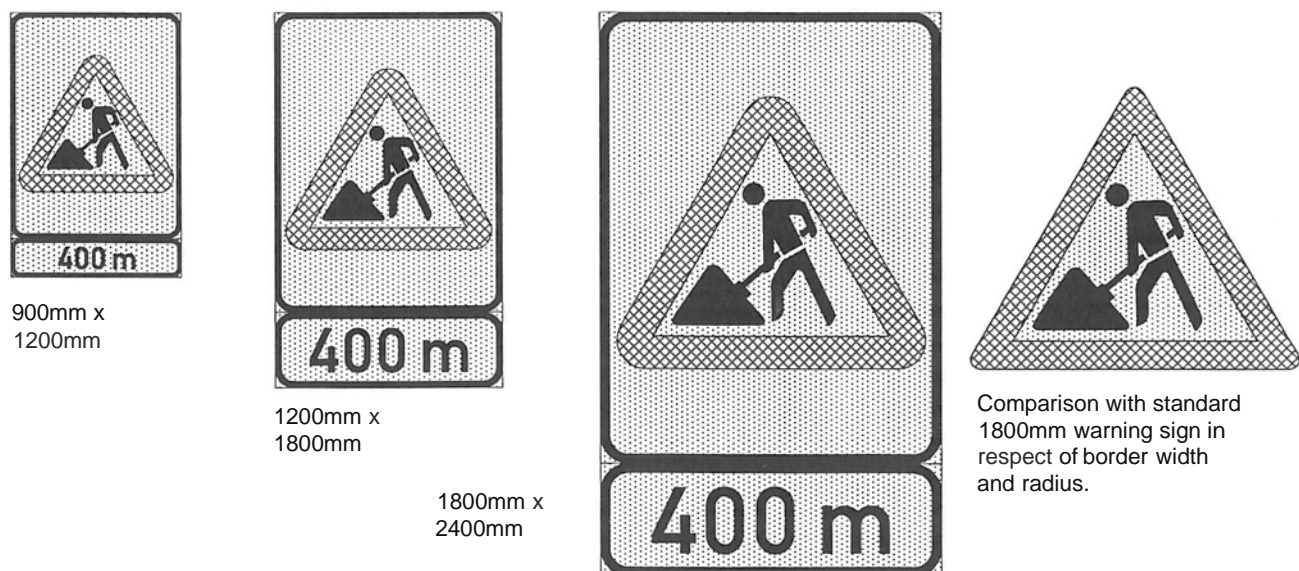
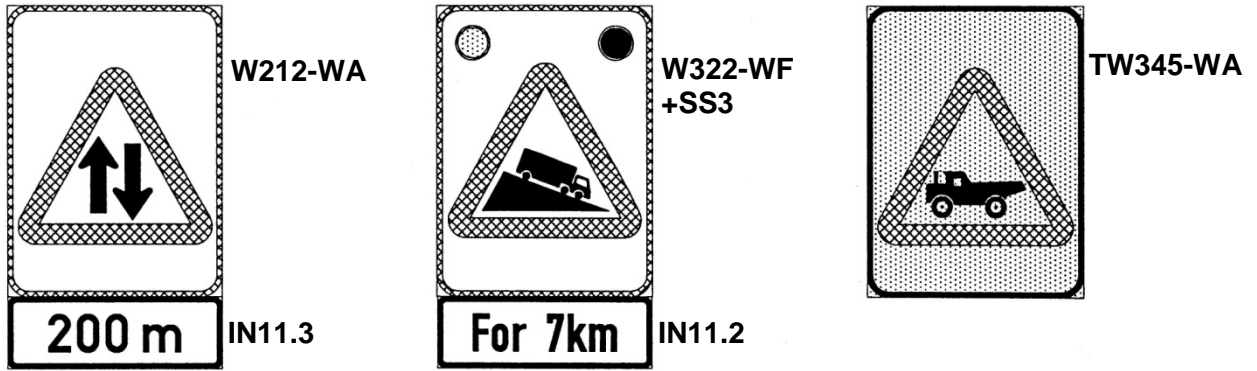


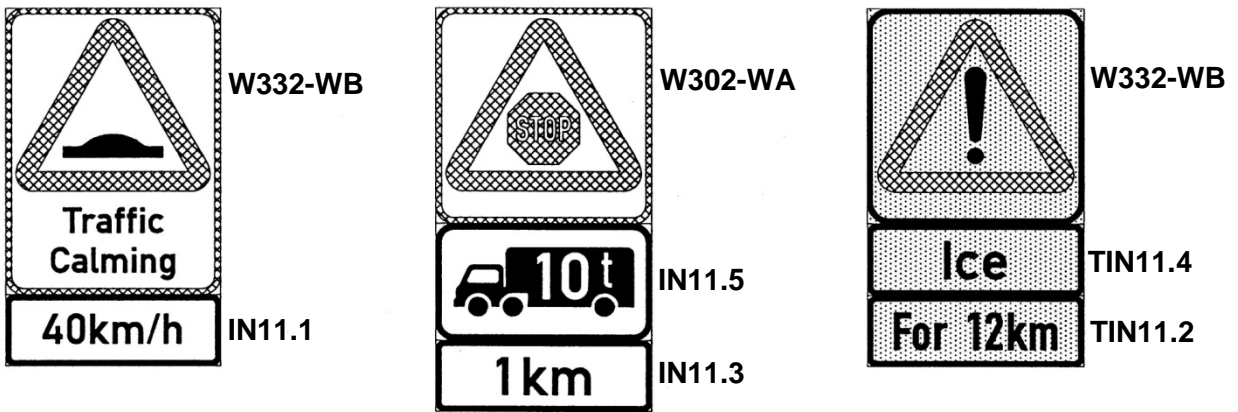
Fig. 3.4

Standard High Visibility Sign Sizes

Examples: Warning Signs



Examples: Dual Information Messages



Examples: Dual Warning or Regulatory and Warning Signs

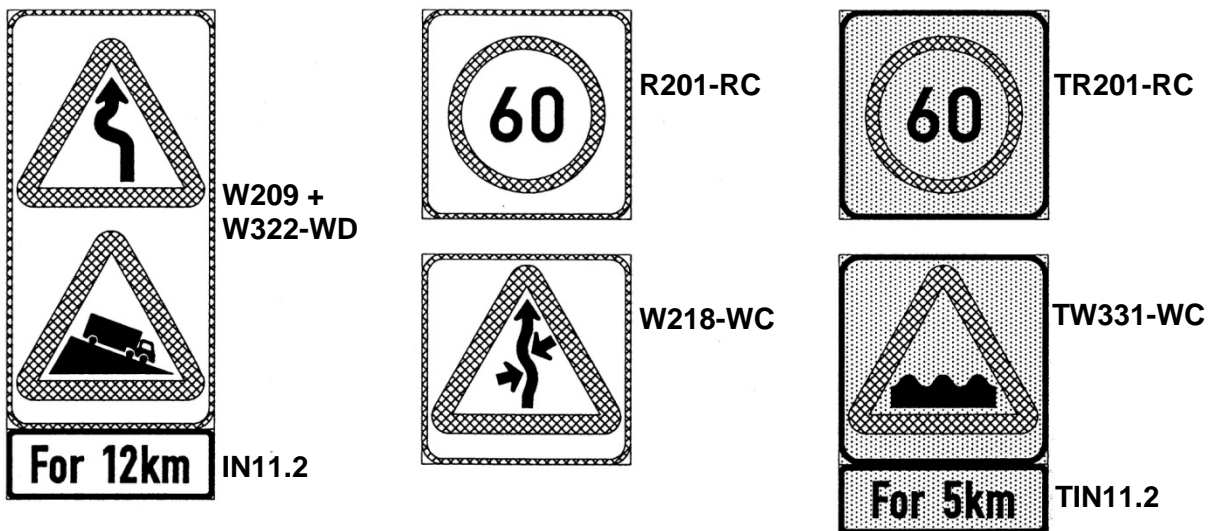


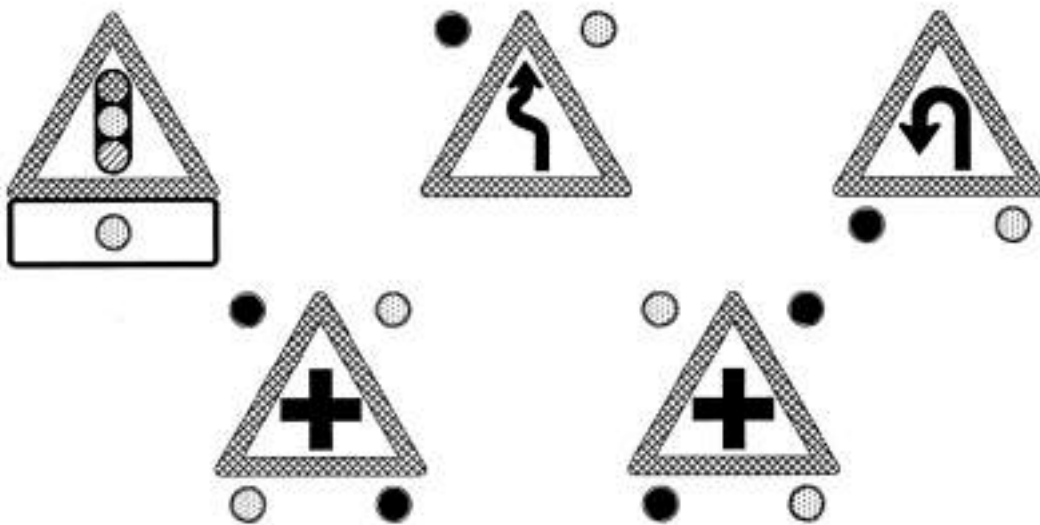
Fig. 3.5

Typical Examples of High Visibility Signs

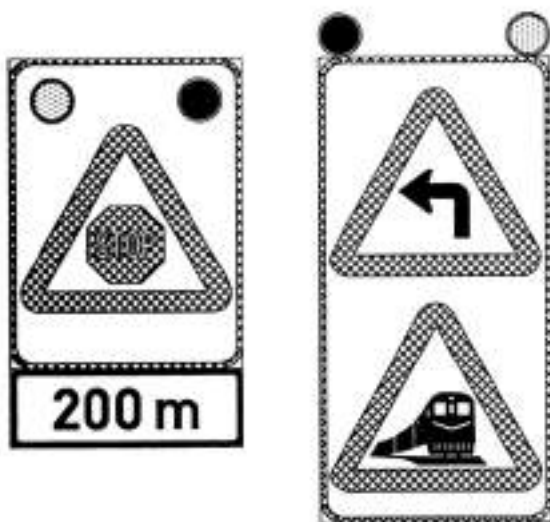
3.6.4 Flashing Lights with Warning Signs

- 1 There are a number of ways in which a flashing yellow light may be used to add emphasis to a warning sign. If the use of EMERGENCY FLASHING LIGHT warning sign W346 AND a second warning sign specific to the nature of the hazard is not appropriate due to space or other considerations a yellow flashing light may be mounted on the same post as the appropriate warning sign. More than one yellow flashing light may be used depending on the degree of severity of the hazard and the history of accidents at the site. Examples show the use of one, two or four lights.
- 2 As indicated in Subsection 3.5.2 dual red flashing disc signals may be installed at a railway crossing in conjunction with signs W403 or W404.
- 3 Details on the technical requirements for flashing lights used with warning signs are covered in Chapter 6. Dimensional details of flashing yellow lights used in HIGH VISIBILITY backgrounds of Types WE and WF are given in Volume 4, Chapter 3.

Examples: Warning Signs with Flashing Yellow Lights



Examples: High Visibility Backgrounds With Flashing Yellow Lights



Examples: Hazard Marker Signs With Red Flashing Disc Signals

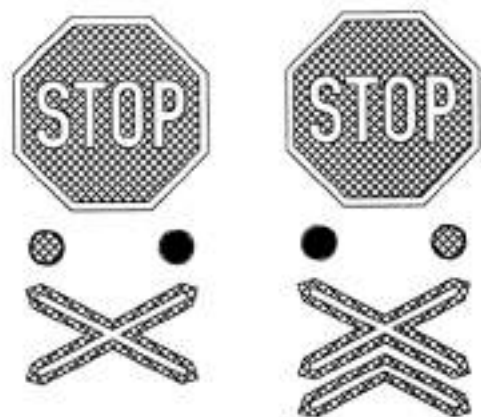


Fig. 3.6 Typical Warning Sign / Flashing Light Combinations

3.6.5 Variable Message

- 1 Development of the use of VARIABLE MESSAGE signs is in its early stages in South Africa. Such signs are costly and can only be justified in extreme situations.
- 2 VARIABLE MESSAGE warning signs are particularly appropriate in hilly country which is subject to sudden adverse changes in weather conditions. Such signs may be justified if a history of accidents exists in spite of other engineering measures taken to reduce these.
- 3 Three types of variable message sign manufacture can be used for warning sign messages:
 - (a) rotating vane
 - (b) roller blind
 - (c) optical (either illuminated bulbs, LED or fibre optics).

The first two types are limited in the number of messages which can be displayed, and which would normally require internal or external illumination.

The optical type can reproduce a wider range of messages but is expensive if the background to the symbol is to be fully illuminated to maintain true colour code. It is therefore recommended that optical VARIABLE MESSAGE warning signs be manufactured so that the red border and white symbol are illuminated and that the background be non-illuminated and semi-matt black in colour. This colour code should apply to both PERMANENT or TEMPORARY uses of VARIABLE MESSAGE warning signs (see Chapter 9).

- 4 According to resources available and the scale of the problem, a road authority may choose to give a warning message in a text form on a much more simple optical type of variable message sign. In this case one colour of display such as white or yellow on a normally matt or semi-matt black background is recommended.

3.7 NATIONAL VARIANTS

3.7.1 General

- 1 Whilst the objective of the SADC Road Traffic Sign System is to achieve the highest possible degree of harmonization of the system throughout the region it is likely that there will be a number of details which will remain unique to individual member countries.
- 2 For the purposes of identification any such signs are considered as NATIONAL VARIANTS specific to one or more of the SADC member countries. Variants can occur in one of three ways, namely:
 - (a) as an ADDITIONAL variant using a modified or different symbol for a sign function used in most member countries; or
 - (b) as a UNIQUE variant where the sign is used in only one country; or
 - (c) as an ADDITIONAL variant to accommodate the language of a SADC member country which does not have English as an official language (at the time of publishing such variations apply to the use of Portuguese in Angola and Mozambique, although every effort has been made to minimise this need by the use of symbolic messages).
- 3 All signs are listed once in colour in the Contents section and are provided with text describing their meaning and function. Any additional National variants involving a modified or different symbol will be identified pictorially in the National Variants section. Any National Variants which are unique to one country will appear in the National Variants section complete with appropriate text.
- 4 All signs are allocated numbers. An additional variant is allocated a three letter suffix identifying the country to which it belongs. In the case of a unique variant such a sign will be allocated a unique number which includes the appropriate National three letter suffix. If the sign becomes more widely used the use of the suffix will be discontinued. The letter codes allocated for each member country are as follows:

(a) Angola	Ang;
(b) Botswana	Bot;
(b) Democratic Republic of Congo	DRC;

- | | |
|------------------|------|
| (d) Lesotho | Les; |
| (e) Malawi | Mal; |
| (f) Mauritius | Mau; |
| (g) Mozambique | Moz; |
| (h) Namibia | Nam; |
| (i) Seychelles | Sey; |
| (j) South Africa | RSA; |
| (k) Swaziland | Sw; |
| (l) Tanzania | Tan; |
| (m) Zambia | Zam; |
| (n) Zimbabwe | Zim. |

- 5 When National Variants occur they will be covered in Subsections of this Section, bearing the name of the country. All National Variants are identified in the Contents by a black dot thus - ●. The purpose in identifying variants is to assist education on road traffic signs within the region for travellers beyond National borders, and to assist sign manufacturers.

3.7.2 Angola

- 1 Angola requires a number of National Variants to cater for the fact that traffic travels on the right side of the roadway. The following signs are additional National Variants to cater for this situation:
 - (a) TRAFFIC CIRCLE warning sign W201-Ang;
 - (b) TWO WAY TRAFFIC warning sign W212-Ang;
 - (c) TWO WAY TRAFFIC CROSSROAD warning sign W213-Ang.

The signs are illustrated in Figure 3.7 with references to the relevant subsections where the applicable text can be found.

- 2 In terms of the principle adopted within the warning sign system that signs placed on the left side of the roadway show their symbols facing to the RIGHT or into the road, when this is appropriate, these signs may be used in a mirrored form in Angola to comply with the principle. The relevant signs are illustrated in Figure 3.8.



W201-Ang

Refer: 3.3.1 page 3.3.1



W212-Ang

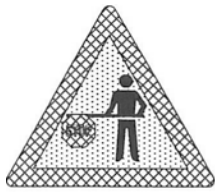
Refer: 3.3.7 page 3.3.4



W213-Ang

Refer: 3.3.8 page 3.3.5

Fig. 3.7 Direction of Movement Additional Variants - ANGOLA



TW305-Ang
Refer: 3.4.5 page 3.4.3



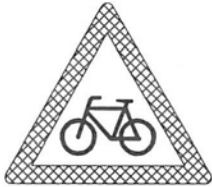
W306-Ang
Refer: 3.4.6 page 3.4.3



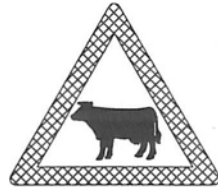
W307-Ang
Refer: 3.4.7 page 3.4.4



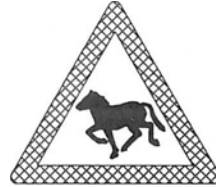
W308-Ang
- Refer: 3.4.8 page 3.4.4



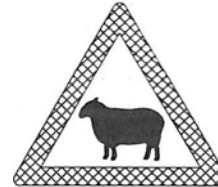
W309-Ang
Refer: 3.4.9 page 3.4.5



W310-Ang
Refer: 3.4.10 page 3.4.5



W311-Ang
Refer: 3.4.10 page 3.4.5



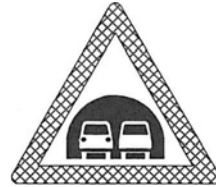
W312-Ang
Refer: 3.4.10 page 3.4.5



W313-Ang
Refer: 3.4.11 page 3.4.6



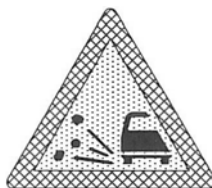
W318-Ang
Refer: 3.4.14 page 3.4.7



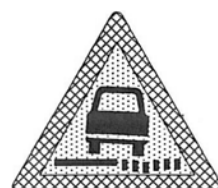
W319-Ang
Refer: 3.4.15 page 3.4.8



W327-Ang
Refer: 3.4.22 page 3.4.12



TW338-Ang
Refer: 3.4.31 page 3.4.17



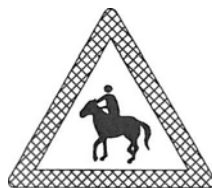
TW342-Ang
Refer: 3.4.34 page 3.4.19



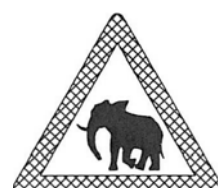
W348-Ang
Refer: 3.4.39 page 3.4.22



W352-Ang
Refer: 3.4.43 page 3.4.24



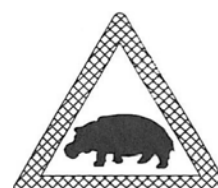
W356-Ang
Refer: 3.4.47 page 3.4.26



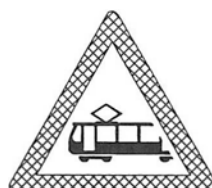
W357-Ang
Refer: 3.4.11 page 3.4.6



W358-Ang
Refer: 3.4.11 page 3.4.6



W359-Ang
Refer: 3.4.11 page 3.4.6



W362-Ang
Refer: 3.4.50 page 3.4.27

Fig. 3.8 Warning Signs with Reversed Symbols - ANGOLA

